

Town of Frisco Trails Master Plan Planning Area 1

Legend

Existing Trails

- Existing USFS System Trails
- Existing Non-System Trails

Bike Facilities

- Bike Lane
- Recreation Path (Paved)

Proposed Trails

- Proposed Trails
- Proposed Connectivity Changes

Points of Interest

- Trailheads
- Parking
- Fishing
- Campground

North

0 0.1 0.2 0.4 0.6 0.8 Miles

Consultants:

SE GROUP

FEHR & PEERS

MARY HART DESIGN
land planning • landscape architecture

Partners:

TOWN OF FRISCO
COLORADO

SUMMIT COUNTY
Open Space & Trails

GOCO
GREAT OUTDOORS COLORADO

U.S. FOREST SERVICE
DEPARTMENT OF AGRICULTURE

Planning Area 1

Goals

- Difficulty Level: Easy ●
- Short and long loops for daily use of nearby residents
- Create better access to street network, Rec Path, and other trails
- Add wayfinding and signage so it is easy to navigate this area from Town
- Anticipate needs of future Lake Hill residents

Existing Trails and Connections

- Work with the Forest Service and private property owners to improve loops for short and longer experiences. Some steeper trails need re-routing for sustainability.

A Trail A: Adopt this non-system trail that has been used by nearby residents to walk along the waterfront.



New Trails

- Trail 1:** A future soft-surface trail would connect the Lake Hill development to adjacent National Forest System lands. It will help disperse use in an area that is anticipated to have future residential growth.
- Trail 2:** This paved trail would connect the Lake Hill development to the Rec Path with a grade-separated connection over the Dillon Dam Road. This would provide residents with a safe and direct bicycle and pedestrian connection.
- Trail 3:** A soft-surface connection between Silverthorne and Frisco that would connect Wilderrest and Frisco, a connection that only exists with major roadways at present. This trail could first connect to the I-70 scenic overlook, then again to the Meadow Creek Trailhead to give users options and multiple access points. Collaboration with the Forest Service and private property owners is key for this trail.
- Trail 4:** An additional easy walking trail between the residential areas and the waterfront to help separate and disperse walkers from bicyclists on the Rec Path.
- Trail 5:** Re-route the Rec Path in this location to ease confusion. With this new alignment, staying straight would keep users by the lake, and turning would get users back to town.
- Trail 18:** Create a new connection between the Willow Creek Trailhead and the Rec Path to provide waterfront access and ease confusion in the neighborhoods north of this location. This is a wetland area where bikes are currently not allowed—sensitivity in design is important here.

New Connections

- Connection 1:** A more direct connection between the Rec Path and the Basecamp/Frisco Transit Center Area. Many of these developments are isolated cul-de-sacs off Dillon Dam Road, and have no connectivity to the rest of the street network.
- Connection 2:** A striped bike lane and better signage to connect two segments of the Rec Path system.
- Connection 3:** A striped bike lane and better signage to connect two segments of the Rec Path system.
- Connection 4:** A paved sidewalk/multi-use path on the west side of Highway 9. There are many major employment centers and shopping areas that would benefit from better pedestrian connections.
- Connection 5:** A paved sidewalk and striped shoulder to create better bicycle and pedestrian connections across I-70 to connect with the Meadow Creek Trailhead and other proposed trail connections.



Consultants:

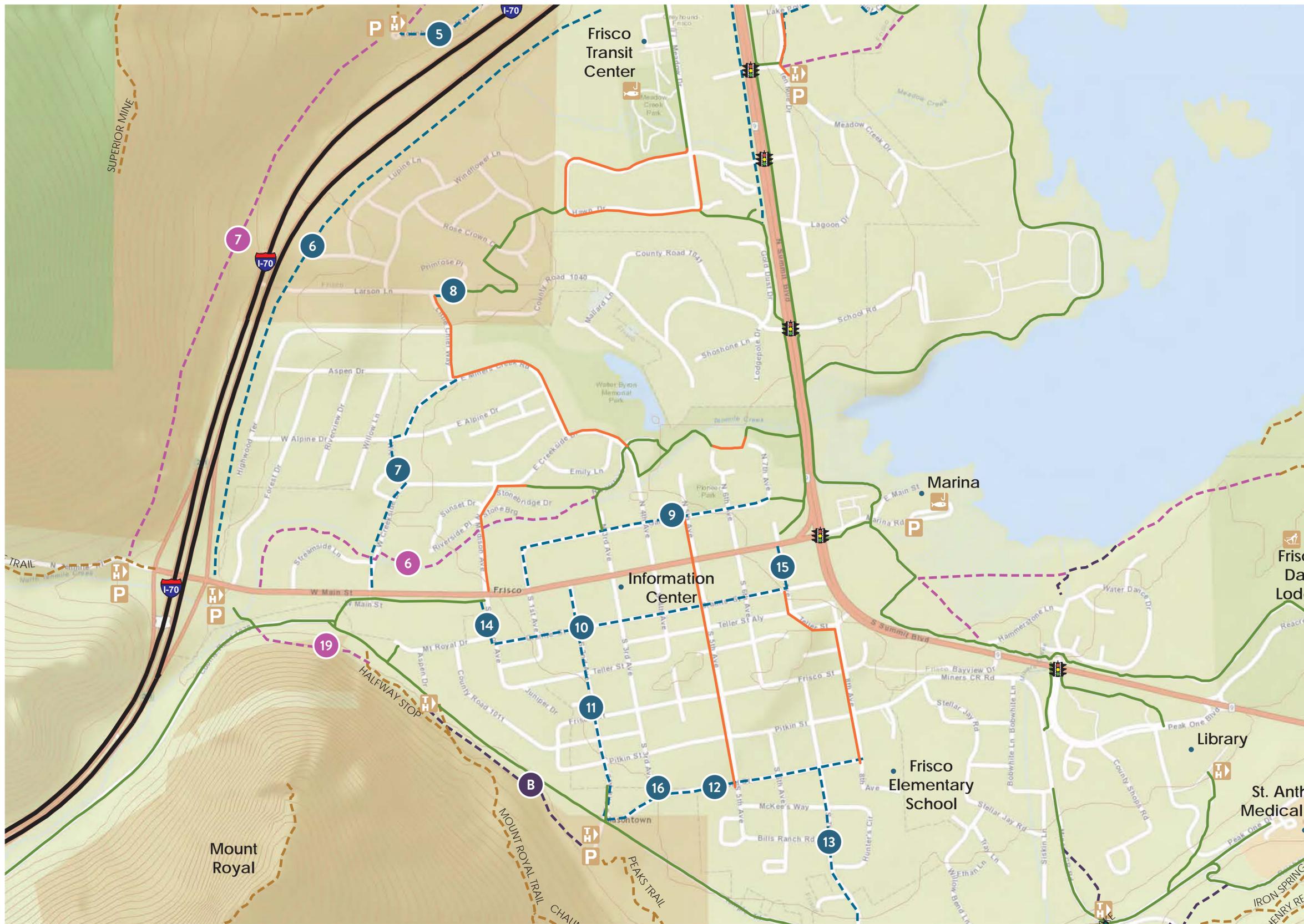


Partners:



