

Attachment A: Application Materials



P.O. Box 5540
711 B Granite Street
Frisco . CO 80443
970.453.7002
www.allen-guerra.com

April 25th, 2025

To: **Town of Frisco**
PO Box 4100
Frisco, CO 80443

Re: **Prime Sauce**
20 East Main Street . Frisco . CO

DESCRIPTION OF PROJECT

Prime Sauce is a new project on the southeast corner of Madison Avenue and Frisco's Main Street. The Granite Street Alley forms the southern boundary, with the Beaver Plaza Condos to the east. The current building which houses Frisco Prime will be removed, along with the current dirt parking lot. The large trees along Granite Street Alley will be protected wherever possible.

With distinctive styling and highly efficient floor plans, this project looks to continue the evolution of "Frisco Style" by picking up many of the cues in the surrounding architecture while introducing some new uses for classic materials. The use of outdoor decks on multiple levels not only keeps the street facades at the pedestrian level, it also allows the occupants to take in the full outdoor experience. With direct views of Mount Royal, Wichita, Chief and Buffalo Peaks as well as some distant views of Grey & Torreys, there are decks on all sides of the project, bringing the outside in and reducing project mass at the same time.

The project will consist of nine units, ranging from 785 sf to 2,156 sf of living space on the upper two levels. Five units are permitted by the base zoning with 4 bonus units, 2 of which will be deed restricted per the Town's standard restriction. The ground floor proposes a 3,555 sf restaurant & bar space, capable of seating 140 customers inside, and another 36 on the covered outdoor patio. A 7,542 sf parking garage containing 25 stalls provides all of the required parking for the residential units above. The units will have 2-3 bedrooms and 1-3.5 bathrooms, with an open office space also shown in Unit 1, 6 & 7. Window trim that blends with the surrounding siding, asymmetrical window patterns and metal siding all serve to give the architecture a current feel, while gable roofs, angle braces, real wood siding, and a brick base help the buildings connect with commercial buildings on Main Street and the homes traditionally found in the area.

The units are arranged on the site to provide the most view and sun exposure possible, while maintaining privacy between the different owners. Window placement along the walls has been designed to allow for great views of Mount Royal to the west, and Grays & Torreys Peaks to the east.

There are a few bulk plane encroachments, totaling 342 cubic feet, but they are exclusively made up of the brick “fins” at the deck edges and the overframed gable roof elements. No living space is contained in any bulk plane encroachment, and the project is under the 350 CF limit for these encroachments.

Two elements that have been added to the project since the Planning Commission saw the Sketch Plan back in November, are the snow storage system on the upper-level decks, and the revitalization of the Right of Way space between the northern property line and the Main Street sidewalk. On the decks, a unique use of planter boxes as snow storage elements solves the problem of where snow on the upper-level decks will be shoveled. These boxes are 18” deep, and equipped with internal drainage systems to route the melting snow to the storm water system. Each deck has at least one dedicated snow storage area, meeting the required square footage for snow storage as outlined in the Town code.

In the space between the northern property line and the Main Street sidewalk, the project proposes to add four more trees, in addition to the required number of trees on site, along with a park bench on a small brick patio and a bike rack, all satisfying the Town’s requirement to provide a community benefit. This area will also receive irrigation and native seeding, to help beautify a currently unused space that will be much improved with the proposed landscaping. A revocable license agreement has been discussed with the Town Engineer, who has indicated that the Town will be in favor of approval. This agreement will be in place prior to certificate of occupancy of the building.

This project is near the Madison Avenue bike path, is within easy walking distance of Main Street and several bus stops and makes perfect sense as a location for additional housing. The developers are excited to begin the redevelopment of this lot and look forward to incorporating the Planning Commission’s comments into a final vision for the project.

Sincerely,








Andrew Stabile, AIA.
Allen-Guerra Architecture

Allen-Guerra Architecture
P.O. Box 5540 . 711 B Granite Street . Frisco . CO 80443
970.453.7002 . www.allen-guerra.com

ARCHITECTURE . CONSTRUCTION . PLANNING . INTERIOR DESIGN

PRIME SAUCE

EXTERIOR MATERIALS SCHEDULE
DATE: 6 SEPTEMBER, 2024

LABEL	ITEM	COLOR	DESCRIPTION
M1	ROOF - MEMBRANE		SINGLE PLY ROOFING MEMBRANE EVERGUARD TPO COLOR "SLATE GRAY"
M2	ROOF - METAL & CAP FLASHING		STANDING SEAM METAL ROOF COLOR "DARK BRONZE" NON-REFLECTIVE
M3	FASCIA		2X CEDAR PER DETAIL - STAIN WITH SUPERDECK 2320 "CAPE BLACKWOOD"
M4	SOFFIT		1x4, T&G CEDAR - STAIN WITH SUPERDECK 2113 "GRANITE"
M5	EXPOSED STEEL POSTS & BEAMS		PAINTED STEEL PER DETAILS, COLOR TO BE GRAPHITE
M6	VERTICAL SIDING		1x6, 1/8" REVEAL T&G CEDAR - SPECIALTY WOOD PRODUCTS COLOR - "WELLSHIRE"
M7	HORIZONTAL SIDING		1x8, 3/8" REVEAL T&G CEDAR - SPECIALTY WOOD PRODUCTS COLOR - "RINO RUSTIC"

NOTE: ALL EXPOSED METAL INCLUDING, BUT NOT LIMITED TO, TYPICAL FLASHING, DOWNSPOUTS, GUTTERS, DRIP EDGE, VENT STACKS, FLUE PIPES, ETC, SHALL BE PAINTED TO BLEND UNABTRUSIVELY WITH ADJACENT SURFACES.



PRIME SAUCE

EXTERIOR MATERIALS SCHEDULE
DATE: 6 SEPTEMBER, 2024

LABEL	ITEM	COLOR	DESCRIPTION
M8	DOOR/WINDOW TRIM		2X CEDAR PER DETAIL – STAIN WITH SUPERDECK 2320 “CAPE BLACKWOOD”
M9	DOORS/ WINDOWS		JEL WEN WINDOW COMPANY ALUMINUM CLADDING COLOR – BLACK
M10	EXPOSED TIMBER		DOUG FIR #1 STAIN WITH SUPERDECK 3504 “WOODRIDGE”
M11	DECKING		BISON PORCELAIN TILE ON PEDESTAL SYSTEM COLOR-LIGHT GRAY
M12	GARAGE DOORS		CEDAR STYLES & RAILS PER DETAILS – STAIN TO MATCH TRIM (M8) – WALLS PANELS TO MATCH (M15) FROSTED GLASS PANELS
M13	BRICK VENEER		2.5” TRUE BRICK SMOOTH FINISH DARK BROWN BRICK WITH LIGHT GREY GROUT
M14	STONE VENEER		KANSAS PRAIRIE STONE PRAIRIE GREY DRY STACKED

NOTE: ALL EXPOSED METAL INCLUDING, BUT NOT LIMITED TO, TYPICAL FLASHING, DOWNSPOUTS, GUTTERS, DRIP EDGE, VENT STACKS, FLUE PIPES, ETC, SHALL BE PAINTED TO BLEND UNABTRUSIVELY WITH ADJACENT SURFACES.



BUILDING TYPE AND OCCUPANCY DATA

JURISDICTION:	TOWN OF FRISCO	
ZONE DISTRICT:	CENTRAL CORE	
ALLOWABLE DENSITY:	16 DU/ACRE	
MAXIMUM LOT COVERAGE:	NONE	
FRONT YARD SETBACK:	3'	(PROVIDED= 3.62')
SIDE YARD SETBACK:	0'	(PROVIDED: 0.5' ± 1.0')
REAR YARD SETBACK:	0'	(PROVIDED: 0.83')
MAXIMUM BUILDING HEIGHT:	40' (PITCHED)	(PROPOSED: 39.0')
	35' (FLAT)	(PROPOSED: 34.9')

OCCUPANCY CLASSIFICATION		
GROUP A-2	RESTAURANT SEATING	2,334 SF
	RESTAURANT KITCHEN	1,221 SF
GROUP R-2	RESIDENTIAL UNITS	13,399 SF
GROUP S-2	PARKING GARAGE	7,542 SF

CONSTRUCTION TYPE:
TYPE VB CONSTRUCTION
AUTOMATIC FIRE SPRINKLERS - NFPA 13

<u>OCCUPANT LOADS</u>		
FIRST LEVEL RESTAURANT SEATING:	(15 SF / OCC)	154 OCCUPANTS
RESTAURANT KITCHEN:	(200 SF / OCC)	7 OCCUPANTS
SECOND LEVEL UNITS:	(200 SF / OCC)	67 OCCUPANTS
THIRD LEVEL UNITS:	(200 SF / OCC)	65 OCCUPANTS

NOTES:

1. ALL WORK TO CONFORM TO CURRENT APPLICABLE CODES UNDER THE JURISDICTION OF THIS WORK INCLUDING:
 - 2018 INTERNATIONAL BUILDING CODE (IBC)
 - 2018 INTERNATIONAL PLUMBING CODE (IPC)
 - 2018 INTERNATIONAL MECHANICAL CODE (IMC)
 - 2018 INTERNATIONAL FLUE GAS CODE (IFGC)
 - 2021 INTERNATIONAL ENERGY CONSERVATION CODE (IECC)
 - 2017 NATIONAL ELECTRICAL CODE (NEC)
 - 2017 IGCANS A17.1 ACCESSIBILITY STANDARD
2. BUILDING IS TO BE EQUIPPED WITH AN AUTOMATIC SPRINKLER SYSTEM THROUGHOUT INSTALLED PER CHAPTER 9 OF THE 2018 IBC.
3. CHAPTER 7: FIRE RESISTANT RATED CONSTRUCTION
 - A. SECTION 708 CORRIDOR WALLS SHALL BE ONE-HOUR RATED PER WALL TYPE #C. SHEET A4.2.
 - B. SECTION 709 WALLS SEPARATING DWELLING UNITS IN THE SAME BUILDING SHALL BE ONE-HOUR RATED PER WALL TYPE #C. SHEET A4.2.
 - C. SECTION 711 FLOOR-CEILING ASSEMBLIES SEPARATING DWELLING UNITS AND SEPARATING DWELLING UNITS FROM OTHER OCCUPANCIES IN THE SAME BUILDING SHALL BE ONE-HOUR RATED PER FLOOR-CEILING ASSEMBLY #12. SHEET A4.2.
 - D. SECTION 717.5.3 SHAFT ENCLOSURES THAT ARE PERMITTED TO BE PENETRATED BY DUCTS AND AIR TRANSFER OPENINGS SHALL BE PROTECTED WITH LISTED FIRE AND SMOKE DAMPERS - SEE MECHANICAL DRAWINGS.
 - E. SECTION 719.2.4 STAIRWELL FIRE RATING SHALL BE PROVIDED IN CONCEALED SPACES BETWEEN STRINGERS AND AT THE TOP AND THE BOTTOM OF RUN.

SITE CALCULATIONS

TOTAL SITE AREA:	13,887	SF	(0.32 ACRES)
PROPOSED FOOTPRINT WITH DECKS & OVERHANGS:	11,841	SF	(85.3% OF TOTAL SITE AREA)
PROPOSED TOTAL DRIVES & SIDEWALKS	775	SF	(5.6% OF TOTAL SITE AREA)
PROPOSED SNOW STORAGE AREA FOR SITE	510	SF	(65% OF DRIVE & SIDEWALK AREA)
<u>TOTAL LOT COVERAGE:</u>	<u>12,616</u>	<u>SF</u>	<u>(90.1% OF TOTAL SITE AREA)</u>

PROPOSED UNCOVERED DECK AREA:	2,049	5F	
PROPOSED DEDICATED DECK SNOW STORAGE:	1,203	5F	(59% OF TOTAL DECK AREA)

BUILDING AREA CALCULATIONS

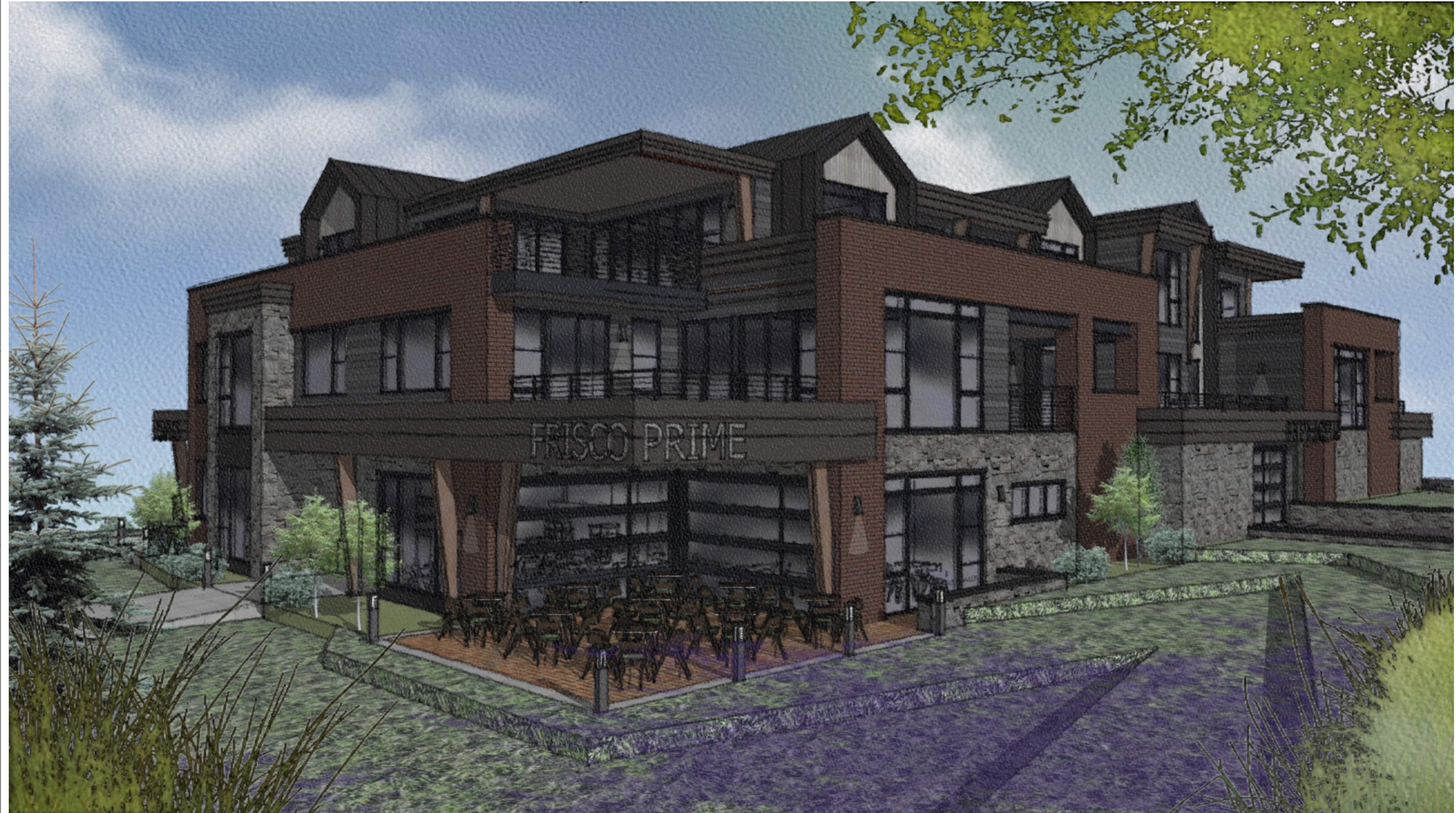
UNIT #	BR	BA	OFFICE	SF
SECOND LEVEL				
201	3	3	1	2,080
202	3	3		1,900
203	3	3		2,036
204	2	2		785
205	2	2		785
THIRD LEVEL				
301	3	3	1	2,177
302	3	3	1	2,066
303	2	2		785
304	2	2		785
TOTAL UNIT RESIDENTIAL:	23			13,399 SF
COMMON RESIDENTIAL				
			HALLWAYS:	1,250 SF
			STORAGE CLOSETS:	646 SF
			MECHANICAL:	284 SF
			PARKING GARAGE:	7,542 SF
			(W/ STAIR LOBBY & TRASH/REC)	
TOTAL COMMON:				9,722 SF
RESTAURANT				
			DINING:	1,328 SF
			BAR:	1,006 SF
			KITCHEN:	964 SF
			BATHROOMS:	257 SF
TOTAL RESTAURANT:				3,555 SF
TOTAL BUILDING AREA:				26,676 SF

PARKING CALCULATIONS

PARKING	
BEDROOMS:	23
VISITOR:	1
(1 PER 5 UNITS)	
TOTAL REQUIRED:	24
TOTAL STALLS PROVIDED:	24

PRIME SAUCE

LOTS 9-12 . BLOCK 2 . KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET . TOWN OF FRISCO . COLORADO



PROJECT DIRECTORY

OWNER 20EMAIN LLC PO BOX 2577 FRISCO . COLORADO . 80443 T: 970.406.1669	GENERAL CONTRACTOR MOUNTAIN BUILDING SOLUTIONS PO BOX 2577 FRISCO . COLORADO 80443 T: 970.406.1669
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<p>ARCHITECT</p> <p>ALLEN-GUERRA ARCHITECTURE</p> <p>711 B GRANITE ST</p> <p>PO BOX 5540</p> <p>FRISCO , COLORADO . 80443</p> <p>T: 970.453.7002</p>	<p>CIVIL ENGINEER</p> <p>TEN MILE ENGINEERING INC.</p> <p>PO BOX 1785</p> <p>FRISCO , COLORADO . 80443</p> <p>T: 970.465.5773</p>
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<p>STRUCTURAL ENGINEER</p> <p>G3 CONSULTING LLC</p> <p>PO BOX 2933</p> <p>BRECKENRIDGE . COLORADO . 80424</p> <p>T: 970.485.2073</p>	<p>MEP ENGINEER</p> <p>TIMBERLAKE ENGINEERING LLC</p> <p>35 W. MAIN ST. #5657</p> <p>FRISCO . COLORADO . 80443</p> <p>T: 573.881.5684</p>
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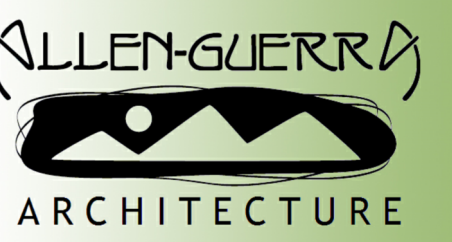
SURVEYOR
RANGE WEST ENGINEERS & SURVEYORS, INC
PO BOX 589
SILVERTHORNE . COLORADO . 80498
T: 970.468.6281

LOCATION MAP



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ALLEN-GUERRA ARCHITECTURE
711 D GRANITE STREET
PO BOX 5540
FRISCO, COLORADO 80443
PH: 970.453.7002 FAX: 970.453.7040
E-MAIL: INFO@ALLEN-GUERRA.COM
WEBSITE: WWW.ALLEN-GUERRA.COM

PRIME SAUCE
LOT 9, 10, 11, 12 BLOCK 2, KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET, TOWN OF FRISCO, COLORADO

TITLE: COVER SHEET

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PROJECT #: 2233





PRIME SAUCE
LOT 9, 10, 11, 12 BLOCK 2, KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET, TOWN OF FRISCO, COLORADO