Town of Frisco Trails Master Plan

Planning Area 2



Legend

Existing Trails

- Existing USFS System Trails
- Existing Non-System Trails

Bike Facilities

- Bike Lane
- Recreation Path (Paved)

Proposed Trails

- Proposed Trails
- Proposed Connectivity Changes

Special Management Area

- Elk Habitat

Points of Interest

- Trailheads
- Parking
- Fishing
- Campground

North

0 0.1 0.2 0.4 0.6 0.8 Miles

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 DEPARTMENT OF AGRICULTURE

Planning Area 2

Goals

- Difficulty Level: Easy ●
- Provide direct connections through and between areas of Town
- Add wayfinding and signage to help navigate through Town, find the Rec Path, and access nearby trails
- Connect major trailheads with connector trails to reduce need for parking and to provide trail options
- Open up Tenmile Creek to more residents and visitors
- Improve trailheads immediately adjacent to Town

Existing Trails and Connections

- There are few existing trails in this area with many trails located just outside of the planning area.
- Existing Rec Path, Frisco “Pathway” multi-use shoulders, and shared lanes exist in Town for bicyclists. These facilities need to be striped, signed, and branded more clearly so that users can understand the network and nearby destinations.

- B Trail B:** Formalize the non-system trail that exists parallel to the Rec Path between the Mt. Royal Trailhead and Miner’s Creek Road.



New Trails

- 6 Trail 6:** An in-town beginner soft-surface trail along Tenmile Creek. Interpretation and benches could be added to provide creekside experiences. This is mostly private property so collaboration and easements with property owners would be key.
- 7 Trail 7:** Provide a soft-surface connection between Meadow Creek Trailhead and North Tenmile Trailhead.
- 19 Trail 19:** Extend the non-system trail at Mt. Royal and connect it directly to the parking lot at the end of West Main Street. This will disperse hiking to this popular section of Rec Path.

New Connections

- 6 Connection 6:** A direct connection between Basecamp and West Main Street. The current pathway system is winding and not intuitive. Wetlands and private property in this area would have to be evaluated.
- 7 Connection 7:** A multi-use pathway connection along Creekside Drive. This would help bicyclists and pedestrians navigate a direct route through this neighborhood.
- 8 Connection 8:** Continue the multi-use pathway along Larson Lane and add signage so that the Rec Path is easier to find.
- 9 Connection 9:** A bike lane on Galena Street to create an alternative path to Main Street, dispersing use.
- 10 Connection 10:** Bike lanes on both sides of the roadway and sidewalk on at least one side of Granite, with access control for parking and driveways.
- 11 Connection 11:** Bike lanes on both sides of roadway along 2nd Avenue to connect Main Street with Rec Path more intuitively.
- 12 Connection 12:** A multi-use pathway connection along Belford Street.
- 13 Connection 13:** Extend the bike lane between Belford Street and Rec Path on 7th Avenue.
- 14 Connection 14:** A multi-use pathway to re-route bicycles off Main Street and direct them towards Granite Street.
- 15 Connection 15:** “Shared Roadway” stamp and additional signage to connect 7th Avenue bike lane to Main Street and prevent users from getting dead-ended at Highway 9 where there are no bicycle or pedestrian facilities.
- 16 Connection 16:** Connect 3rd Avenue to Belford Street with a multi-use pathway for better grid connectivity.