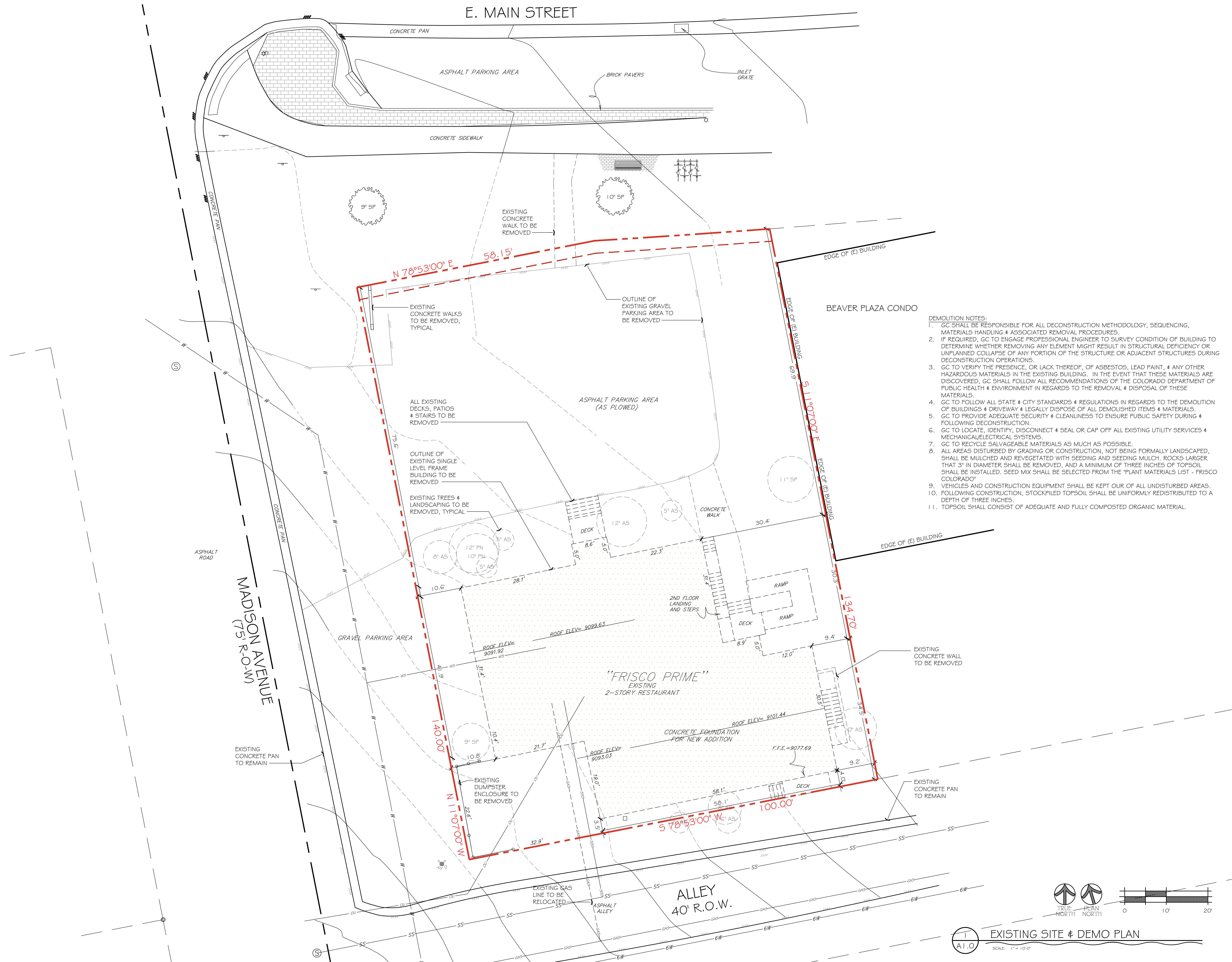
EXISTING SITE & DEMO PLAN

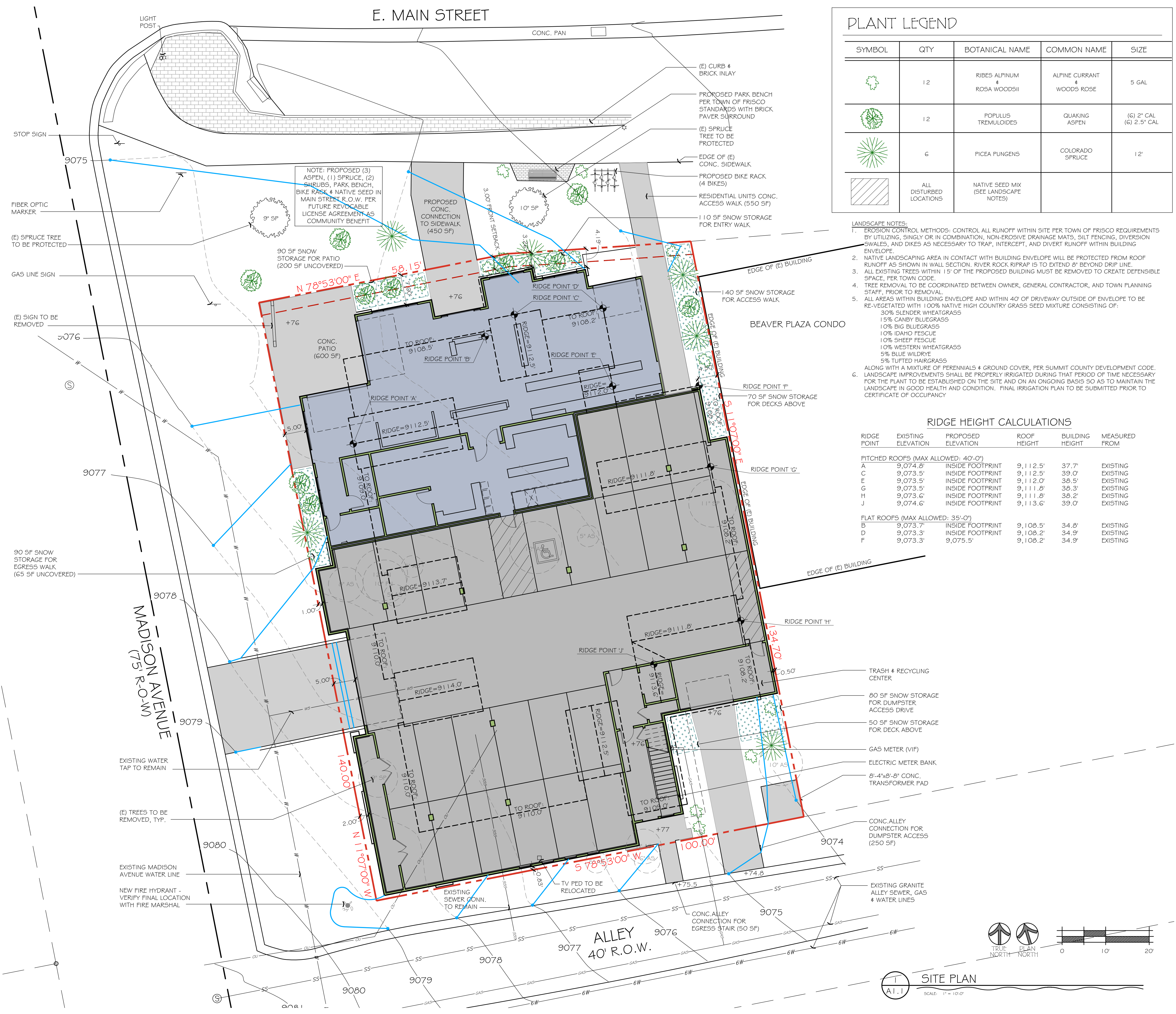
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PLANT LEGEND				
SYMBOL	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	12	RIBES ALPINUM + ROSA WOODSII	ALPINE CURRANT + WOODS ROSE	5 GAL
	12	POPULUS TREMULOIDES	QUAKING ASPEN	(G) 2" CAL (G) 2.5" CAL
	6	PICEA PUNGENS	COLORADO SPRUCE	12'
	ALL DISTURBED LOCATIONS	NATIVE SEED MIX (SEE LANDSCAPE NOTES)		

LANDSCAPE NOTES:
1. EROSION CONTROL METHODS: CONTROL ALL RUNOFF WITHIN SITE PER TOWN OF FRISCO REQUIREMENTS BY UTILIZING, SINGLY OR IN COMBINATION, NON-EROSIVE DRAINAGE MATS, SILT FENCING, DIVERSION SWALES, AND DIKES AS NECESSARY TO TRAP, INTERCEPT, AND DIVERT RUNOFF WITHIN BUILDING ENVELOPE.
2. NATIVE LANDSCAPING AREA IN CONTACT WITH BUILDING ENVELOPE WILL BE PROTECTED FROM ROOF RUNOFF AS SHOWN IN WALL SECTION. RIVER ROCK RIPRAP IS TO EXTEND 8' BEYOND DRIP LINE.
3. ALL EXISTING TREES WITHIN 15' OF THE PROPOSED BUILDING MUST BE REMOVED TO CREATE DEFENSIBLE SPACE, PER TOWN CODE.
4. TREE REMOVAL TO BE COORDINATED BETWEEN OWNER, GENERAL CONTRACTOR, AND TOWN PLANNING STAFF, PRIOR TO REMOVAL.
5. ALL AREAS WITHIN BUILDING ENVELOPE AND WITHIN 40' OF DRIVEWAY OUTSIDE OF ENVELOPE TO BE RE-VEGETATED WITH 100% NATIVE HIGH COUNTRY GRASS SEED MIXTURE CONSISTING OF:
30% SLENDER WHEATGRASS
15% CANBY BLUEGRASS
10% BIG BLUEGRASS
10% IDAHO FESCUE
10% SHEEP FESCUE
10% WESTERN WHEATGRASS
5% BLUE WILDRYE
5% TUFTED HAIRGRASS
ALONG WITH A MIXTURE OF PERENNIALS + GROUND COVER, PER SUMMIT COUNTY DEVELOPMENT CODE.
6. LANDSCAPE IMPROVEMENTS SHALL BE PROPERLY IRRIGATED DURING THAT PERIOD OF TIME NECESSARY FOR THE PLANT TO BE ESTABLISHED ON THE SITE AND ON AN ONGOING BASIS SO AS TO MAINTAIN THE LANDSCAPE IN GOOD HEALTH AND CONDITION. FINAL IRRIGATION PLAN TO BE SUBMITTED PRIOR TO CERTIFICATE OF OCCUPANCY

RIDGE HEIGHT CALCULATIONS					
RIDGE POINT	EXISTING ELEVATION	PROPOSED ELEVATION	ROOF HEIGHT	BUILDING HEIGHT	MEASURED FROM
PITCHED ROOFS (MAX ALLOWED: 40'-0")					
A	9,074.8'	INSIDE FOOTPRINT	9,112.5'	37.7'	EXISTING
C	9,073.5'	INSIDE FOOTPRINT	9,112.5'	39.0'	EXISTING
E	9,073.5'	INSIDE FOOTPRINT	9,112.0'	38.5'	EXISTING
G	9,073.5'	INSIDE FOOTPRINT	9,111.8'	38.3'	EXISTING
H	9,073.6'	INSIDE FOOTPRINT	9,111.8'	38.2'	EXISTING
J	9,074.6'	INSIDE FOOTPRINT	9,113.6'	39.0'	EXISTING
FLAT ROOFS (MAX ALLOWED: 35'-0")					
B	9,073.7'	INSIDE FOOTPRINT	9,108.5'	34.8'	EXISTING
D	9,073.3'	INSIDE FOOTPRINT	9,108.2'	34.9'	EXISTING
F	9,073.3'	9,075.5'	9,108.2'	34.9'	EXISTING

ALLEN-GUERRA ARCHITECTURE

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FRISCO, COLORADO 80443
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PRIME SAUCE
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SITE PLAN

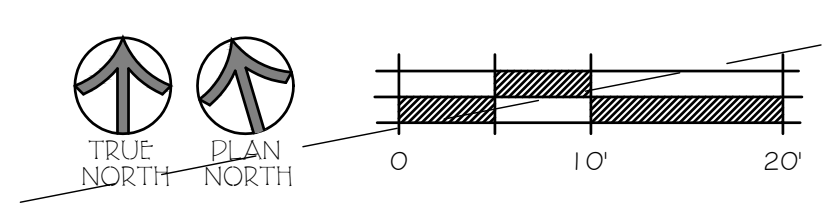
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ALL



SITE PLAN
SCALE: 1" = 10'-0"

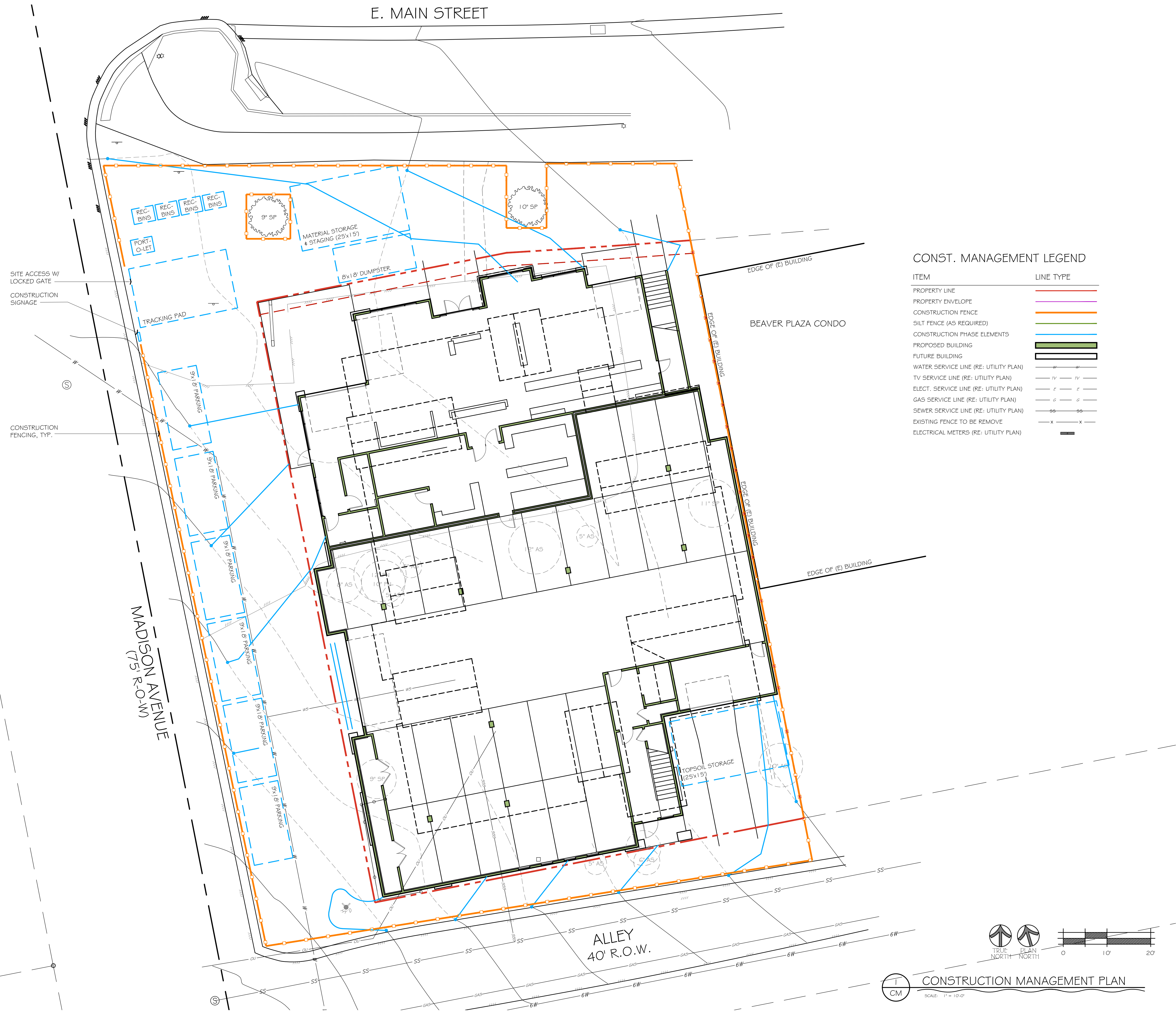


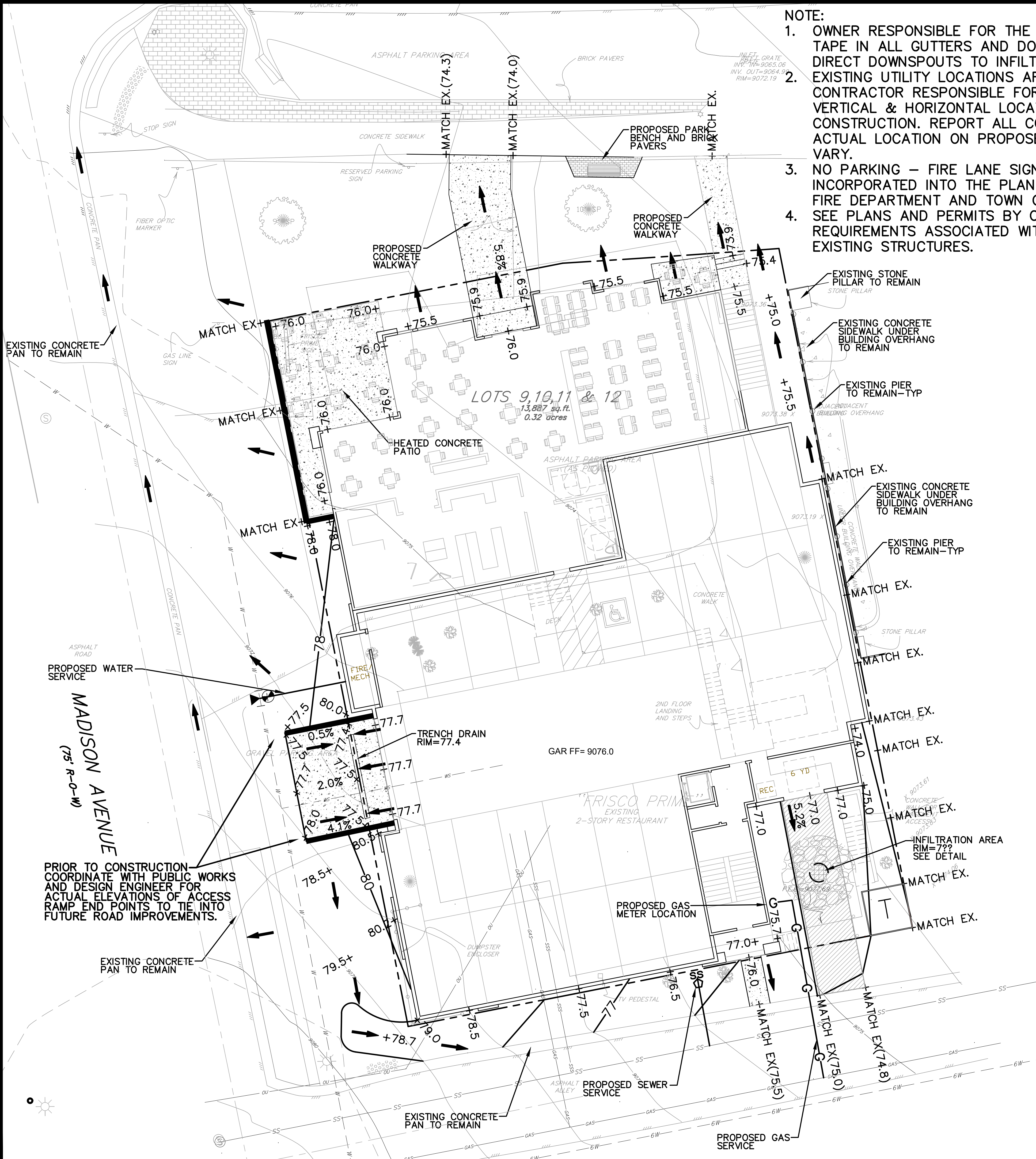
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CM



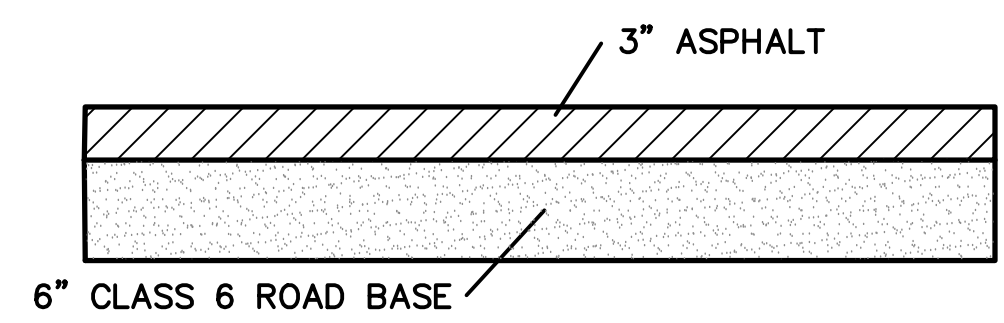
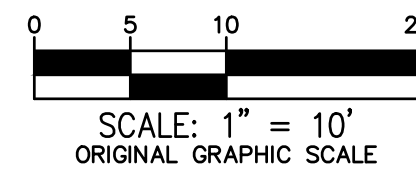


- NOTE:
1. OWNER RESPONSIBLE FOR THE INSTALLATION OF HEAT TAPE IN ALL GUTTERS AND DOWNSPOUTS AS NECESSARY. DIRECT DOWNSPOUTS TO INFILTRATION AREA.
 2. EXISTING UTILITY LOCATIONS ARE APPROXIMATE. CONTRACTOR RESPONSIBLE FOR DETERMINING ACTUAL VERTICAL & HORIZONTAL LOCATIONS PRIOR TO START OF CONSTRUCTION. REPORT ALL CONFLICTS TO ENGINEER. ACTUAL LOCATION ON PROPOSED IMPROVEMENTS MAY VARY.
 3. NO PARKING – FIRE LANE SIGNS SHALL BE INCORPORATED INTO THE PLANS AS DIRECTED BY THE FIRE DEPARTMENT AND TOWN OF FRISCO.
 4. SEE PLANS AND PERMITS BY OTHERS FOR ALL REQUIREMENTS ASSOCIATED WITH THE DEMOLITION OF EXISTING STRUCTURES.