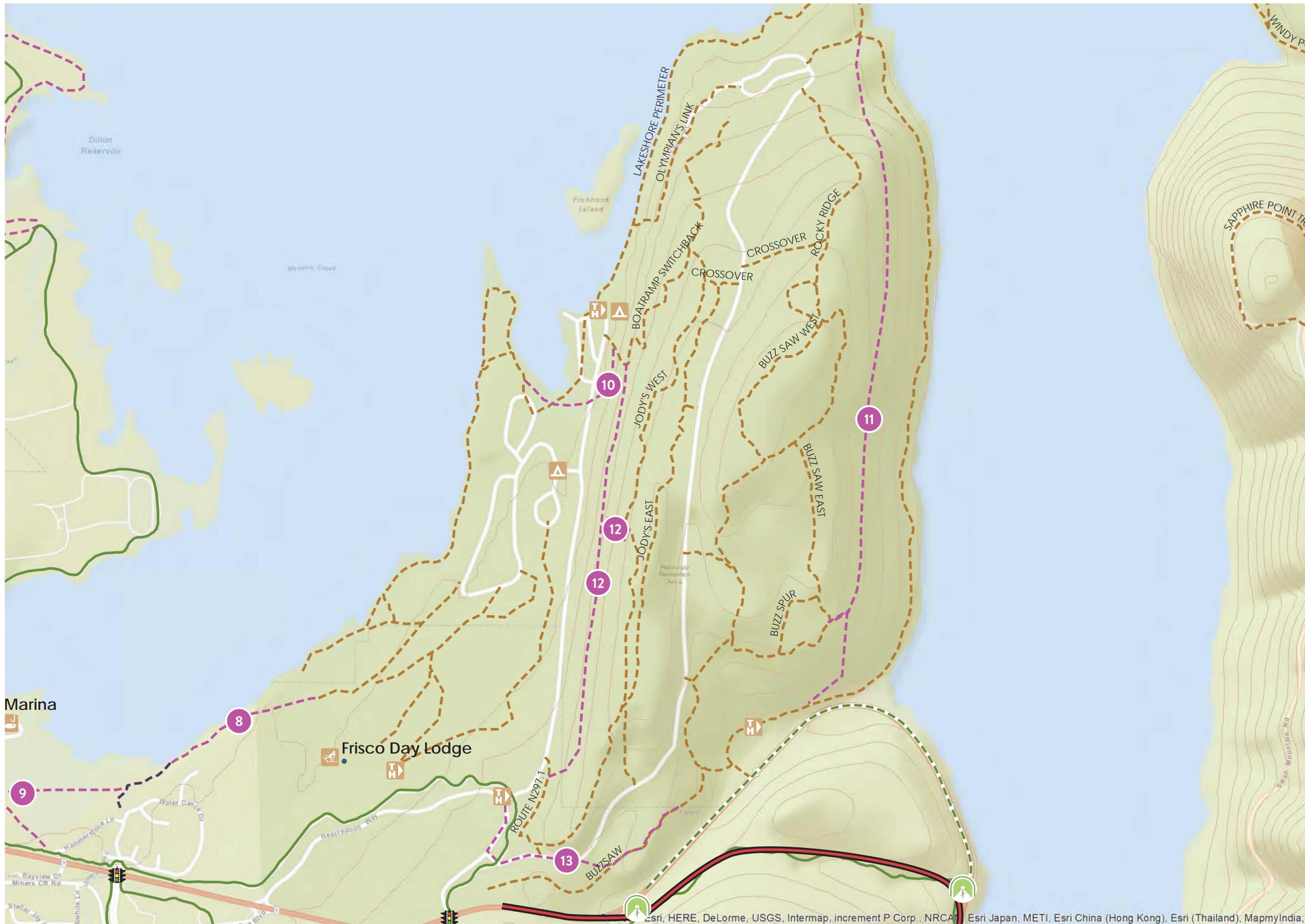


Consultants:



Partners:





Town of Frisco Trails Master Plan Planning Area 3

Legend

Existing Trails

- Existing USFS System Trails
- Existing Non-System Trails

Bike Facilities

- Bike Lane
- Recreation Path (Paved)
- Realigned Recreation Path (Paved)
- New Road Alignment

Proposed Trails

- Proposed Trails
- Proposed Connectivity Changes

Points of Interest

- Trailheads
- Parking
- Fishing
- Campground
- Bikeway Underpass

North

0 0.1 0.2 0.4 0.6 0.8 Miles

Consultants:

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Planning Area 3

Goals

- Difficulty Level: Easy, Intermediate ● ■
- Improve the recreation experience at the Peninsula
- Add wayfinding, signage, and trail maps at major junctions to help navigate the trails
- Connect PRA to Marina
- Create safer crossings at Highway 9 intersections to connect PRA to Town
- Design trails in a way that will benefit Nordic skiing needs, as this is the premier Nordic skiing facility in Town

Existing Trails and Connections

- Existing trails are heavily trafficked with hikers, mountain bikers, and Nordic skiers. Many of the trails need maintenance or possibly be re-routed for long-term sustainability.
- The only way to get to the Marina is along the Rec Path by Highway 9.
- It is difficult to access the PRA unless in a vehicle. Pedestrian and bicyclist access is far from the activities and trailheads within the PRA.

New Trails

- 8 **Trail 8:** Create a soft-surface connection from an existing non-system trail behind the Water Dance neighborhood to the Peninsula. This would provide direct access to the waterfront trails.
- 9 **Trail 9:** Create a paved or soft-surface trail between the Peninsula and the Marina. Re-route the Rec Path in this location to create a better experience and mitigate a dangerous and sharp curve by the sanitation property.
- 10 **Trail 10:** Re-align a short section of trail to connect the outer Peninsula trails to the lakefront trail. This will create a new connection that avoids connecting directly to the campgrounds.
- 11 **Trail 11:** A future soft-surface trail to create additional loops and disperse trail use across the Peninsula trail system.
- 12 **Trail 12:** A future soft-surface trail connecting the Pine Cove Campground with access points to the south. This will provide additional loops and disperse trail use across the system. Switchbacks could connect this trail to Jody's at an appropriate mid-point.
- 13 **Trail 13:** This is the re-routed trail that will provide access to the Perimeter trail from the new, relocated Dickey Trailhead.



Photo: Todd Powell



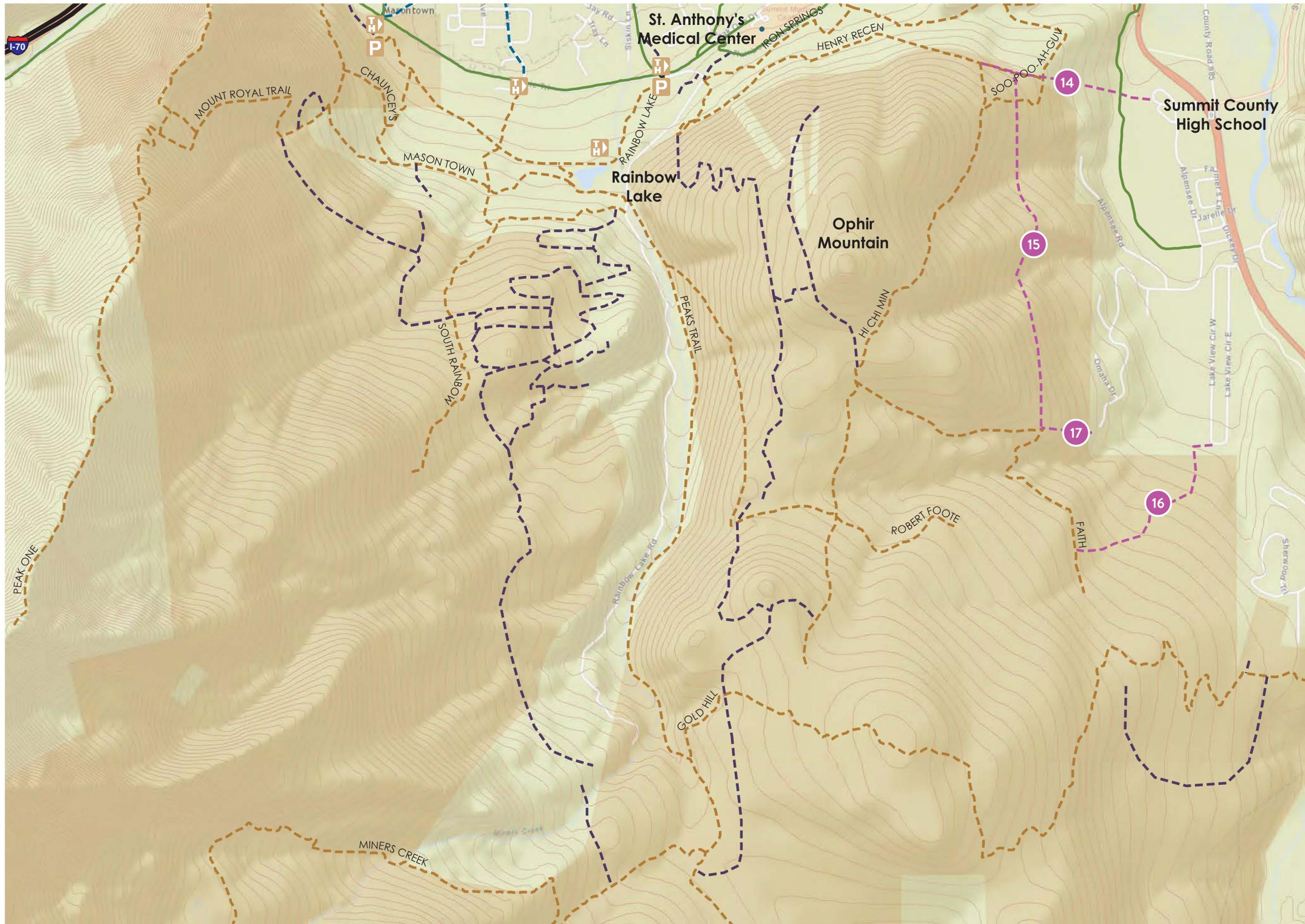
Photo: Todd Powell

Consultants:



Partners:





Town of Frisco Trails Master Plan Planning Area 4

Legend

Existing Trails

- Existing USFS System Trails
- Existing Non-System Trails

Bike Facilities

- Bike Lane
- Recreation Path (Paved)

Proposed Trails

- Proposed Trails
- Proposed Connectivity Changes

Special Management Area

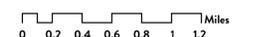
- Elk Habitat

Points of Interest

- Trailheads
- Parking
- Fishing
- Campground



North



Consultants:

SE GROUP
FEHR PEERS

MARY HART DESIGN
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Planning Area 4

Town of Frisco
Trails Master Plan
www.FriscoTrails.com

Goals

- Difficulty Level: Intermediate, Expert ■ ◆
- Create a high-quality hiking and mountain biking system right in Frisco's backyard
- Add signage to the most popular trails while lightly marking others for a more backcountry experience
- Maintain most popular trails: Mt. Royal, Rainbow Lake, Peaks Trail
- Work collaboratively with the Forest Service to achieve trail system, maintenance, and trail sustainability goals and standards

New Trails

- 14 **Trail 14:** A new soft-surface connection between Summit County High School and the Ophir Mountain area.
- 15 **Trail 15:** A new soft-surface connection between Frisco and Gold Hill, avoiding the steep climbs on the Peaks Trail.
- 16 **Trail 16:** Provide access between the Lake View Meadows neighborhood and existing trails.
- 17 **Trail 17:** Provide access between the Highland Meadows neighborhood and existing trails.

Existing Trails and Connections

- There are many heavily trafficked National Forest System trails that are accessed from multiple trailheads along the Rec Path. Many of these trails are steep and in need of maintenance.
- There are many existing non-system trails in this area. These trails need to be evaluated individually as to what purpose they provide, and whether they should be adopted, realigned, or removed.
- Most of this area is elk habitat so careful collaboration with Colorado Parks and Wildlife and the Forest Service will have to take place for new trails in this area.



Consultants:



Partners:

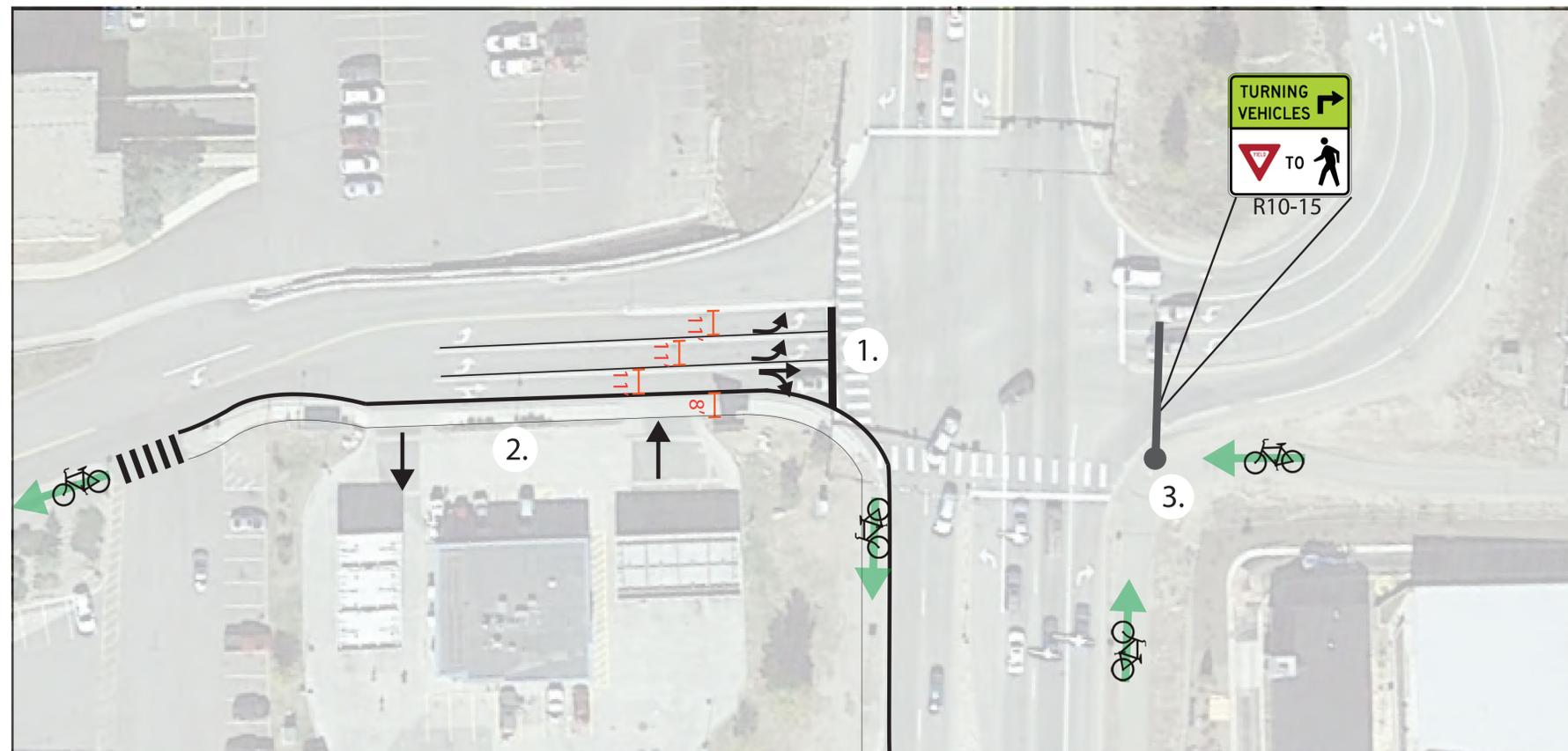


Highway 9 Intersections

Dillon Dam Road and Highway 9 Proposed Intersection Redesign

Unsafe crossings of Highway 9 create an impediment to safe bicycle, pedestrian, and trail connections throughout town. Presented are concepts and ideas that could be furthered through collaboration with CDOT.

Let us know if these are heading in the right direction!



Notes:

1. Take 1' from each EB travel lane, and widen the existing 5' sidewalk to an 8' trail. Option to move concrete median and make trail 10', but truck turn radii will need to be tested.
2. Consider access management at Valero; one way in one way out
3. Consider Leading Pedestrian Interval

Alternatives:

- A. Narrow lanes (as shown above)
- B. Remove EB lane to widen trail and analyze  or 
- C. Analyze  (w/ protected EB right turn when bikes/peds in crosswalk)
 - Consider right turn arrow to eliminate conflict between peds and right turning vehicles



Consultants:



Partners:

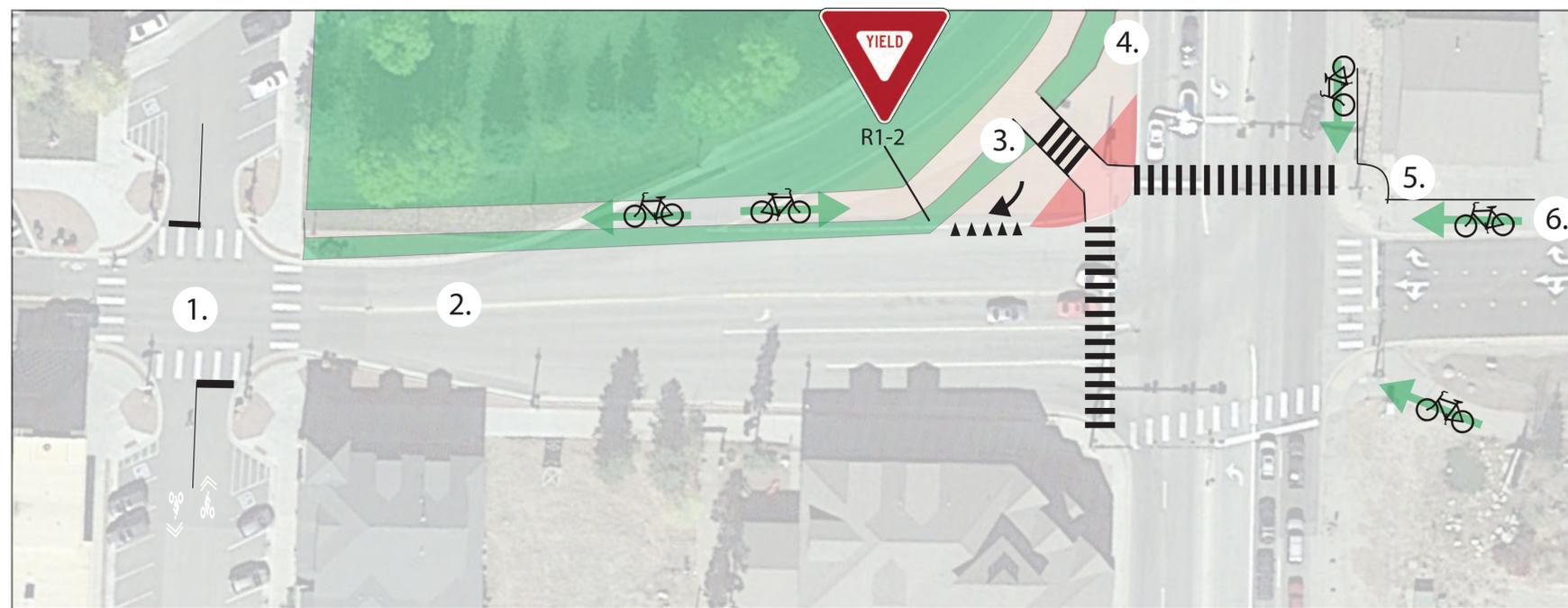
 

Highway 9 Intersections

Main Street And Highway 9 Proposed Intersection Redesign

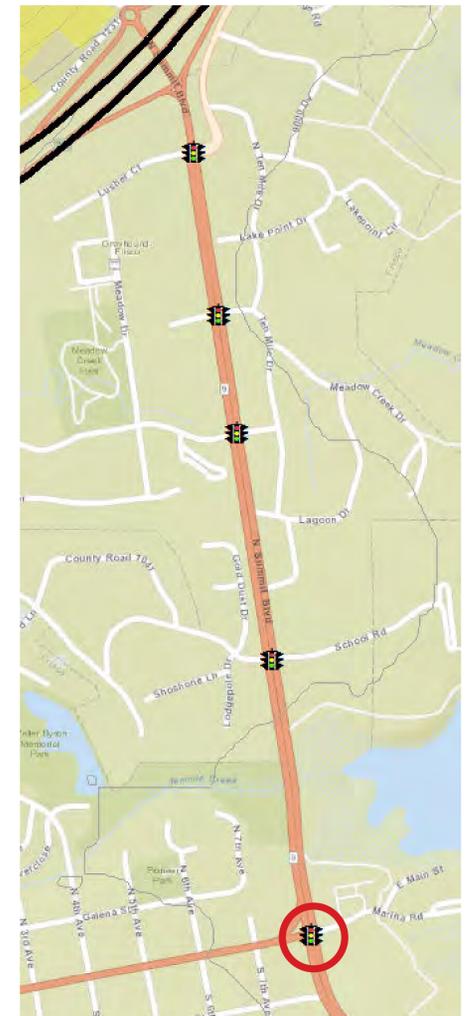
Unsafe crossings of Highway 9 create an impediment to safe bicycle, pedestrian, and trail connections throughout town. Presented are concepts and ideas that could be furthered through collaboration with CDOT.

Let us know if these are heading in the right direction!



Notes:

1. Evaluate as all way stop, with additional path to on-street treatment for bikes
2. Gateway/welcome treatment
3. Crosswalk
4. Evaluate right-turn queueing at slip lane
5. Widen curb area for gathering during red lights
6. Extend bike path to Marina



Consultants:



Partners:



Highway 9 Intersections

Town of Frisco
Trails Master Plan
www.FriscoTrails.com

Unsafe crossings of Highway 9 create an impediment to safe bicycle, pedestrian, and trail connections throughout town. Presented are concepts and ideas that could be furthered through collaboration with CDOT.

Let us know if these are heading in the right direction!



Long-term “Vision” Recommendations

- Build an underpass at Dillon Dam Road: this intersection provides a key connection for Lake Hill and other waterfront neighborhoods (1)
- Build an underpass between the County Commons and the PRA: this is the main desired route to the PRA and ensures safe access from existing transit stops.(2)

General Recommendations

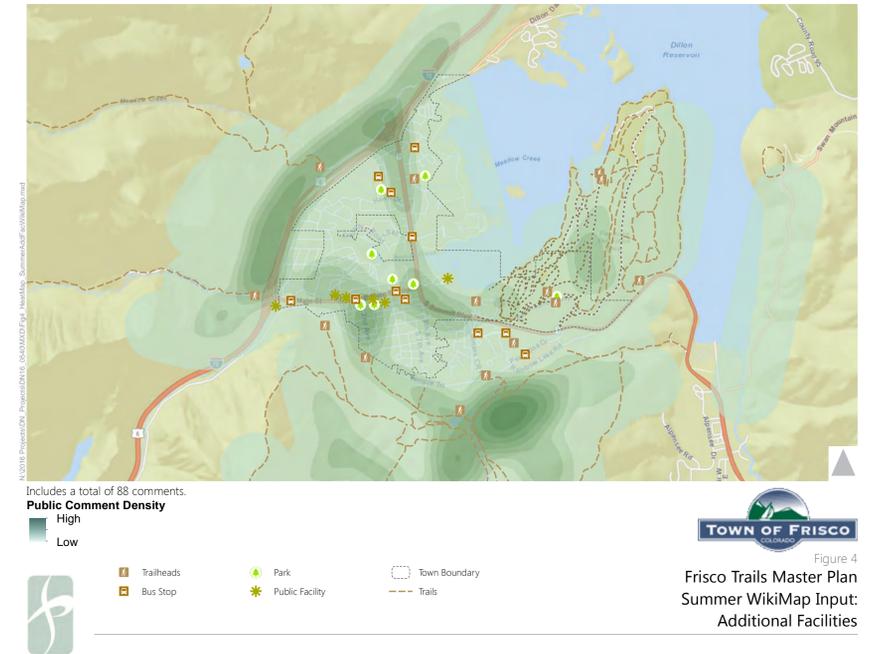
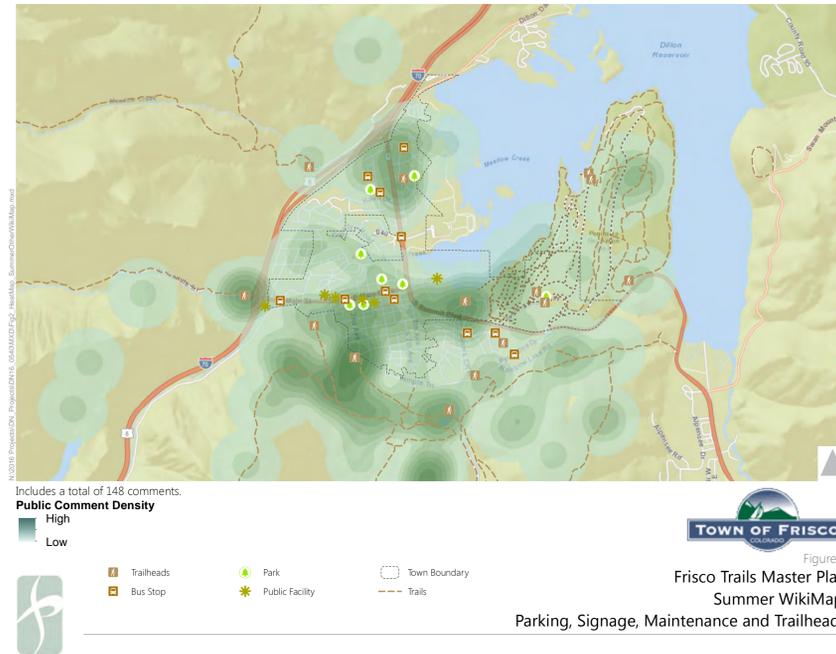
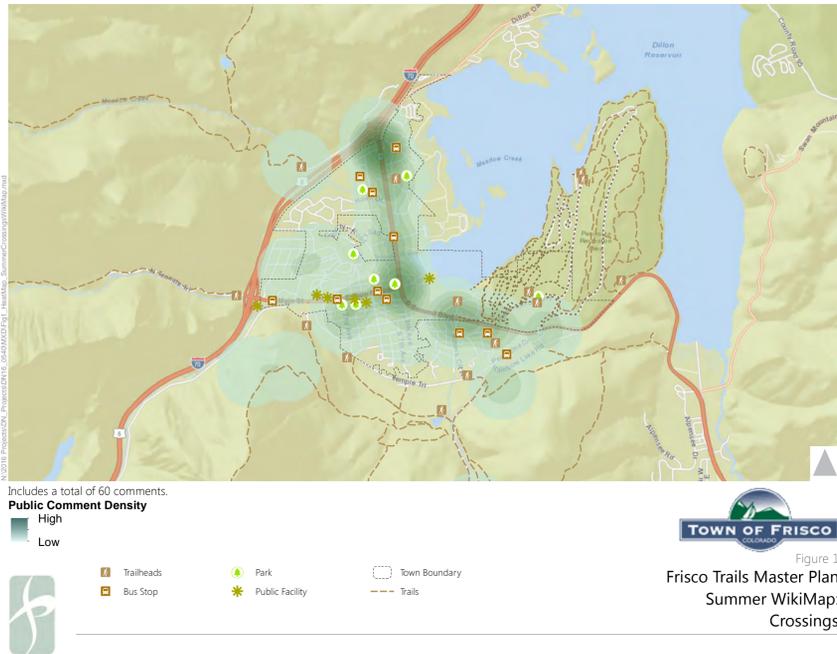
- Evaluate all traffic signals for pedestrian safety, especially wait times and turn phases during walk signals.
- Ensure pedestrian safety measures in the physical design of signalized intersections such as bulb-outs and sidewalks to nearby destinations.