GENERAL AND UTILITY NOTES:

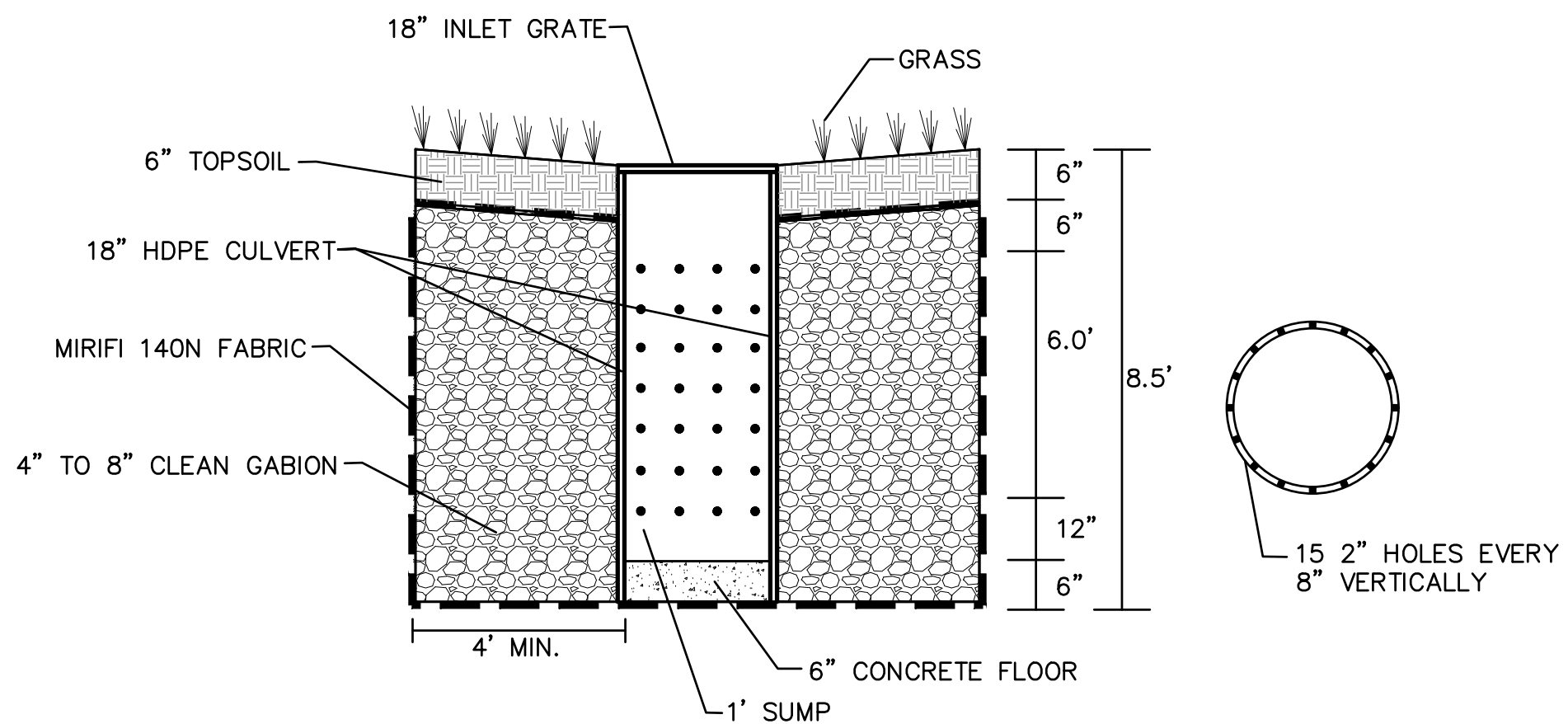
- 1) ALL COLLECTION SYSTEM WORK SHALL CONFORM TO THE FRISCO SANITATION DISTRICT "DESIGN STANDARDS AND SPECIFICATIONS FOR SEWER CONSTRUCTION".
- 2) EXISTING SEWER MAIN ELEVATIONS MUST BE FIELD VERIFIED PRIOR TO CONSTRUCTION.
- 3) ALL DOMESTIC WATER SERVICE INSTALLATIONS SHALL CONFORM TO THE TOWN OF FRISCO WATER CONSTRUCTION STANDARDS. CONTACT JEFF GOBLE 970 668 0836 WITH QUESTIONS.
- 4) SEE LANDSCAPE PLAN FOR INFORMATION ON IRRIGATION SYSTEM DESIGN.
- 5) SEE MECHANICAL AND FIRE PROTECTION PLANS FOR INFORMATION ON WATER METER, BACKFLOW ASSEMBLY LOCATION AND SIZE REQUIREMENTS.
- 6) SEE SITE PLAN FOR INFORMATION ON SNOW STORAGE.
- 7) ALL WATER FROM ROOF DRAINS AND GUTTERS SHALL BE PIPED TO THE INFILTRATION GALLERY. SEE ARCHITECTURAL PLANS FOR DETAILS AND PIPE LOCATIONS.
- 8) LANDOWNER/CONTRACTOR TO COORDINATE THE RELOCATION OF EXISTING ELECTRIC, GAS, CATV AND PHONE LINES WITH UTILITY COMPANIES.
- 9) ALL ROAD AND CONCRETE CUTS SHALL BE BROUGHT BACK TO CURRENT TOWN STANDARDS.
- 10) ALL ROOF DRAINAGE SHALL BE CAPTURED IN ROOF DRAIN AND/OR GUTTERS. NO DIRECT DISCHARGE ALLOWED ON TO TOWN ROW. SEE ARCHITECTURAL PLANS FOR DETAILS.
- 11) ALL WATER INSPECTIONS REQUIRE 24 HOUR NOTICE.
- 12) CONTACT TOWN OF FRISCO PUBLIC WORKS TO DETERMINE IF ADDITIONAL TAP FEES ARE REQUIRED.
- 13) ALL WATER LINE INSTALLATION AND CONNECTIONS MUST COMPLY WITH TOWN OF FRISCO CONSTRUCTION STANDARDS IN EFFECT AT TIME OF BUILDING PERMIT ISSUANCE.
- 14) SEE MECHANICAL PLANS FOR DETAILS OF WATERLINE CONNECTION INTO BUILDING, METER AND BACKFLOW PREVENTION PIPING AND REMOTE METER READOUT LOCATION. REQUIRED BEFORE BUILDING PERMIT IS ISSUED.
- 15) SEE MECHANICAL PLANS FOR DETAILS OF SUMP PUMP AND ASSOCIATED PIPING. ALL SUMP PUMP CONNECTIONS TO STORM DRAINAGE SYSTEM MUST BE DOWN STREAM OF PERFORATED MANHOLE AND INFILTRATION AREA.
- 16) ALL CONSTRUCTION STAGING AND MANAGEMENT MUST COMPLY WITH IBC CHAPTER 33 – SAFEGUARDS DURING CONSTRUCTION
- 17) ALL WATER SERVICE MAINTENANCE AND REPAIR IS THE RESPONSIBILITY OF THE HOA/HOMEOWNERS.

CALL UTILITY NOTIFICATION
CENTER OF COLORADO
811
CALL 2 BUSINESS DAYS IN ADVANCE
BEFORE YOU DIG, GRADE OR EXCAVATE
FOR THE MARKING OF UNDERGROUND
MEMBER UTILITIES.



PAVEMENT SECTION SHALL BE
3" ASPHALT ON 6" OF ROAD BASE
SEE GEOTECHNICAL RECOMMENDATIONS
FOR COMPACTION REQUIREMENTS
SECTION TO BE FIELD VERIFIED BY
GEOTECHNICAL ENGINEER

TYPICAL PAVEMENT SECTION
NO SCALE



ALL INFILTRATION GALLERIES TO BE MAINTAINED BY PROPERTY OWNER. SEDIMENT SHALL BE REMOVED FROM SUMP VIA VACUUM TRUCK ONCE SEDIMENT HAS FILLED SUMP 2/3 FULL.

TYPICAL INFILTRATION AREA DETAIL

NTS



No.	Revision/Issue	Date	Description
3	REVISED	5/15/25	REVISED GARAGE RAMP PER TOF INPUT
2	FINAL PLAN SUBMITTAL	4/25/25	TOF FINAL PLAN SUBMITTAL
1	SKETCH PLAN SUBMITTAL	9/7/24	TOF SKETCH PLAN SUBMITTAL

TEN MILE
ENGINEERING, INC.
Professional Civil Engineers
P.O. Box 1785
Frisco, CO 80443
970.485.5773
Joe@tenmileengineering.com

20 EAST MAIN STREET
LOTS 9-12, BLOCK 19
KING SOLOMON 2ND ADDITION
TOWN OF FRISCO, SUMMIT COUNTY, COLORADO
FINAL PLAN
GRADING, DRAINAGE & UTILITY PLAN

Project	20 EAST MAIN(PRIME)
Date	9/7/24
Scale	1"=10'
Sheet	2



PRIME SAUCE
LOT 9, 10, 11, 12 BLOCK 2, KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET, TOWN OF FRISCO, COLORADO

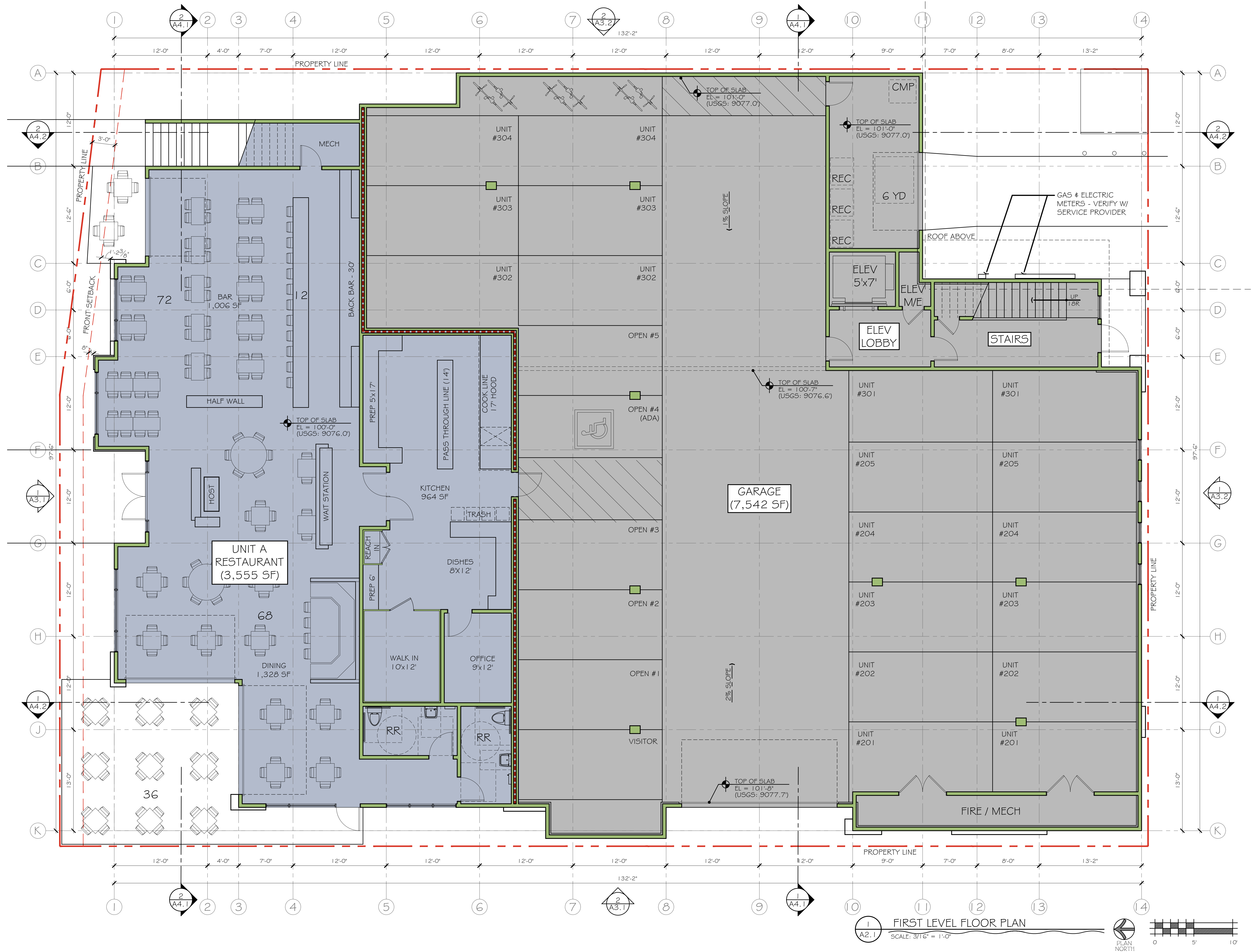
FIRST LEVEL FLOOR PLAN

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ISSUE:	DATE:
PRELIM	28 APR 2022
DESIGN	15 FEB 2024
DRC	2 APR 2024
UPDATE	8 MAY 2024
SKETCH	6 SEP 2024
REVIEW	6 DEC 2024
PLANNING	24 JAN 2025
UPDATE	24 APR 2025

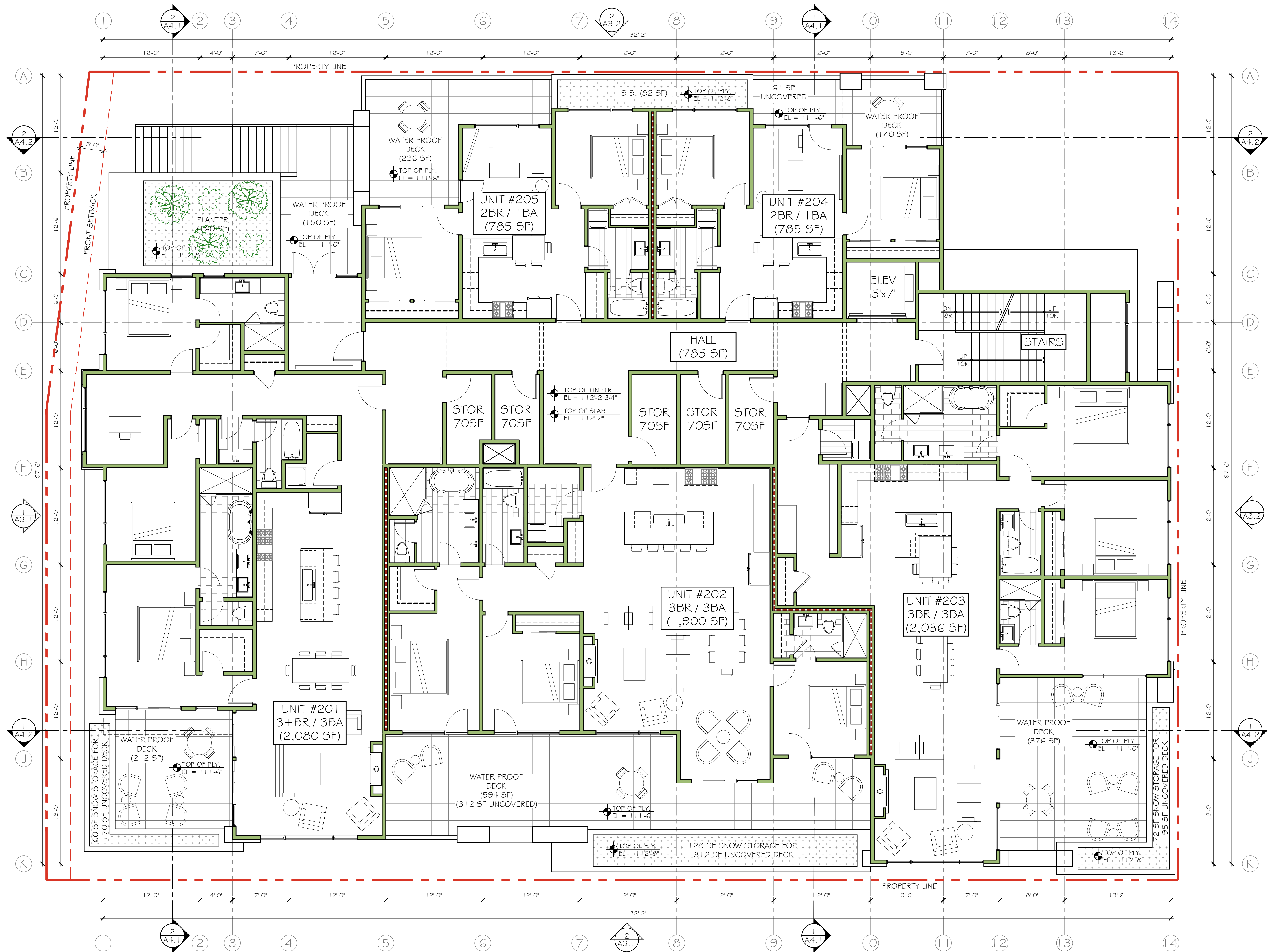
PROJECT #: 2233

A2.1



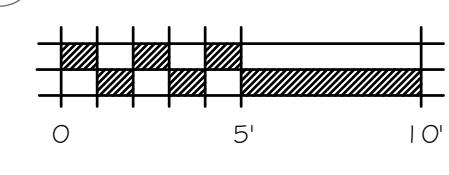
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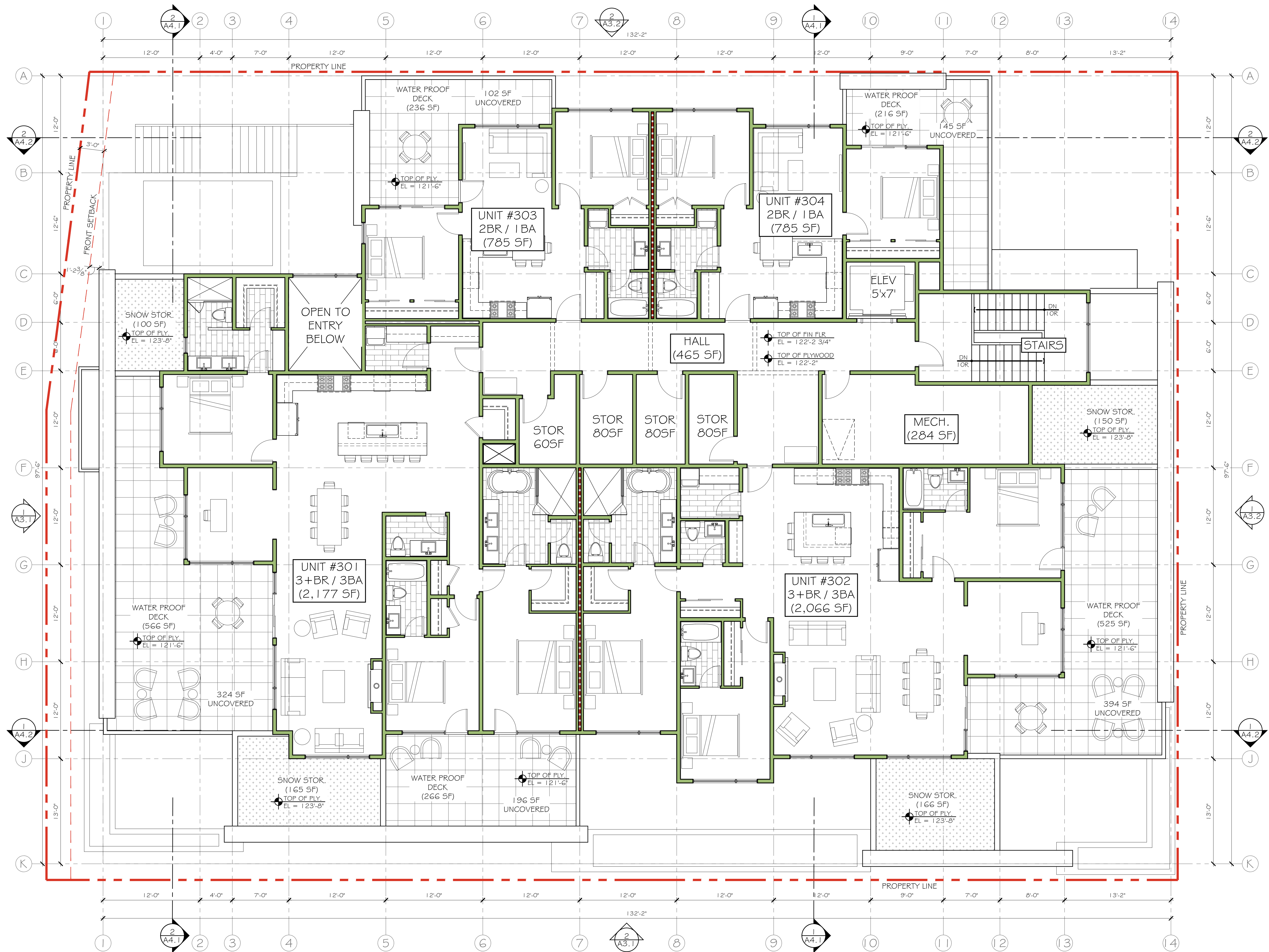
PROJECT #: 2233



SECOND LEVEL FLOOR PLAN

SCALE: 3/16" = 1'-0"





ISSUE:	DATE:
PRELIM	28 APR 2022
DESIGN	15 FEB 2024
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UPDATE	24 APR 2025

PRIME SAUCE
LOT 9, 10, 11, 12 BLOCK 2, KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET, TOWN OF FRISCO, COLORADO

ROOF PLAN

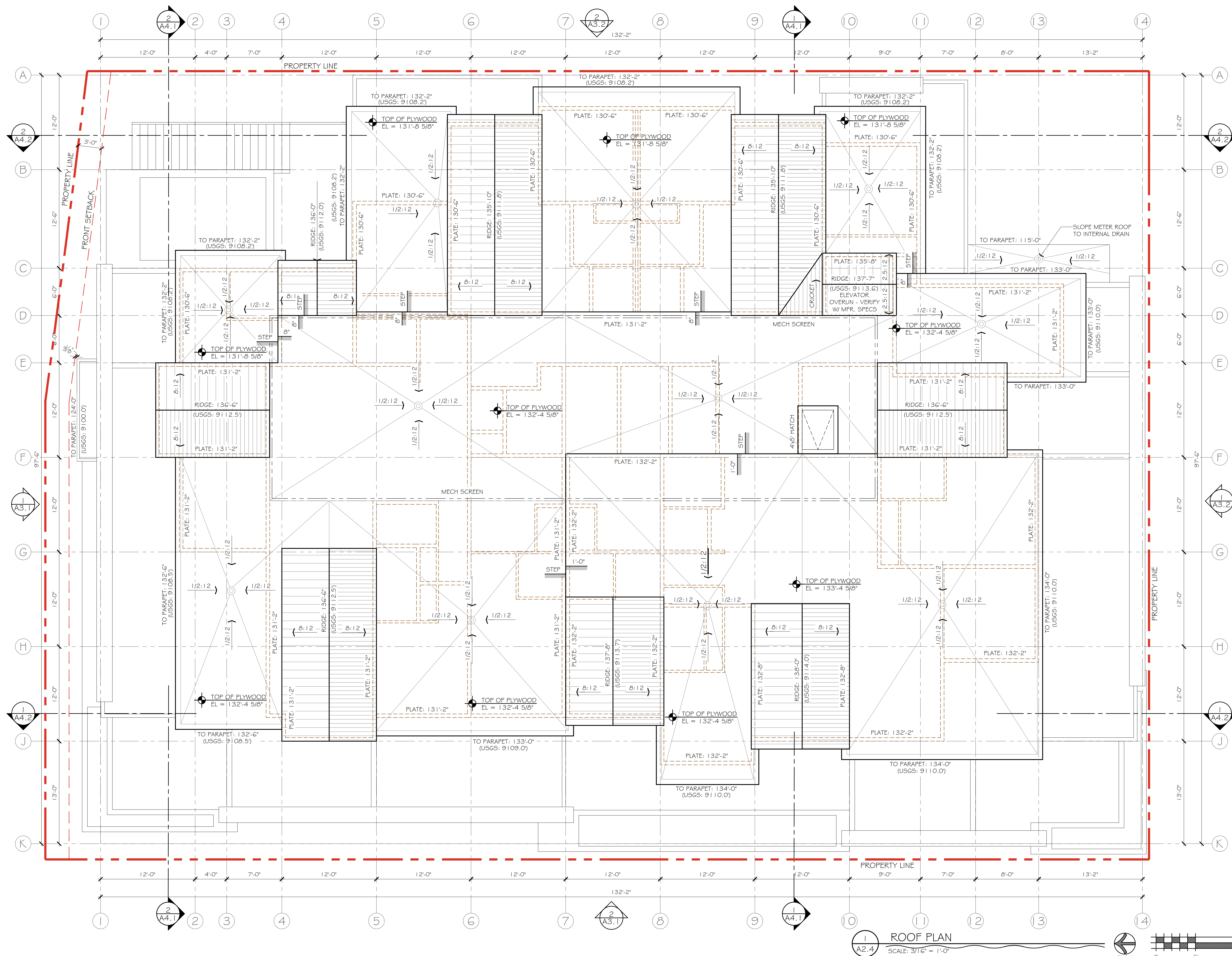
TITLE

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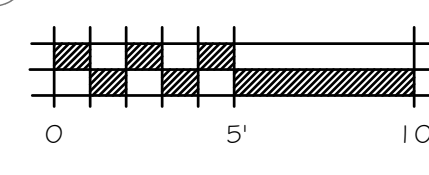
ISSUE:	DATE:
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DESIGN	15 FEB 2024
DRC	2 APR 2024
UPDATE	8 MAY 2024
SKETCH	6 SEP 2024
REVIEW	6 DEC 2024
PLANNING	24 JAN 2025
UPDATE	24 APR 2025

PROJECT #: 2233

A2.4



1
A2.4
ROOF PLAN
SCALE: 3/16" = 1'-0"





1 NORTH ELEVATION
3/16" = 1'-0"

TOTAL AREA: 3,330 SF
BRICK AREA: 810 SF (24.3%)
METAL AREA: 640 SF (19.2%)

TOTAL BUILDING
TOTAL AREA: 11,210 SF
BRICK AREA: 2,598 SF (23.2%)
METAL AREA: 2,550 SF (22.7%)



2 WEST ELEVATION
3/16" = 1'-0"

TOTAL AREA: 2,300 SF
BRICK AREA: 446 SF (19.4%)
METAL AREA: 562 SF (24.4%)

PRIME SAUCE
LOTS 9, 10, 11 & 12, BLOCK 2, KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET, TOWN OF FRISCO, COLORADO

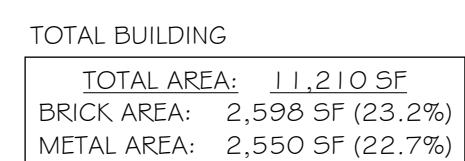
EXTERIOR ELEVATIONS

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UPDATE	24 APR 2025

PROJECT# 2233

A3.1



ISSUE	DATE
PRELIM	2 MAY 2022
DRC	2 APR 2024
UPDATE	9 MAY 2024
SKETCH	6 SEP 2024
REVIEW	6 DEC 2024
PLANNING	24 JAN 2025
UPDATE	24 APR 2025

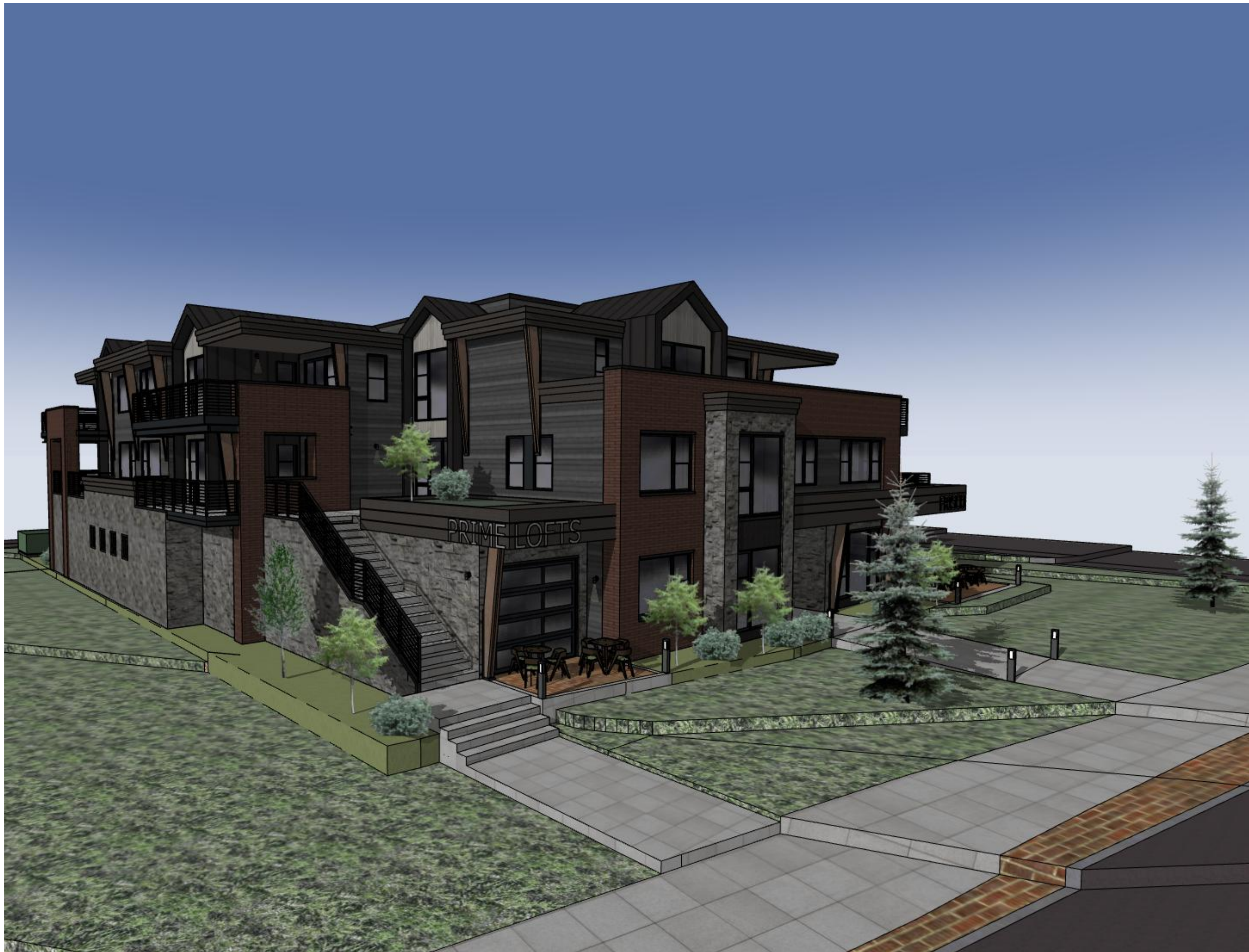
PROJECT# 2233

PRIME SAUCE
 LOTS 9, 10, 11 & 12, BLOCK 2, KING SOLOMON SUBDIVISION #2
 20 EAST MAIN STREET, TOWN OF FRISCO, COLORADO

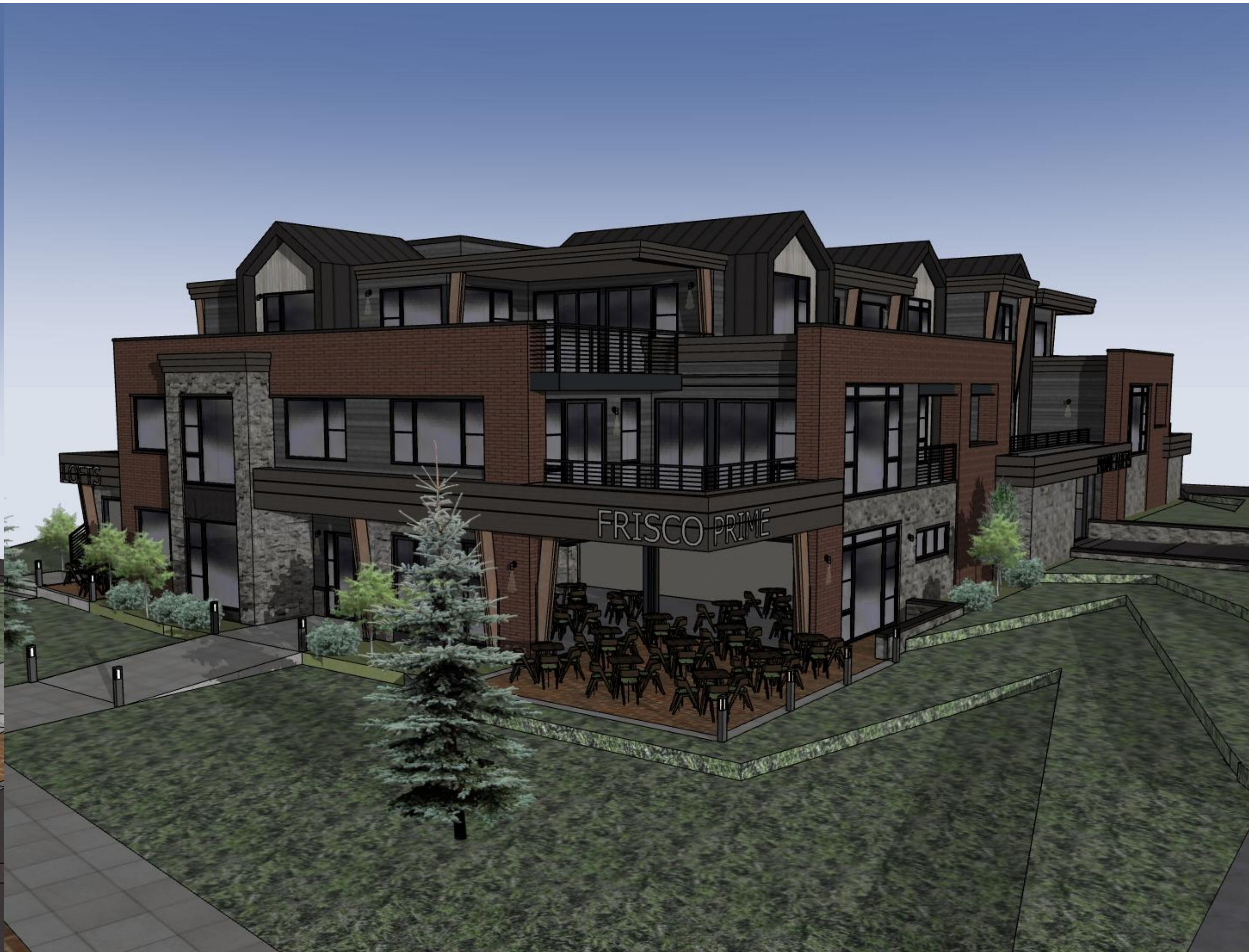
EXTERIOR ELEVATIONS

CONVINCED THAT A S. 401 PERMIT IS NECESSARY FOR THE PROPOSED CONSTRUCTION OF A NEW

A3.2



2
A3.3
NORTHEAST PERSPECTIVE
N.T.S.



1
A3.3
NORTHWEST PERSPECTIVE
N.T.S.

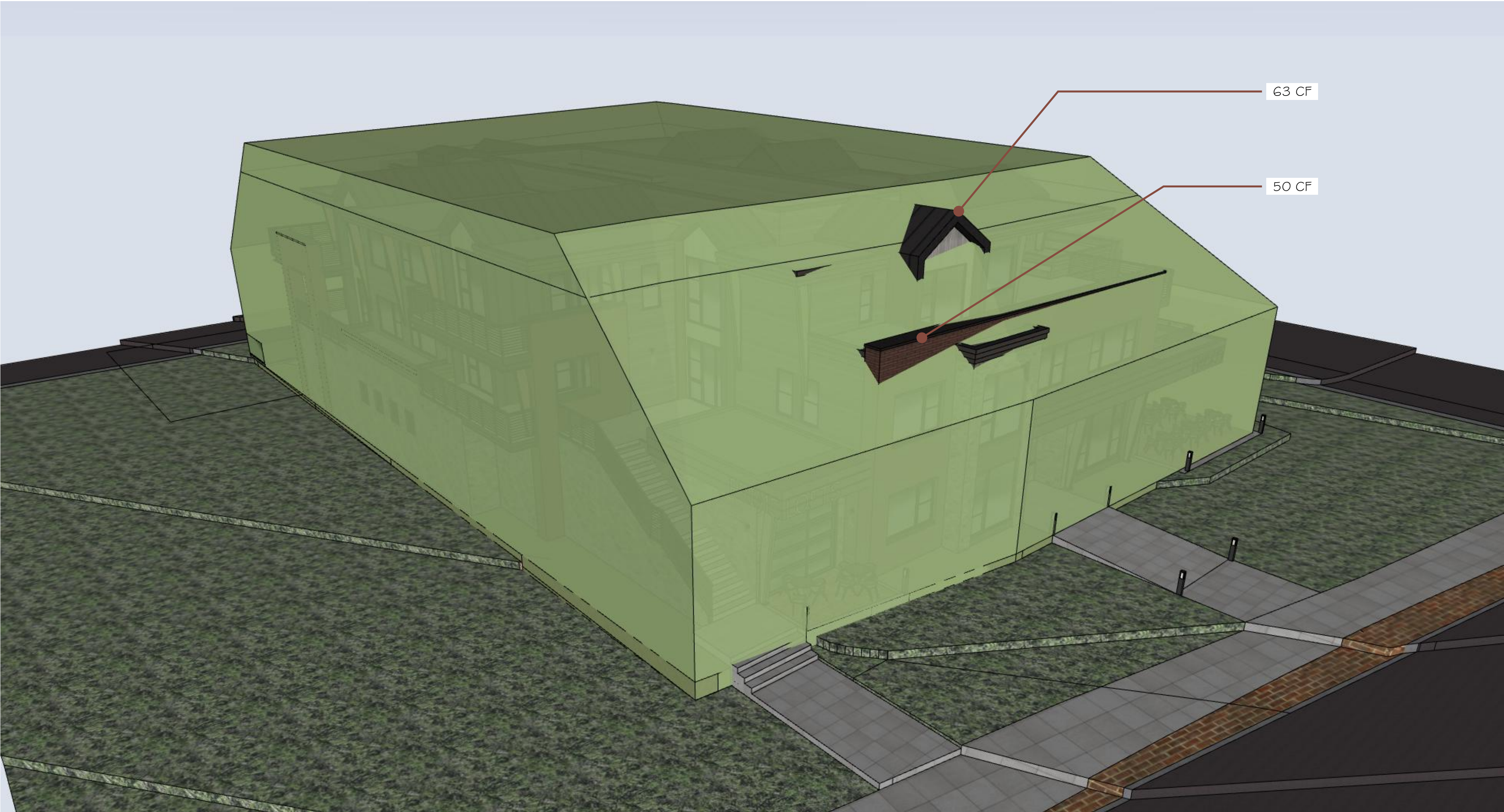


4
A3.3
SOUTHWEST PERSPECTIVE
N.T.S.



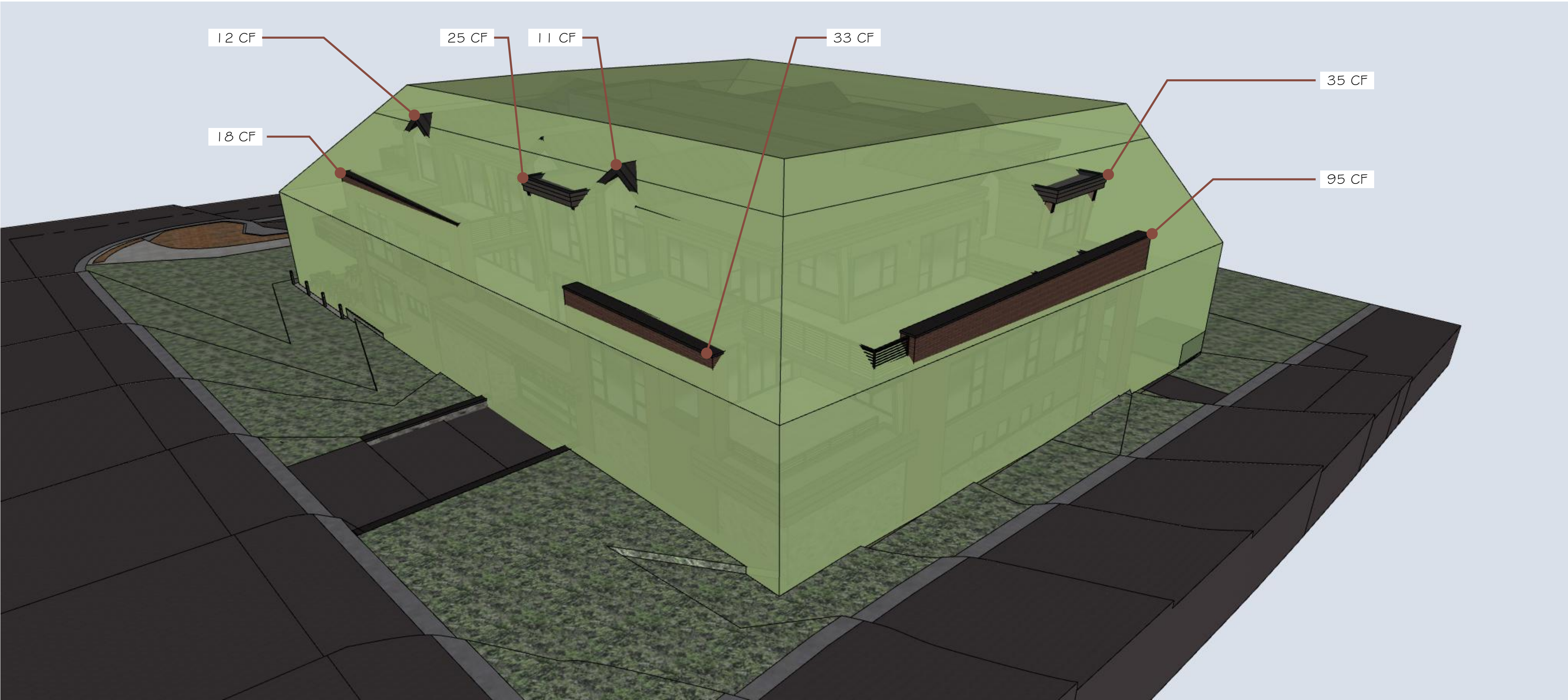
3
A3.3
SOUTHEAST PERSPECTIVE
N.T.S.

ISSUE	DATE
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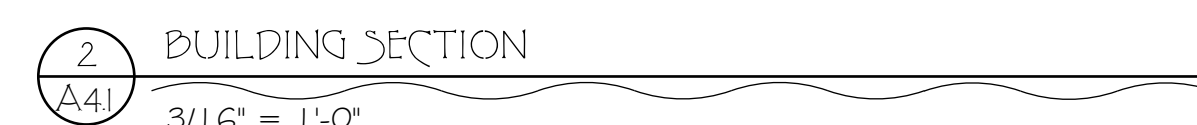
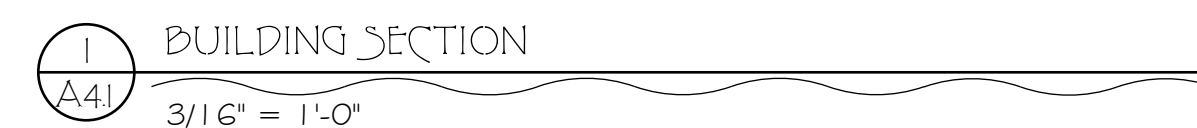
1 BULKPLANE FROM NORTHEAST
A3.4 N.T.S.

NOTE: 342 CF TOTAL BULK PLANE
ENCROACHMENT



2 BULKPLANE FROM SOUTHWEST
A3.4 N.T.S.

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PROJECT# 2233	



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20 EAST MAIN STREET, TOWN OF FRISCO, COLORADO

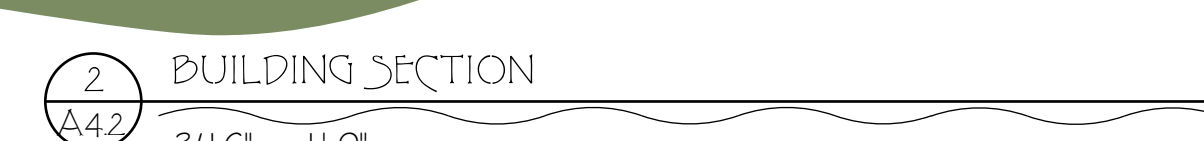
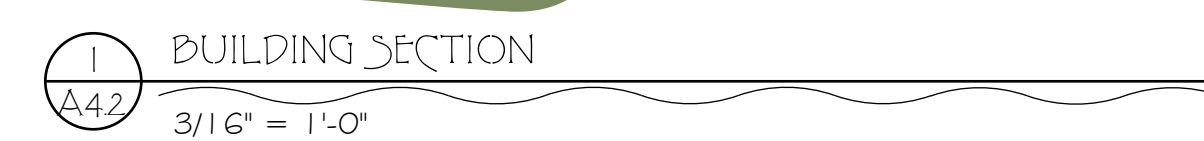
BUILDING SECTIONS

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PROJECT# 2233

A4.1



PRIME SAUCE
LOTS 9, 10, 11 & 12. BLOCK 2. KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET. TOWN OF FRISCO. COLORADO

BUILDING SECTIONS

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PROJECT# 2233

A4.2




LUMINAIRE SCHEDULE									
SYMBOL	QTY	LABEL	[MANUFAC]	CATALOG	WATTS	LUMENS	BUG	LLF	MTG HT
	9	B	McGRAW-EDISON	BRT6-A1-727-U-T5-42-BZ (DARK BRONZE)	9.4	960	B1-U0-G0	0.900	3.5
	1	D	HALO	SMD4R6927WH	9.5	690	B0-U0-G0	0.900	8
	21	W	LUMIERE	9002-W1-RW-LED2797-W-BZ-L1-UNV-RSM (DARK BRONZE)	10	685	B1-U0-G0	0.900	8, 9

CALCULATION STATISTICS		
REFLECTANCES	CALCULATION SPACING	CALCULATION HEIGHTS
DIRECT METHOD ONLY	OVERALL: 10'-0" x 10'-0" / DECK & PATIO: 5'-0" x 5'-0"	SITE: 0'-0" A.F.G. / PROPERTY LINE: 0'-0" A.F.G.

CALCULATION SUMMARY						
LABEL	UNITS	AVG	MAX	MIN	AVG/MIN	MAX/MIN
_OVERALL SITE	Fc	0.17	22.3	0.0	N.A.	N.A.
_PROPERTY LINE	Fc	0.01	0.3	0.0	N.A.	N.A.
DECK & STAIRS	Fc	3.67	9.8	0.7	5.24	14.00
PATIO	Fc	4.76	8.2	1.0	4.76	8.20

FIXTURE 'B'



McGraw-Edison
BRT6 Bollard
Round LED Pedestrian Luminaire

Product Certifications

- Interactive Menu
 - Ordering Information page 2
 - Product Specifications page 3
 - Optical Distributions page 2
 - Energy and Performance Data page 1

FIXTURE 'D'




HALO
SMD4 Series
4" Round and Square

Typical Applications
Residential

- Ultra-low profile surface luminaire with wide distribution
- Ceiling or wall mounting in compatible junction boxes
- Retrofit 4" recessed downlights with screwbase adapter included
- 2700K, 3000K, 3500K, 4000K or 5000K color temperature; 90 CRI
- Dimmable with 120V dimmer

FIXTURE 'W'

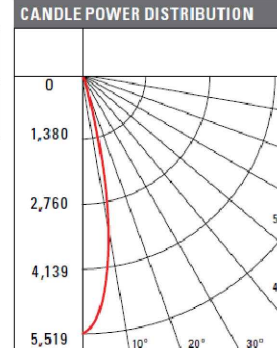


Lanterra 9002
LED
INTERIOR / EXTERIOR
CYLINDER FLOOD LIGHT
CERTIFICATION DATA
cULus - 1998
Wet Location Listed - IP66
LM79/LM80 Compliant
ROHS* Compliant
10W LED, L70/90,000 @ 25° Celsius

Color Metric Summary

Tm 30 91	Re = 92.5
Lumens 8002 (W1) X FL LED4080	Rg = 92.8
M - 90 CRI - 90	Rb = 92.2
CR/CE	Re = 92.1

Candle Power Distribution



Cone of Light

Beam Angle	Horizontal Footcandle	Vertical Footcandle
2°	1379.9	0.4
4°	344.5	1.4
6°	153.2	2.2
8°	88.2	3.3
10°	54.2	5.0
15°	24.5	9.8
20°	12.8	15.8
30°	6.1	28.8
40°	3.4	45.8

Candela Table

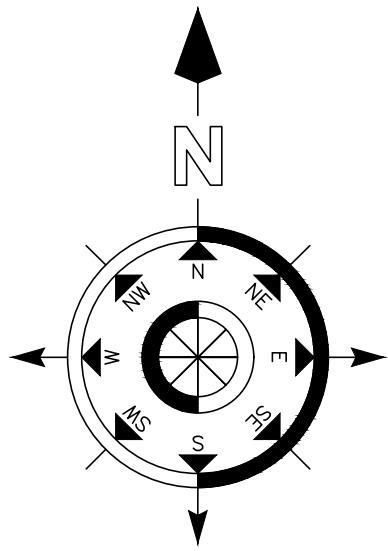
Angle	Candela
0°	3339
5°	4009
10°	3391
15°	1202
20°	457
30°	133
40°	22
50°	4
60°	2
70°	1
80°	0.4
90°	0

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REVISIONS:	DATE:
REVISED SITE BKGRD, ELIM. EAST 'W' FIXTURE	4/23/2025

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APRIL 23, 2025
Date
Mark Timberlake, P.E.
PE-0049319



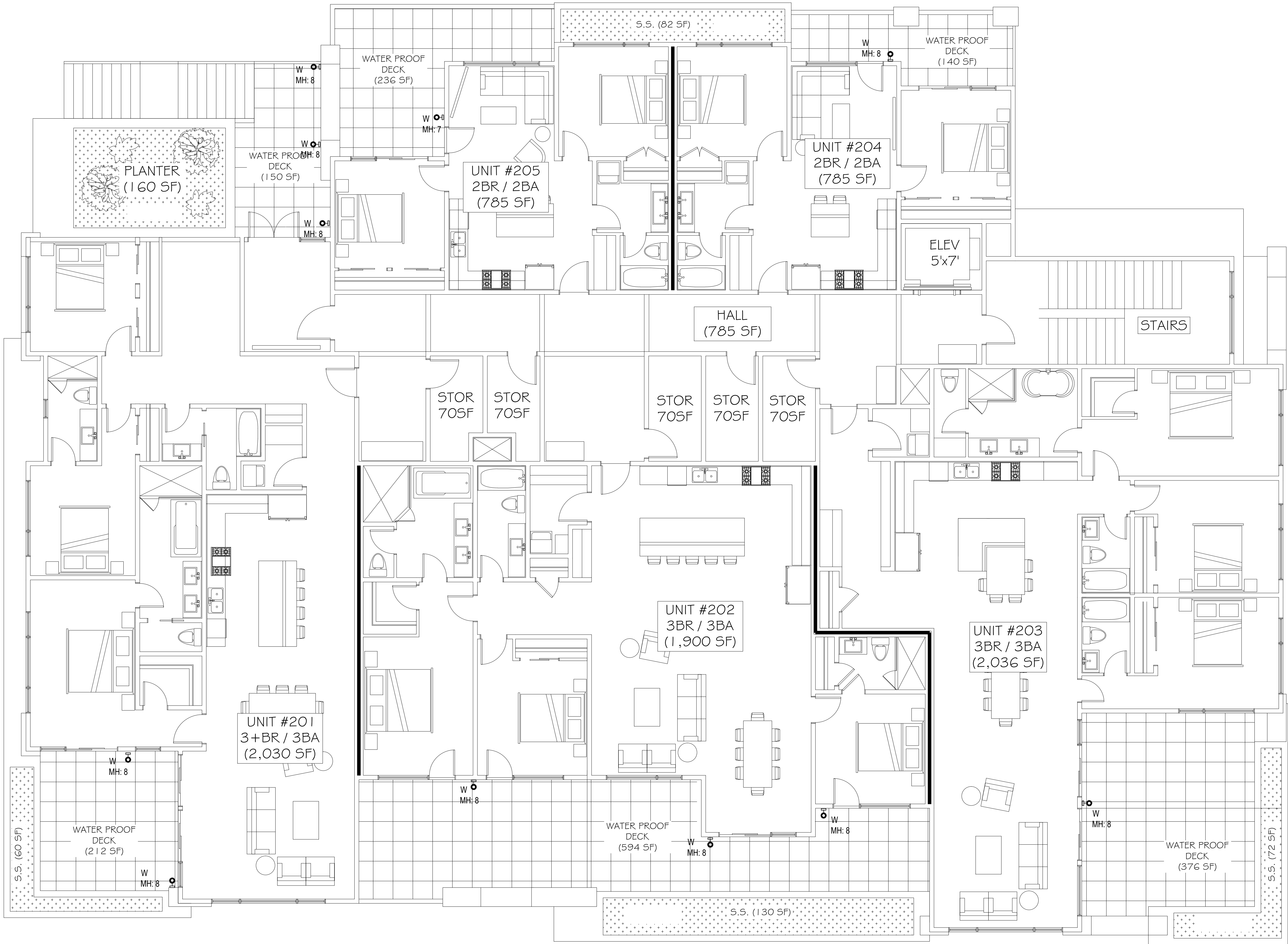
TIMBERLAKE ENGINEERING, L.L.C.
Mark B. Timberlake P.E.
35 W MAIN ST. #5657
FRISCO, CO 80443
(573) 881-5684
mark@tecengineering.com

SITE LIGHTING PLAN

PRIME SAUCE

20 EAST MAIN STREET		FRISCO, CO
DATE:	FEB. 27, 2025	SHEET NO.
DRAWN BY:	MBT	
PROJECT NO:	2408	
SCALE:	1"=10'	

MEP2.0



KEY NOTES

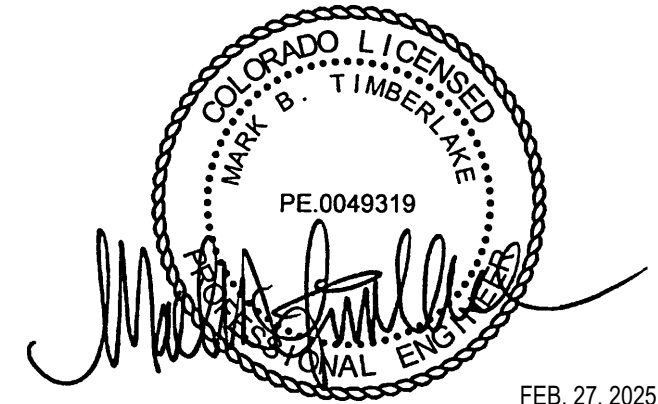
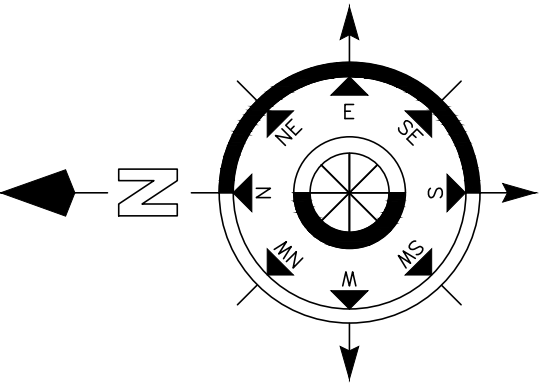
- ALL MOUNTING HEIGHTS (MH:) ARE MEASURED ABOVE THE ADJACENT DECK LEVEL
- SEE MEP2.1 FOR FIXTURE LEGEND

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REVISIONS:	DATE:

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Mark Timberlake, P.E.
PE-0049319

FEB. 27, 2025
Date

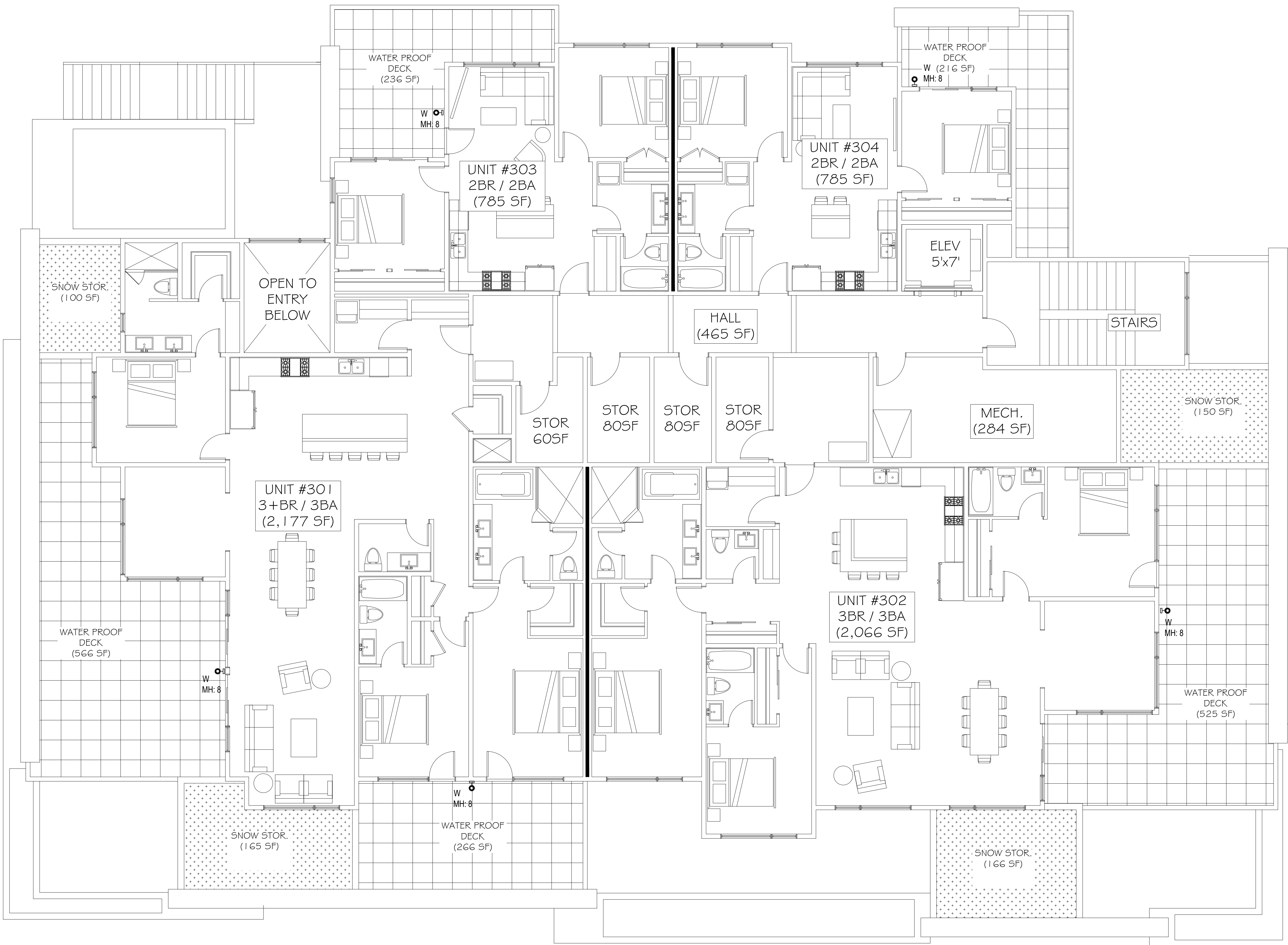
TIMBERLAKE ENGINEERING, L.L.C.
Mark B. Timberlake P.E.
35 W. MAIN ST. #5657
FRISCO, CO 80443
(573) 881-5684
mark@tecoengineering.com

BUILDING LIGHTING PLAN - 2ND FL.

PRIME SAUCE

20 EAST MAIN STREET		FRISCO, CO
DATE:	FEB. 27, 2025	SHEET NO.
DRAWN BY:	MBT	
PROJECT NO:	2408	
SCALE:	3/16" = 1'-0"	

MEP2.2



KEY NOTES

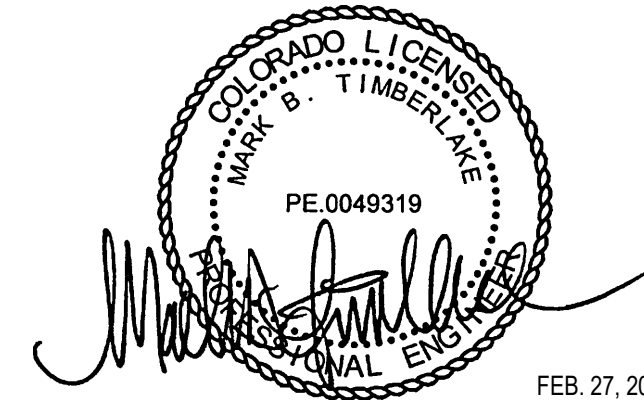
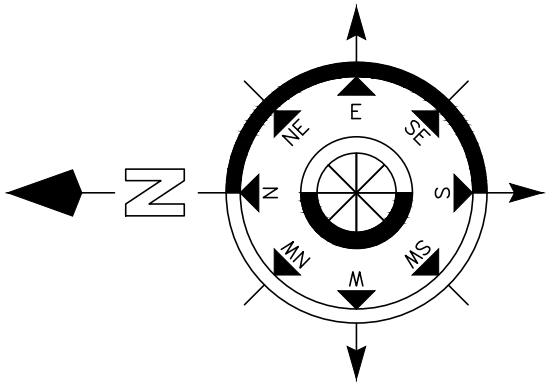
- ALL MOUNTING HEIGHTS (MH) ARE MEASURED ABOVE THE ADJACENT DECK LEVEL
- SEE MEP2.1 FOR FIXTURE LEGEND

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Mark Timberlake, P.E.
PE-0049319

FEB. 27, 2025
Date



TIMBERLAKE
ENGINEERING, L.L.C.
Mark B. Timberlake P.E.
35 W. MAIN ST. #5657
FRISCO, CO 80443
(573) 881-5684
mark@tecoengineering.com

BUILDING LIGHTING PLAN - 3RD LEVEL

PRIME SAUCE

20 EAST MAIN STREET		FRISCO, CO.
DATE:	FEB. 27, 2025	SHEET NO.
DRAWN BY:	MBT	
PROJECT NO:	2408	
SCALE:	3/16" = 1'-0"	

MEP2.3



LSC TRANSPORTATION CONSULTANTS, INC.

1889 York Street
Denver, CO 80206
(303) 333-1105
FAX (303) 333-1107
E-mail: lscdenver@lsctrans.com

January 22, 2025

Mr. Stephen Lunney
Mountain Building Solutions
610 Main Street, #11
Frisco, CO 80443

Re: Prime Sauce
Frisco, CO
LSC #240670

Dear Mr. Lunney:

In response to your request, LSC Transportation Consultants, Inc. has prepared this traffic impact analysis for the proposed Prime Sauce development. As shown on Figure 1, the site is located south of W. Main Street and east of S. Madison Avenue in Frisco, Colorado.

REPORT CONTENTS

The report contains the following: the existing roadway and traffic conditions in the vicinity of the site including the lane geometries, traffic controls, posted speed limits, etc.; the existing weekday peak-hour traffic volumes; the typical weekday site-generated traffic volume projections for the site; the assignment of the projected traffic volumes to the area roadways; the projected background and resulting total traffic volumes on the area roadways; the site's projected traffic impacts; and any recommended roadway improvements to mitigate growth in background traffic or from the impact of the site.

LAND USE AND ACCESS

The site is proposed to include about 9 townhome dwelling units and a 3,324 square-foot restaurant. Access is proposed from S. Madison Avenue as shown in the conceptual site plan in Figure 2.

ROADWAY AND TRAFFIC CONDITIONS

Area Roadways

The major roadways in the site's vicinity are shown on Figure 1 and are described below.

- **W. Main Street** is an east-west, two-lane roadway north of the site. The intersection with S. Madison Street is all-way stop-sign controlled. The posted speed limit in the vicinity of the site is 25 mph west of S. Madison Street and 20 mph east of Madison Street.
- **S. Madison Avenue** is a north-south, two-lane roadway west of the site. The intersection with W. Main Street is all-way stop-sign controlled and the intersection with Granite Alley is two-way stop-sign controlled. The posted speed limit in the vicinity of the site is 20 mph.

Existing Traffic Conditions

Figure 3a shows the existing October traffic volumes, existing traffic control, and lane geometry in the site's vicinity on a typical weekday. The weekday peak-hour traffic volumes and daily traffic counts are from the attached traffic counts conducted by Counter Measures in October, 2024.

Figure 3b shows the estimated July, 2024 volumes, existing traffic control, and lane geometry in the site's vicinity on a typical weekday. A seasonal adjustment factor of 1.34 was applied to the October traffic counts conducted in October, 2024 based on CDOT continuous count locations on US 6 west of Swan Road and on SH 9 south of Tiger Road.

2026 and 2045 Background Traffic

Figure 4 shows the estimated 2026 background traffic and Figure 5 shows the estimated 2045 background traffic. The area is mostly built out but the background traffic estimates assume one percent annual growth to maintain a conservative analysis.

Existing, 2026, and 2045 Background Levels of Service

Level of service (LOS) is a quantitative measure of the level of congestion or delay at an intersection. Level of service is indicated on a scale from "A" to "F." LOS A is indicative of little congestion or delay and LOS F is indicative of a high level of congestion or delay. Attached are specific level of service definitions for unsignalized intersections.

The intersections in the study area were analyzed as appropriate to determine the existing, 2026, and 2045 background levels of service using Synchro Version 11. Table 1 shows the level of service analysis results. The level of service reports are attached.

- 1. Madison Avenue/Main Street:** This all-way stop controlled intersection currently operates at an overall LOS "B" during both morning and afternoon peak-hours and is expected to do so through 2026. The intersection is expected to operate at LOS "C" during both morning and afternoon peak-hours through 2045.
- 2. Madison Avenue/Peak School Driveway:** All movements at this stop-sign controlled intersection currently operate at LOS "B" or better and are expected to do so through 2045.
- 3. Madison Avenue/Site Access:** This intersection was analyzed only in the total traffic scenarios.
- 4. Madison Avenue/Granite Alley:** All movements at this stop-sign controlled intersection currently operate at LOS "A" during both morning and afternoon peak-hours and are expected to operate at LOS "B" or better through 2045.

TRIP GENERATION

Table 2 shows the estimated average weekday, morning peak-hour, and afternoon peak-hour trip generation for the proposed site for three separate scenarios based on the rates from *Trip Generation, 11th Edition, 2021* by the Institute of Transportation Engineers (ITE).

The site is projected to generate about 421 vehicle-trips on the average weekday, with about half entering and half exiting during a 24-hour period. During the morning peak-hour, which generally occurs for one hour between 6:30 and 8:30 a.m., about 18 vehicles would enter and about 17 vehicles would exit the site. During the afternoon peak-hour, which generally occurs for one hour between 4:00 and 6:00 p.m., about 21 vehicles would enter and about 14 vehicles would exit.

TRIP DISTRIBUTION

Figure 6 shows the estimated directional distribution of the site-generated traffic volumes on the area roadways. The estimates were based on the location of the site with respect to the regional population, employment, and activity centers; and the site's proposed land use.

TRIP ASSIGNMENT

Figure 7 shows the site-generated traffic volumes which are the directional distribution percentages (from Figure 6) applied to the trip generation estimate (from Table 2).

2026 AND 2045 TOTAL TRAFFIC

Figure 8 shows the 2026 total traffic which is the sum of the 2026 background traffic volumes (from Figure 4) and the site-generated traffic volumes (from Figure 7). Figure 8 also shows the 2026 total traffic lane geometry and traffic control.

Figure 9 shows the 2045 total traffic which is the sum of the 2045 background traffic volumes (from Figure 5) and the site-generated traffic volumes (from Figure 7). Figure 9 also shows the 2045 total traffic lane geometry and traffic control.

PROJECTED LEVELS OF SERVICE

The intersections in the study area were analyzed to determine the 2026 and 2045 total levels of service. Table 1 shows the level of service analysis results for each movement or lane group. The level of service reports are attached.

- 1. Madison Avenue/Main Street:** This all-way stop controlled intersection is expected to operate at an overall LOS "B" during both morning and afternoon peak-hours through 2026 and at LOS "C" through 2045.
- 2. Madison Avenue/Peak School Driveway:** All movements at this stop-sign controlled intersection are expected to operate at LOS "B" or better through 2045.
- 3. Madison Avenue/Site Access:** All movements at this stop-sign controlled intersection are expected to operate at LOS "A" through 2045.
- 4. Madison Avenue/Granite Alley:** All movements at this stop-sign controlled intersection are expected to operate at LOS "B" or better through 2045.

CONCLUSIONS AND RECOMMENDATIONS

Trip Generation

1. The site is projected to generate about 421 vehicle-trips on the average weekday, with about half entering and half exiting during a 24-hour period. During the morning peak-hour, about 18 vehicles would enter and about 17 vehicles would exit the site. During the afternoon peak-hour, about 21 vehicles would enter and about 14 vehicles would exit.

Projected Levels of Service

2. The all-way stop controlled Madison Avenue/Main Street intersection is expected to operate at an overall LOS "C" or better through 2045 with or without development of the site.
3. All movements at the unsignalized intersections analyzed are expected to operate at LOS "B" or better during both morning and afternoon peak-hours through 2045.

Conclusions

4. The impact of the Prime Sauce development can be accommodated by the existing roadway network.

Recommendations

5. The site access approach to Madison Avenue should be stop-sign controlled.

* * * * *

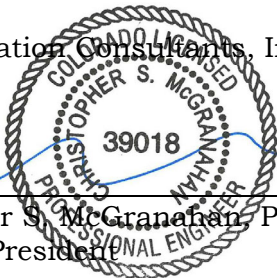
We trust our findings will assist you in gaining approval of the proposed Prime Sauce development. Please contact me if you have any questions or need further assistance.

Respectfully submitted,

LSC Transportation Consultants, Inc.

By:


Christopher S. McGranahan, P.E.
Principal/President



CSM/wc

1-22-25

Enclosures: Tables 1 and 2
Figures 1 - 9
Traffic Count Reports
Level of Service Definitions
Level of Service Reports

Table 1
Intersection Levels of Service Analysis
Prime Sauce
Frisco, CO
LSC #240670; January, 2025

Intersection No. and Location	Traffic Control	Existing Traffic		2026 Background Traffic		2026 Total Traffic		2045 Background Traffic		2045 Total Traffic	
		Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service
		AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
1) <u>Madison Avenue/Main Street</u>	AWSC										
NB Approach		A	B	A	B	A	B	B	B	B	C
EB Approach		B	B	B	B	B	C	C	C	C	D
WB Approach		A	B	A	B	A	B	B	C	B	C
SB Approach		A	B	A	B	A	B	A	B	A	B
Entire Intersection Delay (sec /veh)		11.6	12.8	11.9	13.1	12.2	13.8	15.8	19.4	17.2	21.2
Entire Intersection LOS		B	B	B	B	B	B	C	C	C	C
2) <u>Madison Avenue/Peak School Driveway</u>	TWSC										
NB Approach		A	A	A	A	A	A	A	A	A	A
EB Approach		B	B	B	B	B	B	B	B	B	B
WB Approach		B	B	--	--	--	--	--	--	--	--
SB Approach		A	A	A	A	A	A	A	A	A	A
Critical Movement Delay		11.3	10.9	11.3	10.9	11.7	11.3	12.1	11.6	12.6	12.1
3) <u>Madison Avenue/Site Access</u>	TWSC										
WB Approach		--	--	--	--	A	A	--	--	A	A
SB Approach		--	--	--	--	A	A	--	--	A	A
Critical Movement Delay		--	--	--	--	9.1	9.4	--	--	9.2	9.7
4) <u>Madison Avenue/Granite Alley</u>	TWSC										
WB Approach		A	A	A	A	A	A	A	B	A	B
SB Approach		A	A	A	A	A	A	A	A	A	A
Critical Movement Delay		9.4	9.9	9.4	9.9	9.4	9.9	9.8	10.4	9.8	10.4

Table 2
ESTIMATED TRAFFIC GENERATION
Prime Sauce
Frisco, CO
LSC #240670; January, 2025

Trip Generating Category	Quantity	Trip Generation Rates ⁽¹⁾				Total Trips Generated					
		Average	AM Peak-Hour		PM Peak-Hour		Average	AM Peak-Hour		PM Peak-Hour	
		Weekday	In	Out	In	Out	Weekday	In	Out	In	Out
Currently Proposed Land Use											
Townhomes ⁽²⁾	9 DU ⁽³⁾	7.20	0.120	0.360	0.336	0.234	65	1	3	3	2
Restaurant ⁽⁴⁾	3.324 KSF ⁽⁵⁾	107.20	5.264	4.306	5.521	3.529	356	17	14	18	12
Total =							421	18	17	21	14

Notes:

- (1) Source: *Trip Generation*, Institute of Transportation Engineers, 11th Edition, 2021
- (2) ITE Land Use No. 215 - Single Family Attached Housing
- (3) DU = Dwelling Unit
- (4) ITE Land Use No. 932 - High-Turnover Sit-Down Restaurant
- (5) KSF = 1,000 square feet



Figure 1

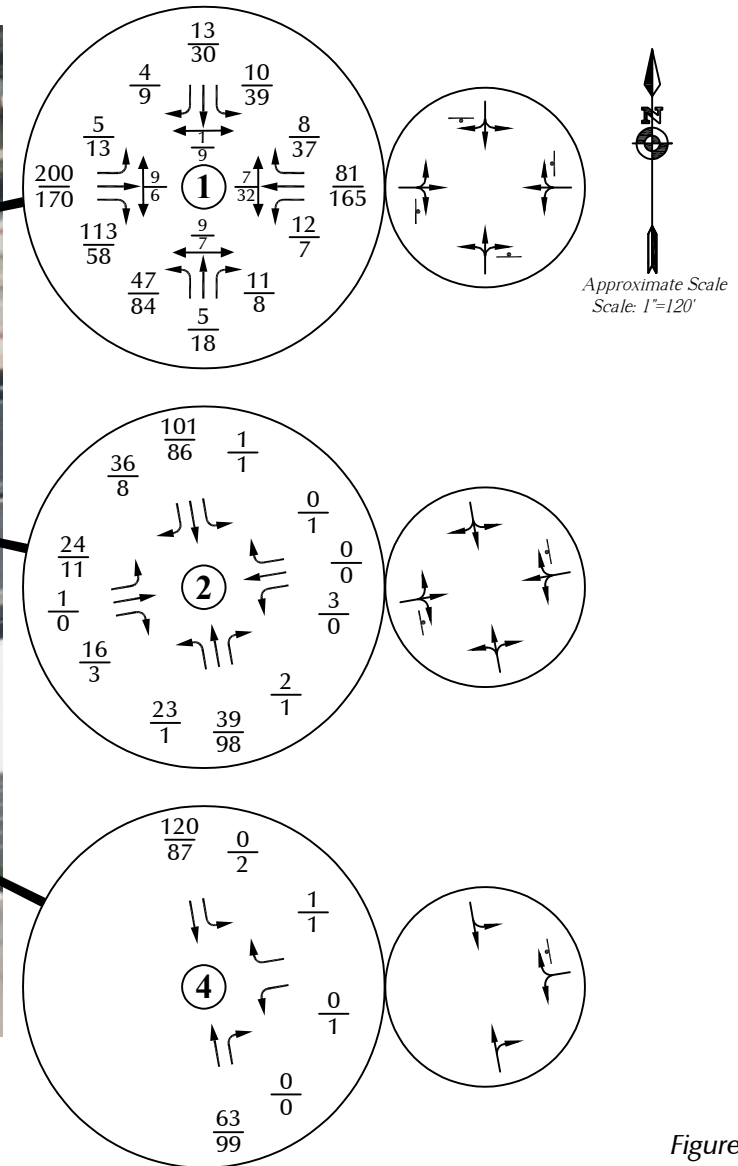
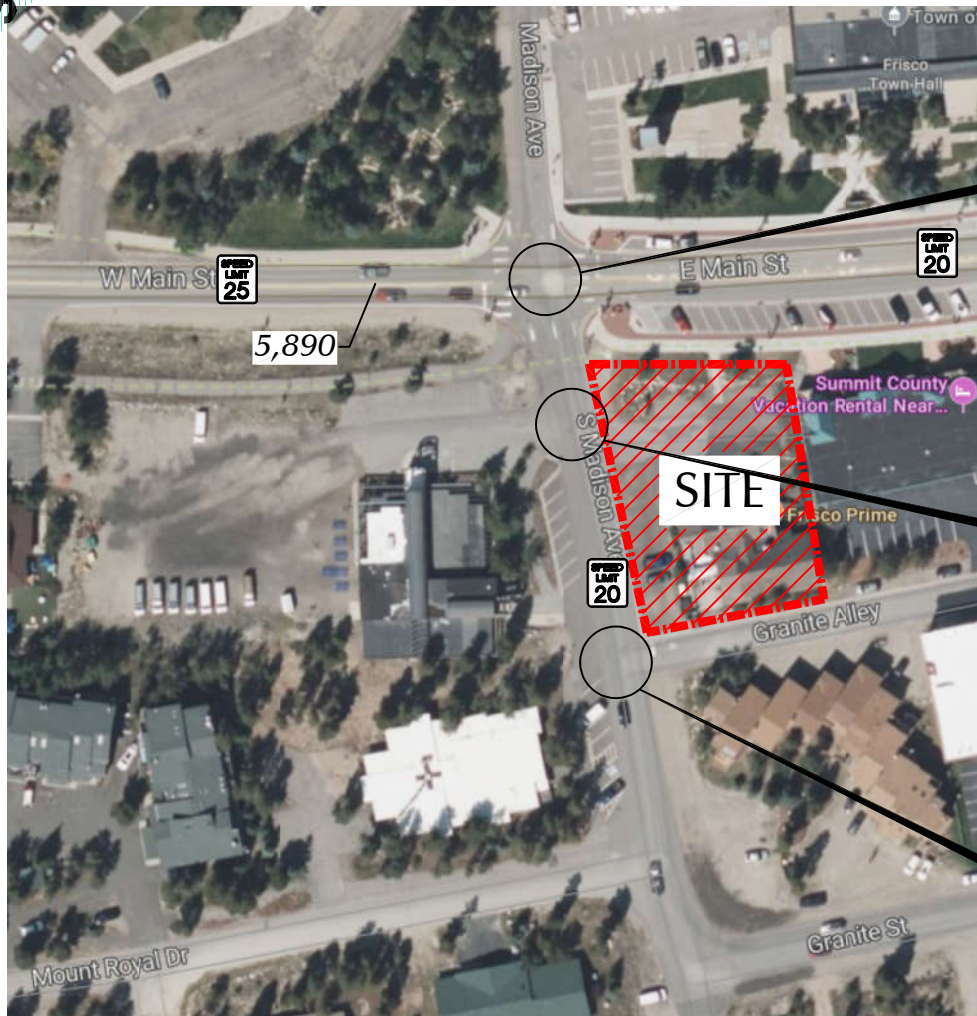
Vicinity Map

Prime Sauce (LSC #240670)



Figure 2

Prime Sauce (LSC #240670)





LEGEND:

-  = Stop Sign
-  = Speed Limit
- $\frac{26}{35}$ = $\frac{\text{AM Peak Hour Traffic}}{\text{PM Peak Hour Traffic}}$
- 1,000 = Average Daily Traffic

Seasonal Traffic Volumes in the Area
US 6 west of Swan Road
 Oct 2023 = 11,726vpd
 July 2023 = 16,241vpd
SH 9 south of Tiger Road
 Oct 2023 = 19,796vpd
 July 2023 = 26,085vpd
 Seasonal Adjustment Factor = 1.34

Existing October Traffic, Lane Geometry and Traffic Control

Prime Sauce (LSC #240670)

Figure 3a



A seasonal adjustment factor of 1.34 was applied to the October traffic counts conducted in October, 2024. Details are provided in Figure 3a.

LEGEND:

\downarrow = Stop Sign
 $\frac{26}{35}$ = $\frac{\text{AM Peak Hour Traffic}}{\text{PM Peak Hour Traffic}}$
 1,000 = Average Daily Traffic

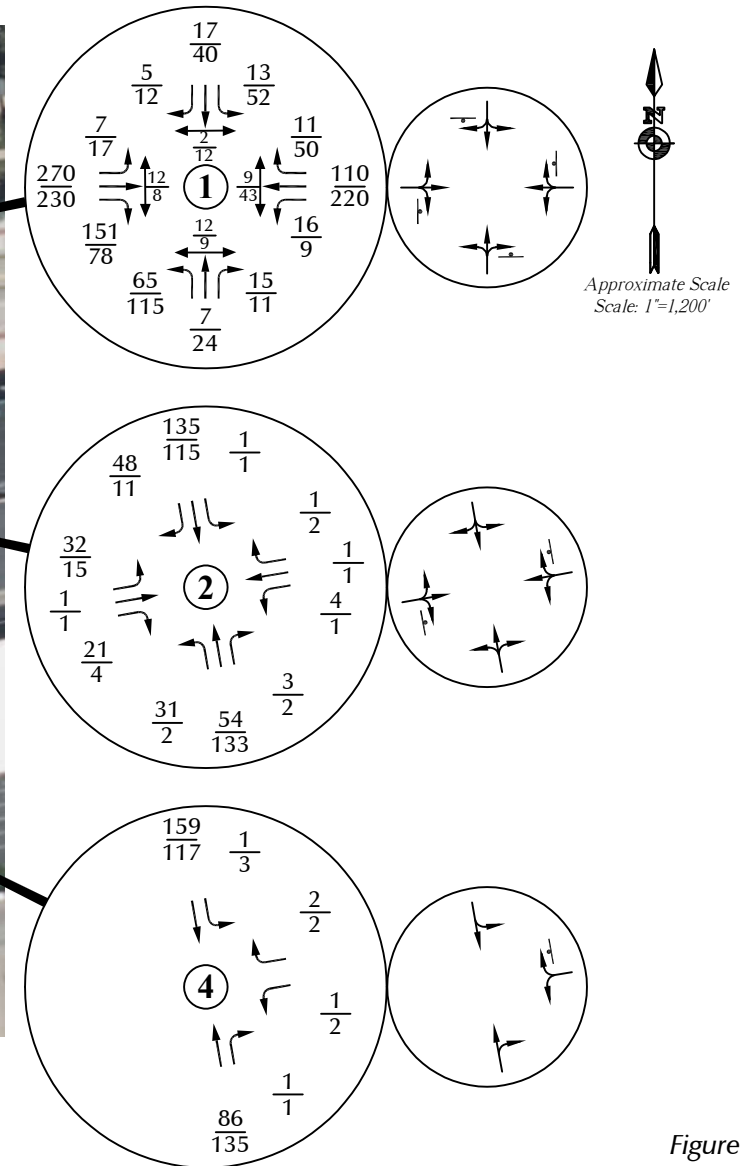
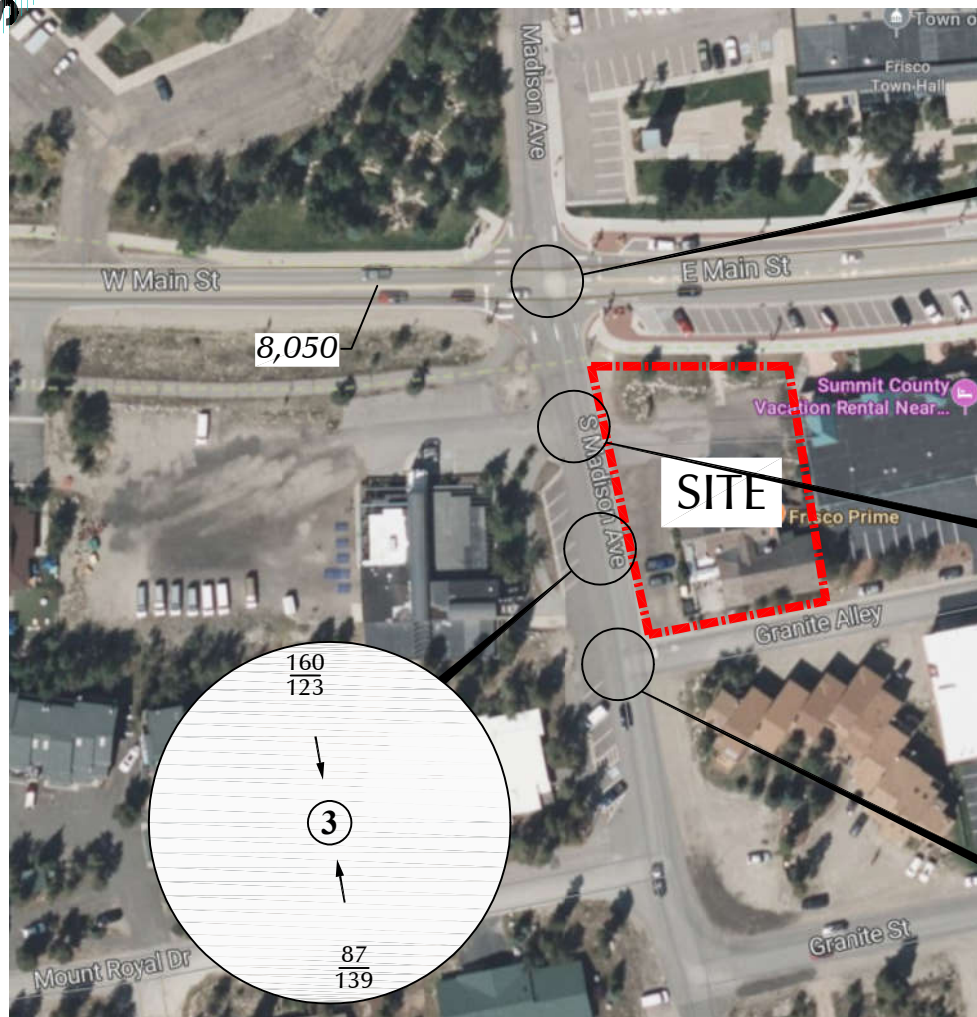


Figure 3b

*Estimated July, 2024 Traffic,
Lane Geometry and Traffic Control*

Prime Sauce (LSC #240670)



Note: Assumes one percent annual growth to maintain a conservative analysis.

LEGEND:

- \downarrow = Stop Sign
 $\frac{26}{35}$ = $\frac{\text{AM Peak Hour Traffic}}{\text{PM Peak Hour Traffic}}$
 1,000 = Average Daily Traffic

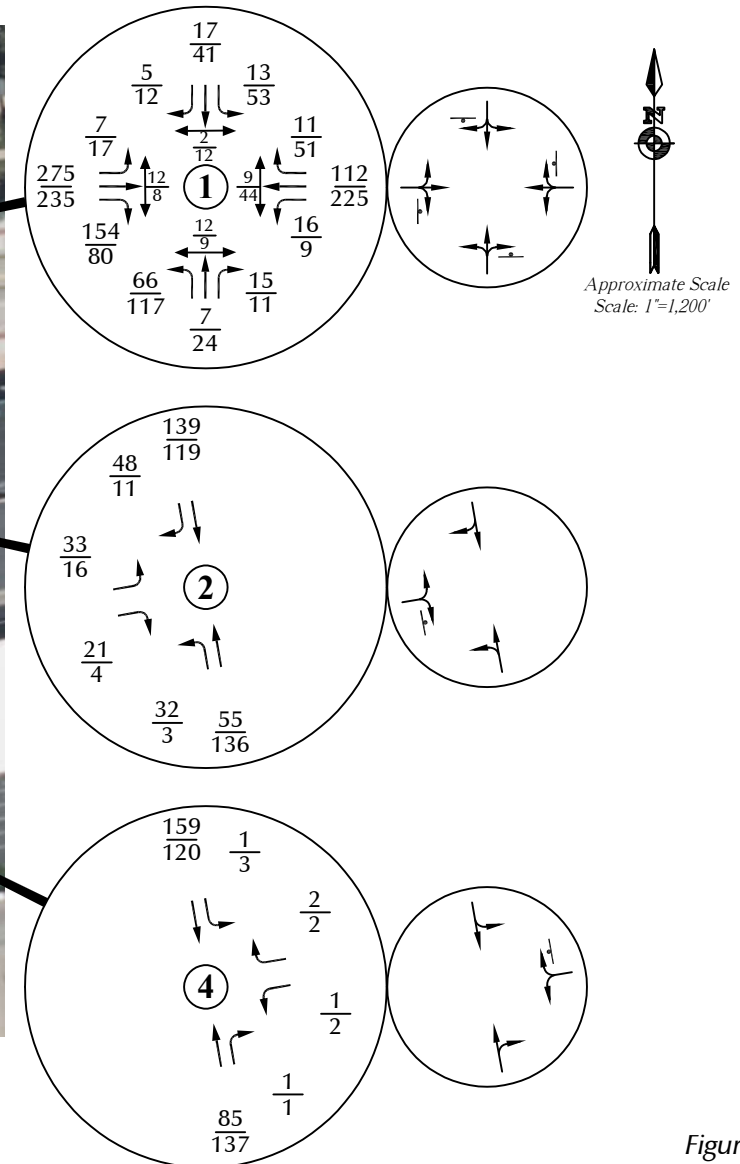
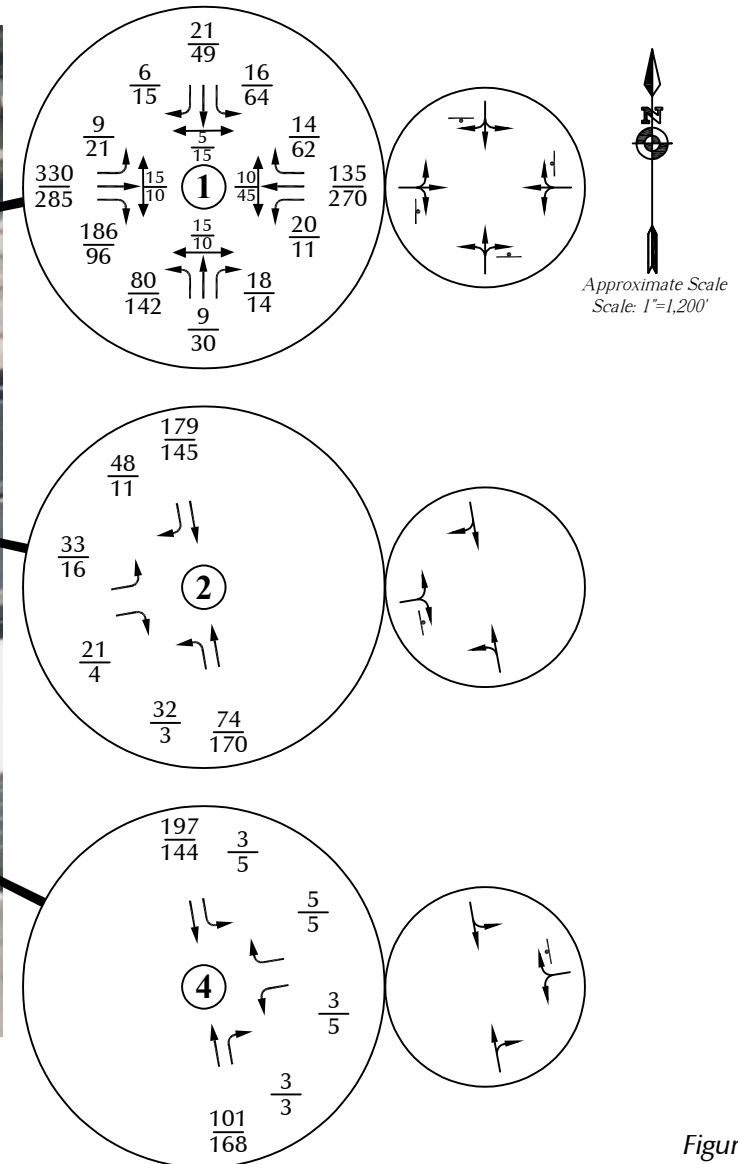


Figure 4
*Year 2026 Background Traffic,
Lane Geometry and Traffic Control*

Prime Sauce (LSC #240670)



† = Stop Sign
 $\frac{26}{35} = \frac{\text{AM Peak Hour Traffic}}{\text{PM Peak Hour Traffic}}$
 1,000 = Average Daily Traffic

Figure 5

*Year 2045 Background Traffic,
Lane Geometry and Traffic Control*

Prime Sauce (LSC #240670)



Approximate Scale
Scale: 1"=1,200'


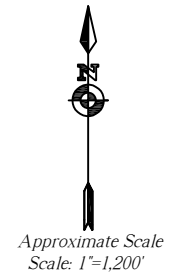
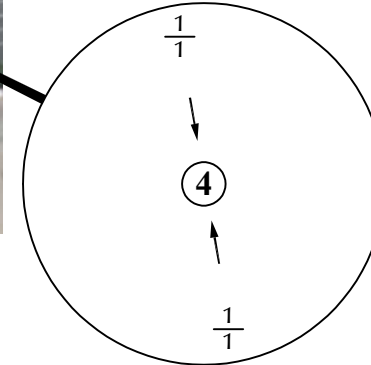
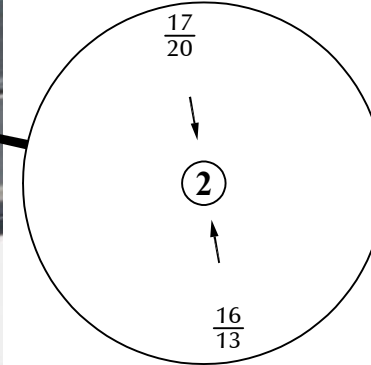
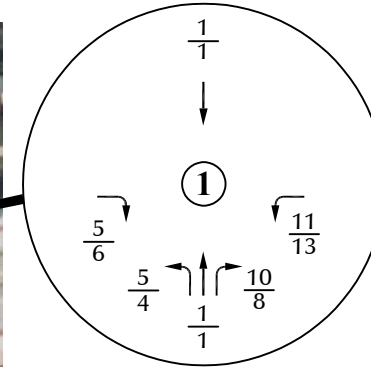
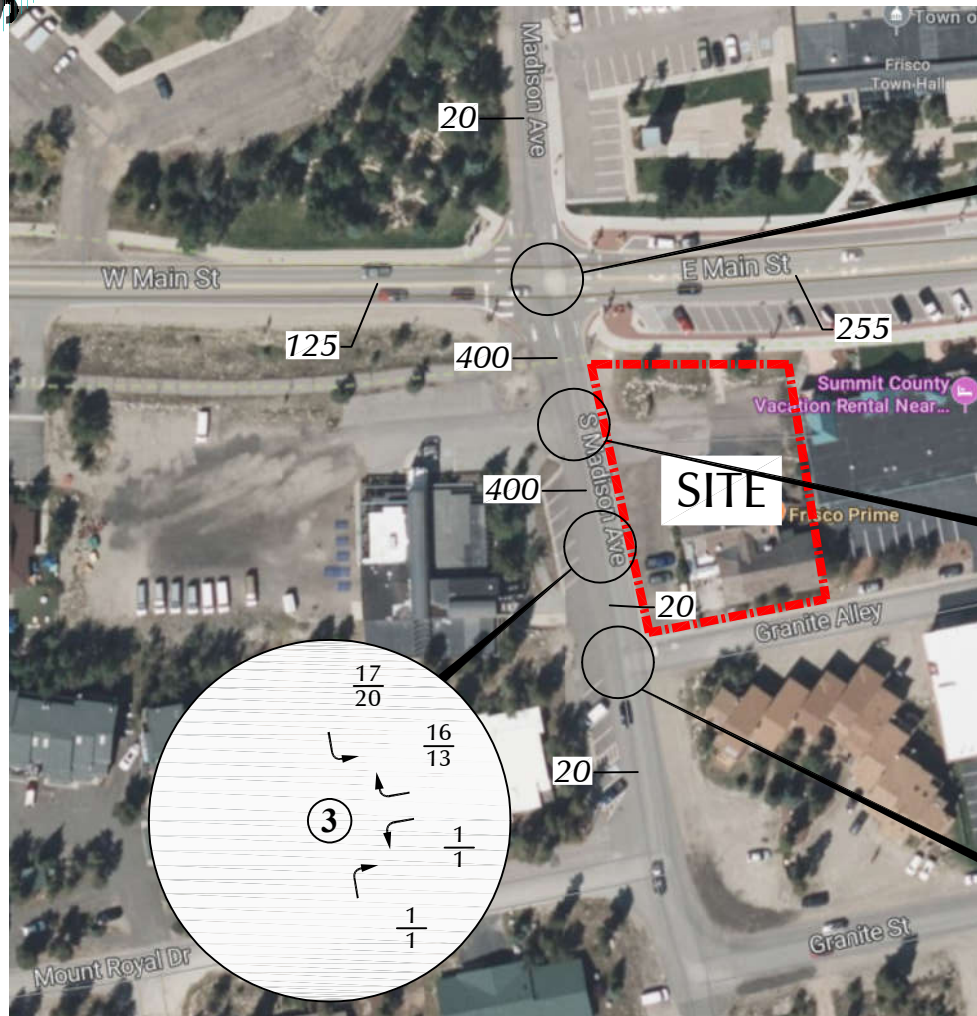
LEGEND:
 = Percent Directional Distribution

Figure 6
Directional Distribution of Site-Generated Traffic

Prime Sauce (LSC #240670)



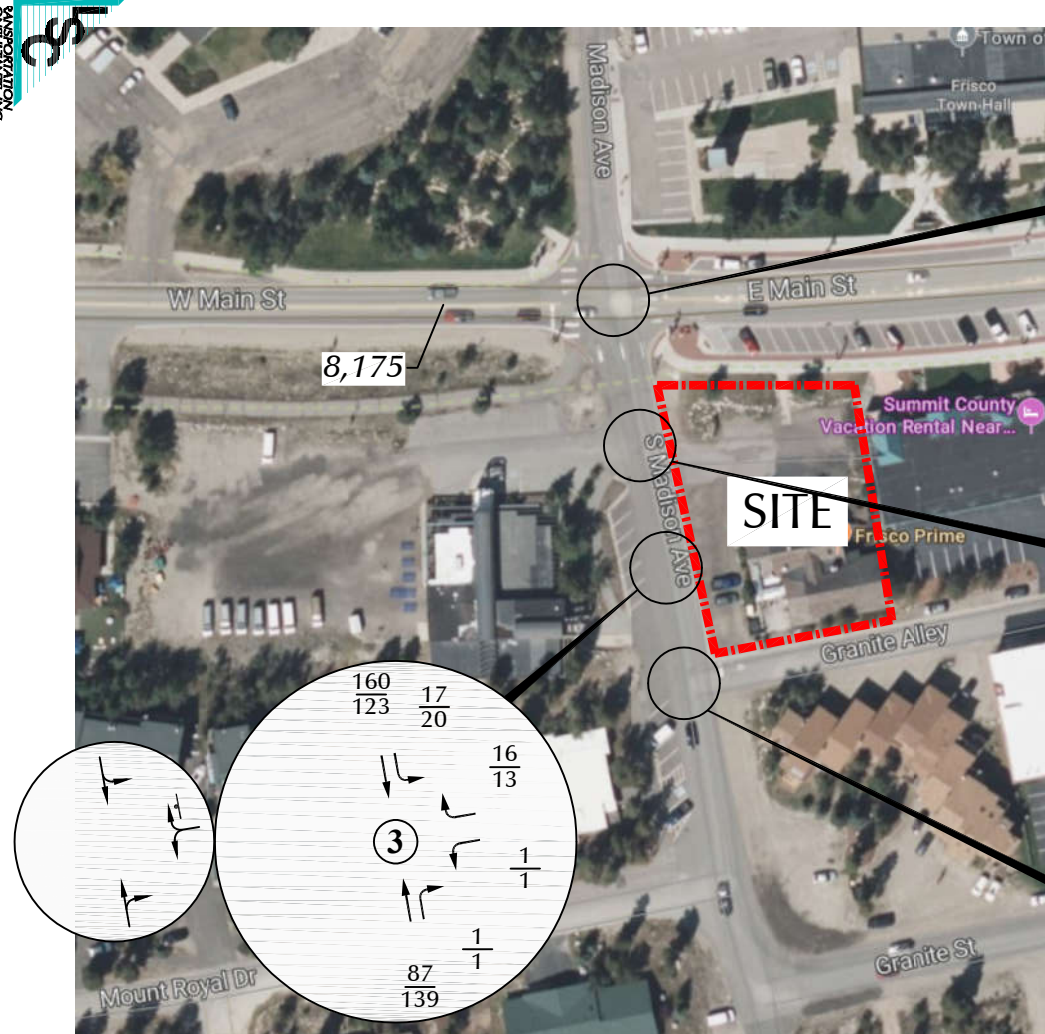
LEGEND:

$\frac{26}{35}$ = AM Peak Hour Traffic
 $\frac{35}{26}$ = PM Peak Hour Traffic
 1,000 = Average Daily Traffic

Figure 7

Assignment of Site-Generated Traffic

Prime Sauce (LSC #240670)



Note: These volumes are the sum of the volumes in Figures 4 and 7.

LEGEND:

- ⌋ = Stop Sign
- $\frac{26}{35}$ = $\frac{\text{AM Peak Hour Traffic}}{\text{PM Peak Hour Traffic}}$
- 1,000 = Average Daily Traffic

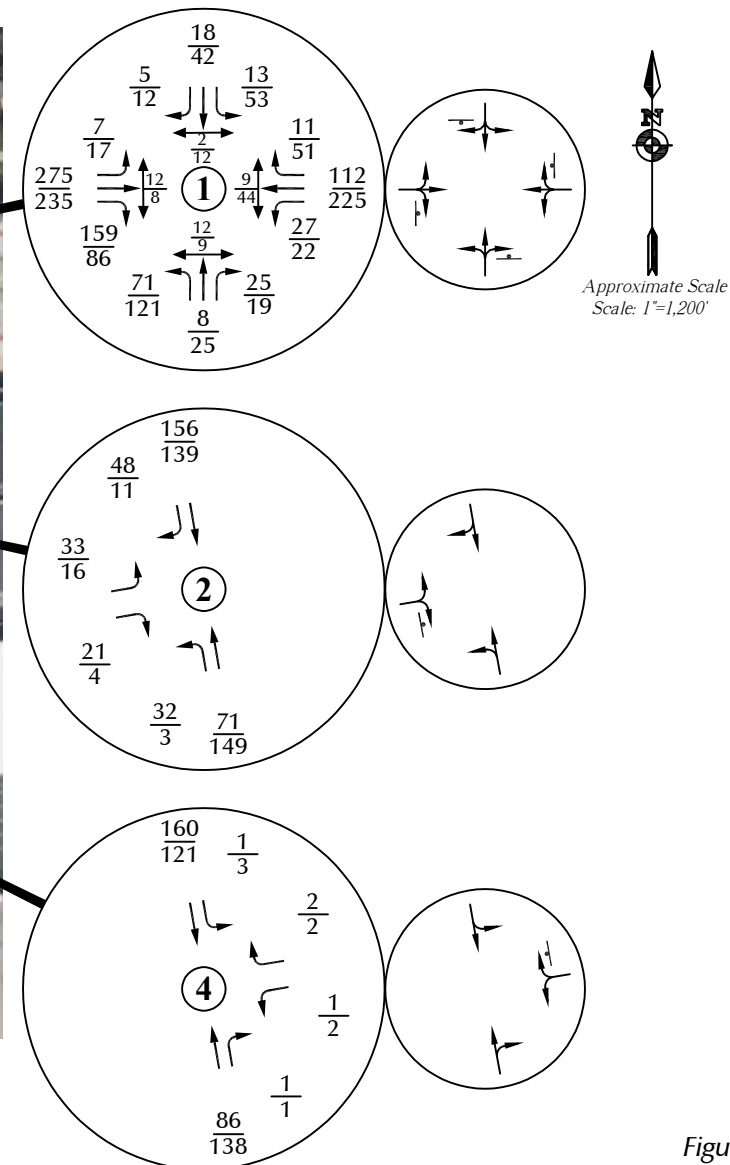


Figure 8
**Year 2026 Total Traffic,
Lane Geometry and Traffic Control**

Prime Sauce (LSC #240670)

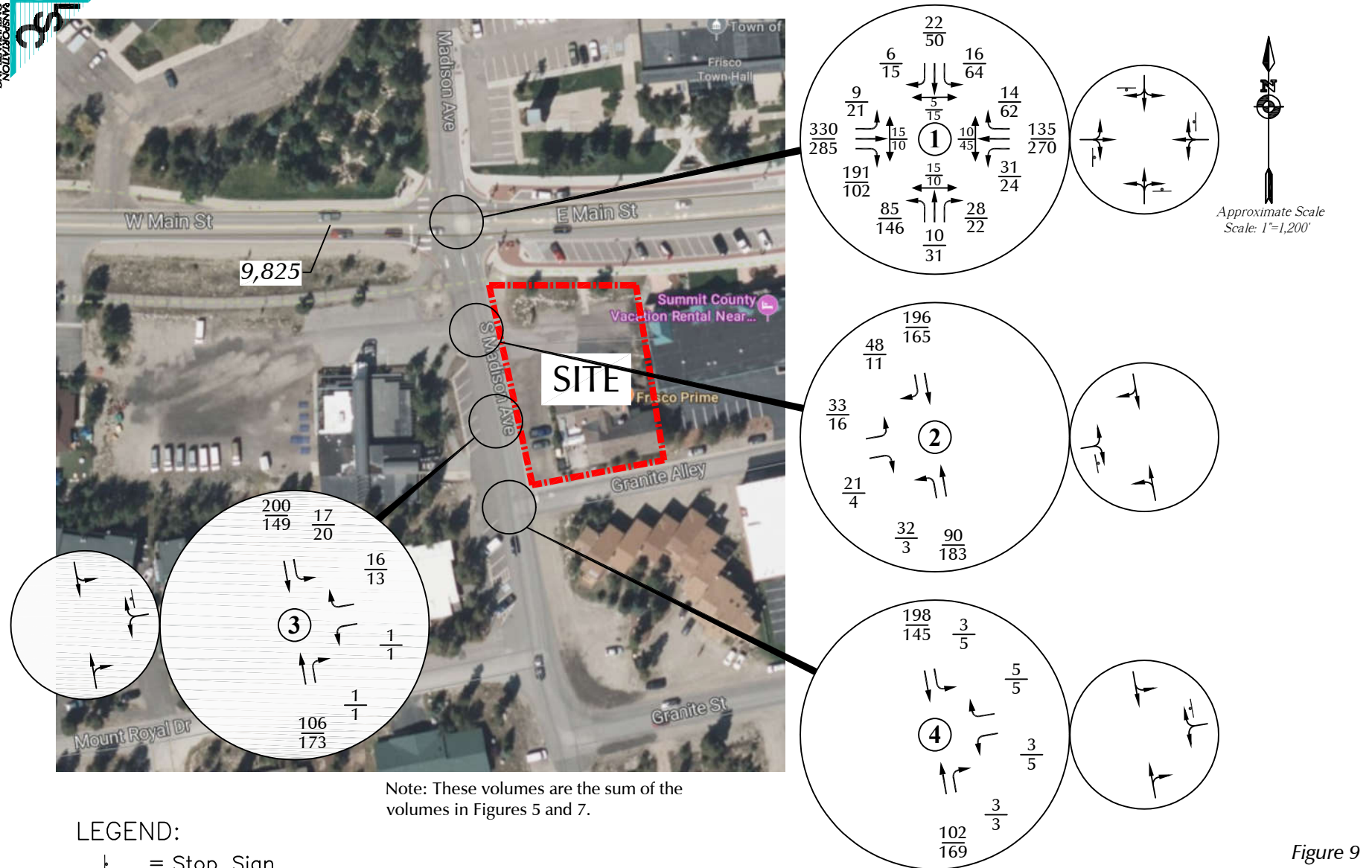


Figure 9
**Year 2045 Total Traffic,
Lane Geometry and Traffic Control**

Prime Sauce (LSC #240670)

COUNTER MEASURES INC.

1889 YORK STREET
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303-333-7409

N/S STREET: MADISON AVE
E/W STREET: GRANITE ALLEY
CITY: FRISCO
COUNTY: SUMMIT

File Name : MADIGRANITEALLEY
Site Code : 00000025
Start Date : 10/15/2024
Page No : 1

Groups Printed- Not Used

	MADISON AVE Southbound				GRANITE ALLEY Westbound				MADISON AVE Northbound				NO ACCESS Eastbound				Int. Total
Start Time	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	
Factor	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	
06:30 AM	0	3	0	0	0	0	0	0	0	3	0	0	0	0	0	0	6
06:45 AM	0	4	0	0	0	0	0	0	0	5	0	0	0	0	0	0	9
Total	0	7	0	0	0	0	0	0	0	8	0	0	0	0	0	0	15
07:00 AM	1	5	0	0	0	0	0	0	0	14	0	0	0	0	0	0	20
07:15 AM	0	9	0	0	0	0	0	0	0	5	0	0	0	0	0	0	14
07:30 AM	0	22	0	0	0	0	0	0	0	12	0	0	0	0	0	0	34
07:45 AM	0	41	0	0	0	0	0	0	0	27	0	0	0	0	0	0	68
Total	1	77	0	0	0	0	0	0	0	58	0	0	0	0	0	0	136
08:00 AM	0	21	0	0	0	0	0	0	0	11	0	0	0	0	0	0	32
08:15 AM	0	22	0	0	0	0	1	0	0	16	0	0	0	0	0	0	39
Total	0	43	0	0	0	0	1	0	0	27	0	0	0	0	0	0	71
04:00 PM	0	25	0	0	1	0	1	0	0	34	0	1	0	0	0	0	62
04:15 PM	0	15	0	0	0	0	0	0	0	25	0	0	0	0	0	0	40
04:30 PM	1	27	0	0	0	0	0	0	0	24	0	0	0	0	0	0	52
04:45 PM	1	20	0	0	0	0	0	0	0	20	0	0	0	0	0	0	41
Total	2	87	0	0	1	0	1	0	0	103	0	1	0	0	0	0	195
05:00 PM	0	34	0	0	0	0	1	0	0	21	0	0	0	0	0	0	56
05:15 PM	0	23	0	0	0	0	0	0	0	32	1	0	0	0	0	0	56
05:30 PM	0	11	0	0	0	0	1	0	0	14	0	0	0	0	0	0	26
05:45 PM	0	14	0	0	0	0	0	0	0	15	0	0	0	0	0	0	29
Total	0	82	0	0	0	0	2	0	0	82	1	0	0	0	0	0	167
Grand Total	3	296	0	0	1	0	4	0	0	278	1	1	0	0	0	0	584
Apprch %	1.0	99.0	0.0	0.0	20.0	0.0	80.0	0.0	0.0	99.3	0.4	0.4	0.0	0.0	0.0	0.0	
Total %	0.5	50.7	0.0	0.0	0.2	0.0	0.7	0.0	0.0	47.6	0.2	0.2	0.0	0.0	0.0	0.0	

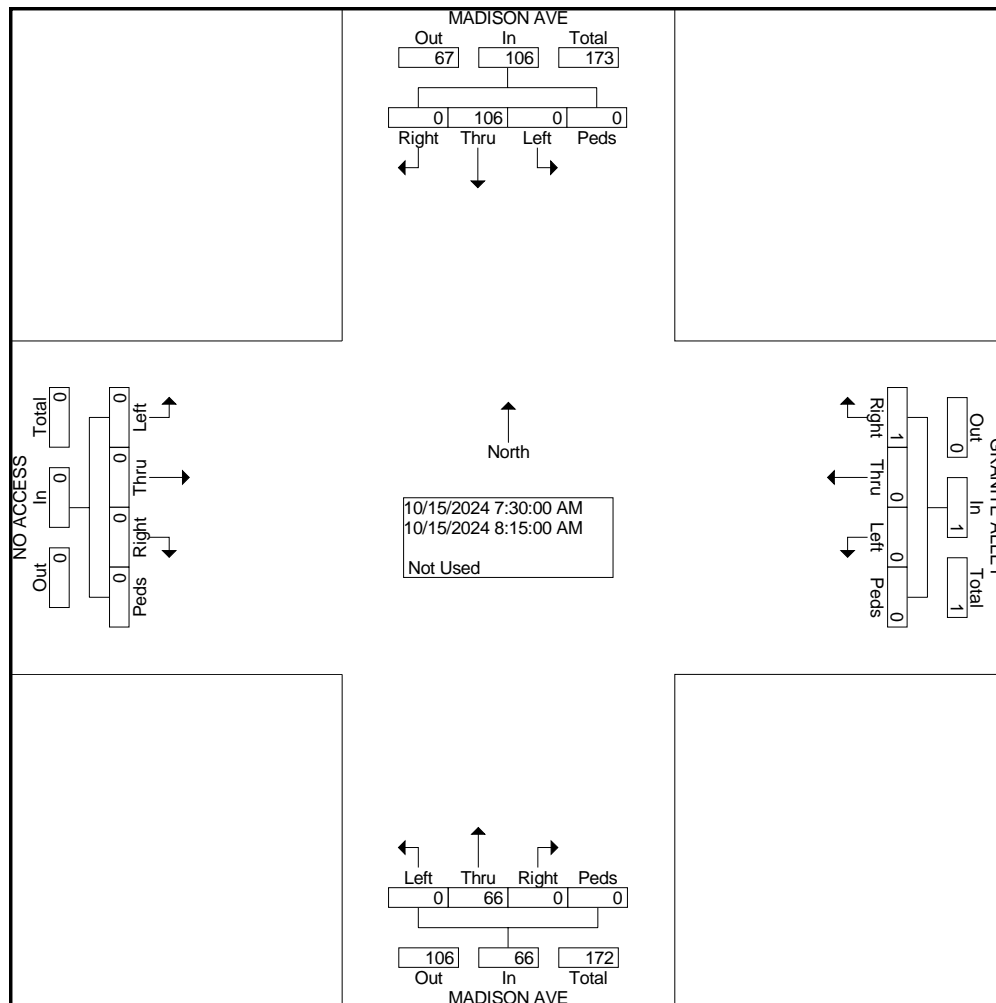
COUNTER MEASURES INC.

1889 YORK STREET
DENVER.COLORADO
303-333-7409

N/S STREET: MADISON AVE
E/W STREET: GRANITE ALLEY
CITY: FRISCO
COUNTY: SUMMIT

File Name : MADIGRANITEALLEY
Site Code : 00000025
Start Date : 10/15/2024
Page No : 2

	MADISON AVE Southbound					GRANITE ALLEY Westbound					MADISON AVE Northbound					NO ACCESS Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour From 07:30 AM to 08:15 AM - Peak 1 of 1																					
Intersection	07:30 AM																				
Volume	0	106	0	0	106	0	0	1	0	1	0	66	0	0	66	0	0	0	0	0	173
Percent	0.0	100.0	0.0	0.0		0.0	0.0	100.0	0.0		0.0	100.0	0.0	0.0		0.0	0.0	0.0	0.0		
07:45																					
Volume	0	41	0	0	41	0	0	0	0	0	0	27	0	0	27	0	0	0	0	0	68
Peak Factor																					0.636
High Int.	07:45 AM					08:15 AM					07:45 AM										
Volume	0	41	0	0	41	0	0	1	0	1	0	27	0	0	27						
Peak Factor	0.64					0.25					0.61										
	6					0					1										



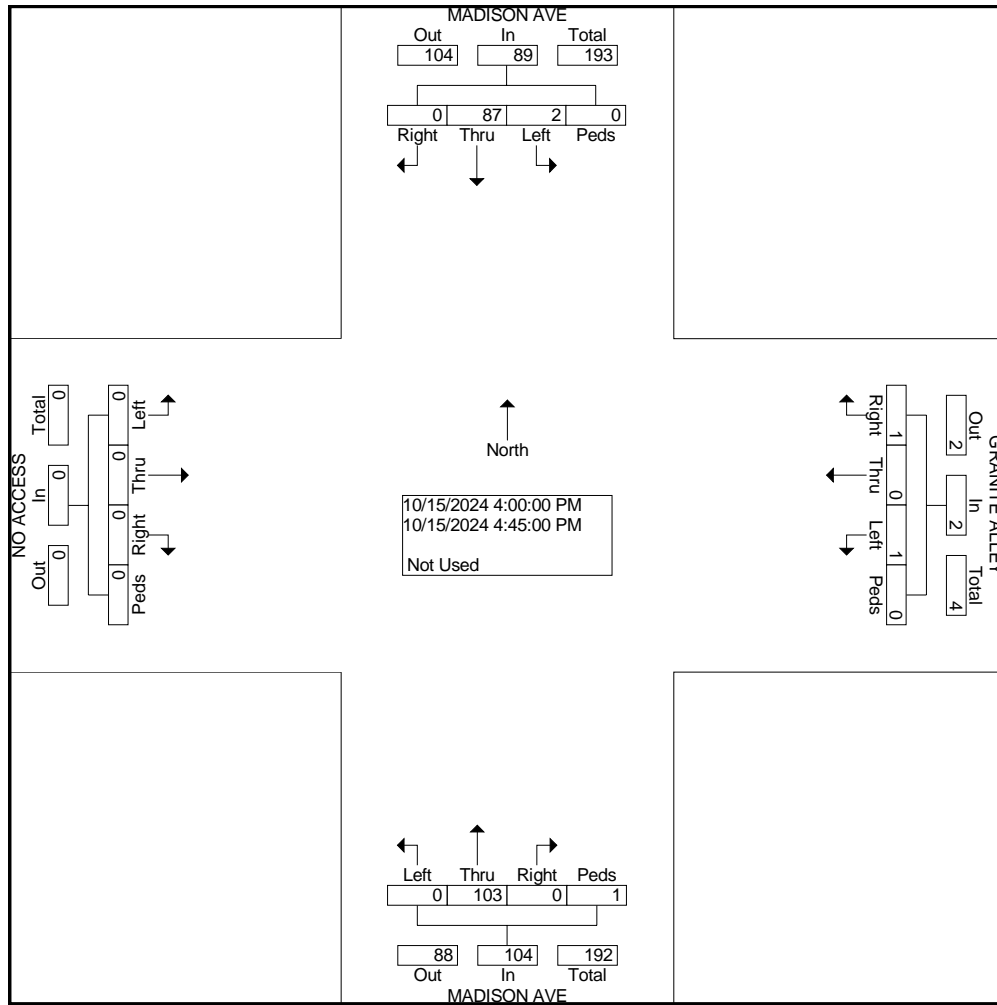
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	MADISON AVE Southbound					GRANITE ALLEY Westbound					MADISON AVE Northbound					NO ACCESS Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour From 04:00 PM to 04:45 PM - Peak 1 of 1																					
Intersection	04:00 PM																				
Volume	2	87	0	0	89	1	0	1	0	2	0	103	0	1	104	0	0	0	0	0	195
Percent	2.2	97.8	0.0	0.0		50.0	0.0	50.0	0.0		0.0	99.0	0.0	1.0		0.0	0.0	0.0	0.0		
04:00 Volume	0	25	0	0	25	1	0	1	0	2	0	34	0	1	35	0	0	0	0	0	62
Peak Factor																					0.786
High Int.	04:30 PM					04:00 PM					04:00 PM										
Volume	1	27	0	0	28	1	0	1	0	2	0	34	0	1	35						
Peak Factor					0.79					0.25					0.74						
					5					0					3						



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1889 YORK STREET
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N/S STREET: MADISON AVE
E/W STREET: PEAK SCHOOL ENTRANCE
CITY: FRISCO
COUNTY: SUMMIT

File Name : MADPEAK
Site Code : 00000025
Start Date : 10/15/2024
Page No : 1

Groups Printed- VEHICLES

	MADISON AVE Southbound				BUSINESS ACCESS Westbound				MADISON AVE Northbound				PEAK SCHOOL ENTRANCE Eastbound				Int. Total
Start Time	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	
Factor	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	
06:30 AM	0	4	0	0	0	0	0	0	0	3	0	0	0	0	0	0	7
06:45 AM	0	4	0	0	0	0	0	0	0	5	0	0	0	0	0	0	9
Total	0	8	0	0	0	0	0	0	0	8	0	0	0	0	0	0	16
07:00 AM	0	9	0	0	0	0	0	0	0	12	0	1	0	0	0	0	22
07:15 AM	0	9	0	0	0	0	0	0	0	6	0	0	0	0	0	0	15
07:30 AM	0	26	7	0	1	0	0	0	3	8	0	0	2	0	1	0	48
07:45 AM	1	35	18	0	0	0	0	1	17	7	0	0	13	1	10	1	104
Total	1	79	25	0	1	0	0	1	20	33	0	1	15	1	11	1	189
08:00 AM	0	17	10	0	1	0	0	0	3	4	1	0	8	0	5	3	52
08:15 AM	0	19	1	0	1	0	0	0	0	16	1	1	1	0	0	0	40
Total	0	36	11	0	2	0	0	0	3	20	2	1	9	0	5	3	92
04:00 PM	1	22	1	0	0	0	1	0	1	29	0	1	3	0	2	1	62
04:15 PM	0	17	3	0	0	0	0	0	0	18	0	0	3	0	0	0	41
04:30 PM	0	25	4	0	0	0	0	0	0	21	1	0	5	0	1	0	57
04:45 PM	0	24	0	0	0	0	0	0	0	15	0	0	0	0	0	0	39
Total	1	88	8	0	0	0	1	0	1	83	1	1	11	0	3	1	199
05:00 PM	1	37	1	0	0	0	1	0	0	14	0	0	0	0	0	0	54
05:15 PM	0	23	1	0	0	0	1	1	1	27	0	0	0	0	0	0	54
05:30 PM	1	11	0	0	0	0	1	0	0	12	0	0	2	0	1	0	28
05:45 PM	0	14	0	0	0	0	0	0	0	14	0	0	0	0	0	0	28
Total	2	85	2	0	0	0	3	1	1	67	0	0	2	0	1	0	164
Grand Total	4	296	46	0	3	0	4	2	25	211	3	3	37	1	20	5	660
Apprch %	1.2	85.5	13.3	0.0	33.3	0.0	44.4	22.2	10.3	87.2	1.2	1.2	58.7	1.6	31.7	7.9	
Total %	0.6	44.8	7.0	0.0	0.5	0.0	0.6	0.3	3.8	32.0	0.5	0.5	5.6	0.2	3.0	0.8	

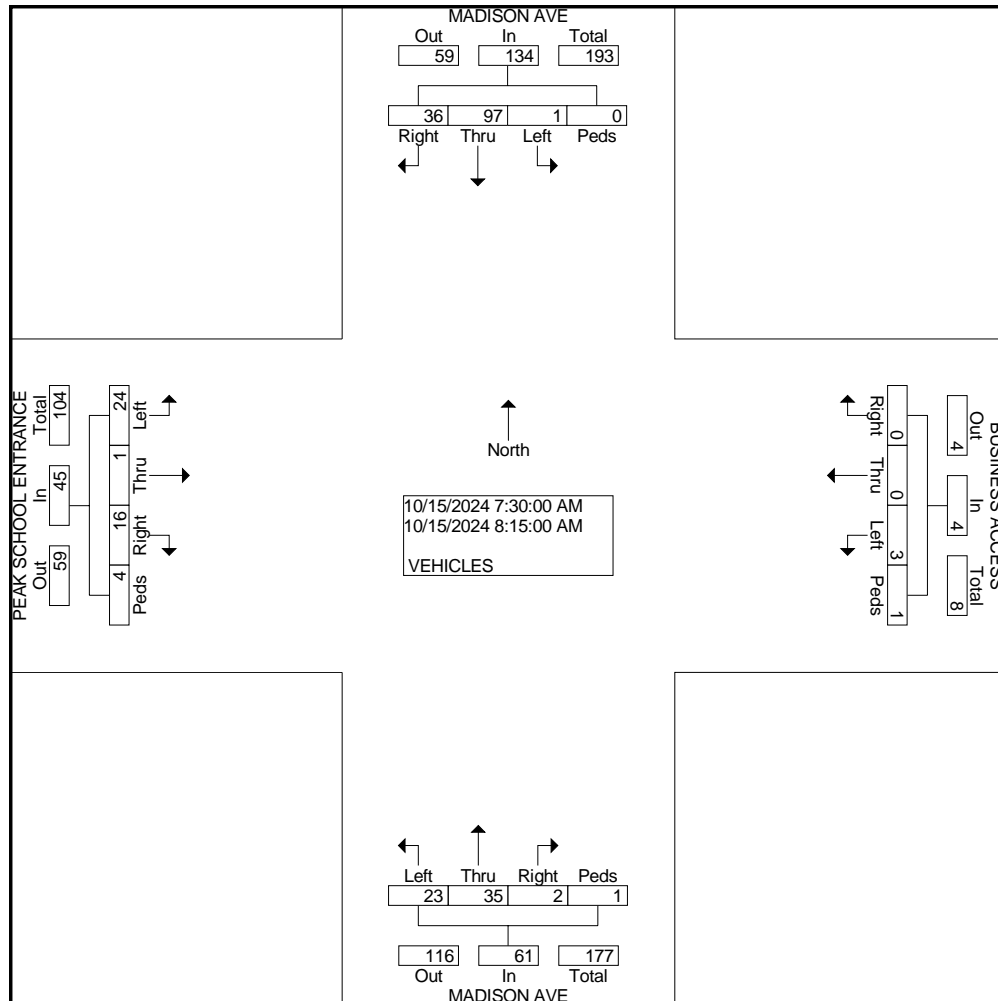
COUNTER MEASURES INC.

1889 YORK STREET
DENVER.COLORADO
303-333-7409

N/S STREET: MADISON AVE
E/W STREET: PEAK SCHOOL ENTRANCE
CITY: FRISCO
COUNTY: SUMMIT

File Name : MADPEAK
Site Code : 00000025
Start Date : 10/15/2024
Page No : 2

	MADISON AVE Southbound					BUSINESS ACCESS Westbound					MADISON AVE Northbound					PEAK SCHOOL ENTRANCE Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour From 07:30 AM to 08:15 AM - Peak 1 of 1	07:30 AM																				
Intersection	07:30 AM																				
Volume	1	97	36	0	134	3	0	0	1	4	23	35	2	1	61	24	1	16	4	45	244
Percent	0.7	72.4	26.9	0.0		75.0	0.0	0.0	25.0		37.7	57.4	3.3	1.6		53.3	2.2	35.6	8.9		
07:45																					
Volume	1	35	18	0	54	0	0	0	1	1	17	7	0	0	24	13	1	10	1	25	104
Peak Factor																					0.587
High Int.	07:45 AM					07:30 AM					07:45 AM					07:45 AM					
Volume	1	35	18	0	54	1	0	0	0	1	17	7	0	0	24	13	1	10	1	25	
Peak Factor	0.62					1.00					0.63					0.45					
	0					0					5					0					