Feedback and additional ideas:



Summer WikiMap Input

Town of Frisco
Trails Master Plan
www.FriscoTrails.com



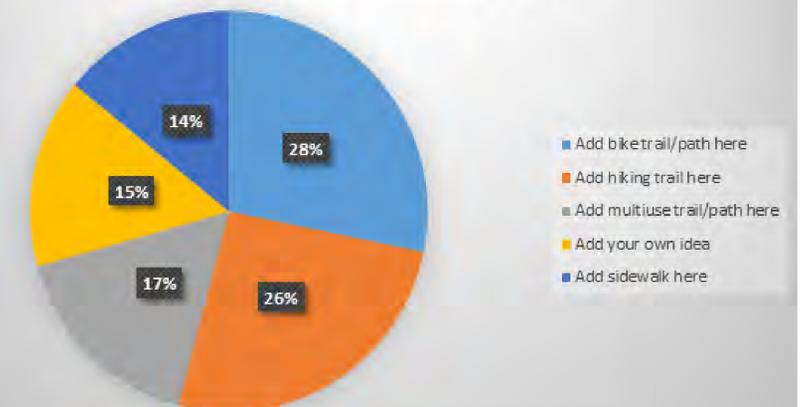
The crossings of most concern were studied further. See the Intersection Concepts boards.

The trail recommendations were heard loud and clear! See these in action for each planning area.

Summer WikiMap Point Input



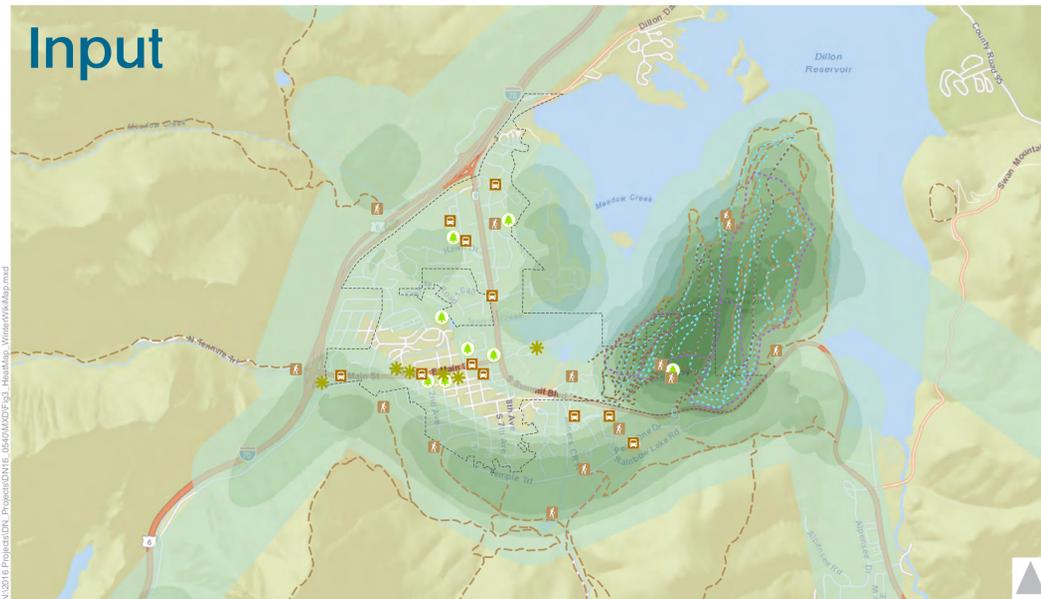
Summer WikiMap Line Input



Winter Input and Recommendations

Town of Frisco
Trails Master Plan
www.FriscoTrails.com

Input



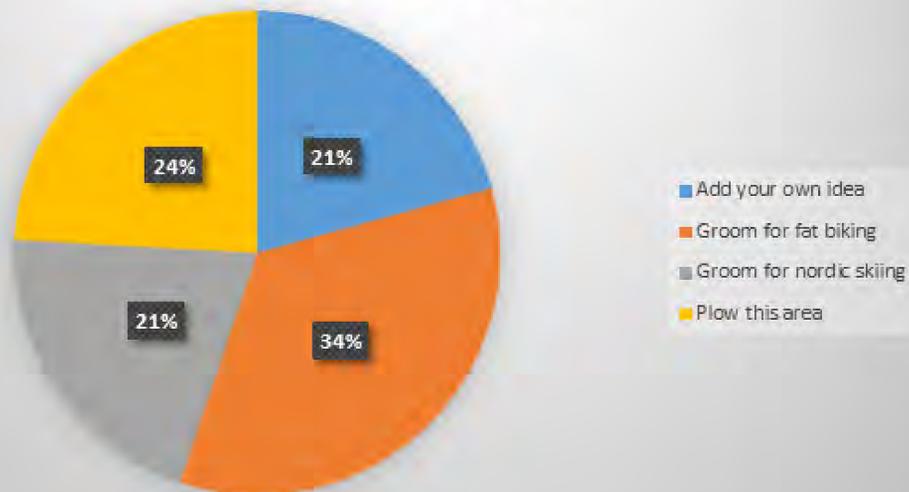
Includes a total of 34 comments.

Public Comment Density



Figure 3
Frisco Trails Master Plan
Winter WikiMap Input:
Groom or Plow

Winter WikiMap Input



Recommendations

- Plow Rec Path between 7th Avenue and the West Main Street parking lot. Plow Rec Path connection Larson Lane to the Frisco Transit Center. This will create paved routes for daily connections around Town.
- Groom Rec Path for Nordic skiing between 7th Avenue and new underpass to PRA for the ability to ski from Town to the PRA. Explore grooming Rec Path all the way to Breckenridge along new alignment by Lake Dillon.
- Pilot a fat biking loop at the PRA - the appropriate alignment will be determined by Nordic Center.
- The Nordic Center will be run by the Town of Frisco starting this winter. This gives the Town an opportunity to assess the current trail system as well as explore opportunities for new and more difficult Nordic trails.



What about bikes on Main Street?

Comments we have heard...

- “Too many bikes on the sidewalk!”
- “Biking on Main Street is dangerous!”
- “We need other bike routes”
- “Tourists LIKE to bike Main Street - it is a way to experience Town.”



Recommendations

- Implement alternate bike routes on Galena and Granite Streets
- Increase wayfinding and signage to discourage bike use on sidewalks, use Main Street as a shared street, or use alternate routes
- Re-stamp “shared road” in the center of the lane, encouraging cyclists to take the lane
- Employ additional traffic calming measures such as a stop sign at 7th Avenue, a speed messaging sign at the east entrance, and enforcement of speeding in the downtown
- Explore back-in angle parking for cyclist safety instead of front-in angle parking
- Close off a half-block of 3rd Ave for a pedestrian plaza and shared space

Exploring the “Woonerf” concept of shared space...

Woonerf is a Dutch word for a “living street” where the space between businesses and residences is a shared space - a place for people to play, socialize, and engage in the community. With both physical design and cultural programming, these streets can calm traffic and create a great place for people.





How strongly do you support the following ideas that could implement the recommendations you saw today?

A sales tax or hotel tax measure to create a dedicated funding stream for trail development and maintenance

An allocation from Frisco's general fund to create a staff member or team dedicated to trail development and maintenance

The creation of a non-profit organization, initially funded by Town of Frisco, that works independently to secure grants, raise money, implement the recommendations in the plan, and maintain trails

User fees for particular trails or parking fees at popular trailheads

Development impact fees for new developments in the Town of Frisco

	Strongly support	Somewhat support	Do not support	Strongly do not support
A sales tax or hotel tax measure to create a dedicated funding stream for trail development and maintenance				
An allocation from Frisco's general fund to create a staff member or team dedicated to trail development and maintenance				
The creation of a non-profit organization, initially funded by Town of Frisco, that works independently to secure grants, raise money, implement the recommendations in the plan, and maintain trails				
User fees for particular trails or parking fees at popular trailheads				
Development impact fees for new developments in the Town of Frisco				



Consultants:

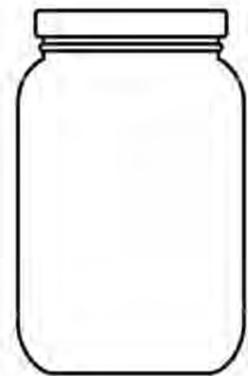
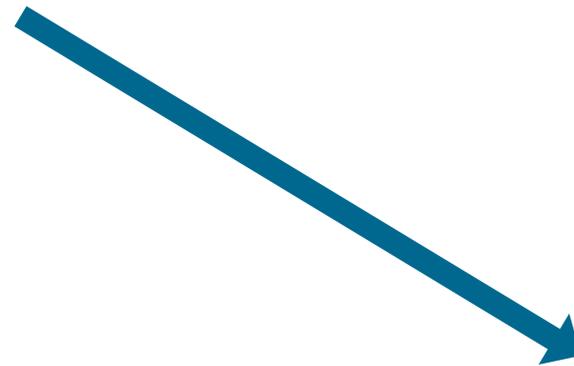
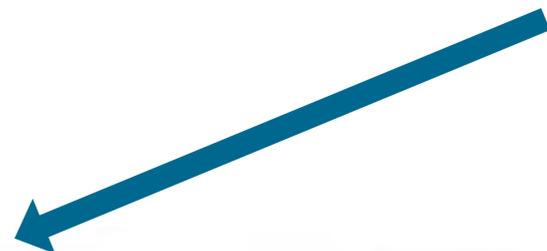


Partners:

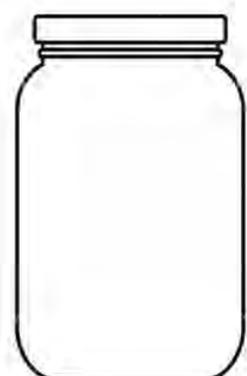




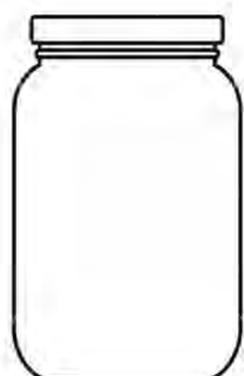
How do you want to spend your dollars in Frisco?



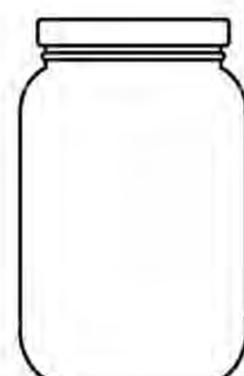
New soft-surface trails



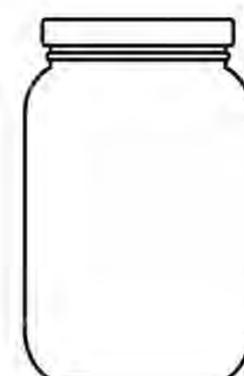
Trail maintenance and adoption of non-system trails



New paved connections in and around Town



Designing safer intersections



Nordic trails, increased grooming and plowing



Consultants:



Partners:

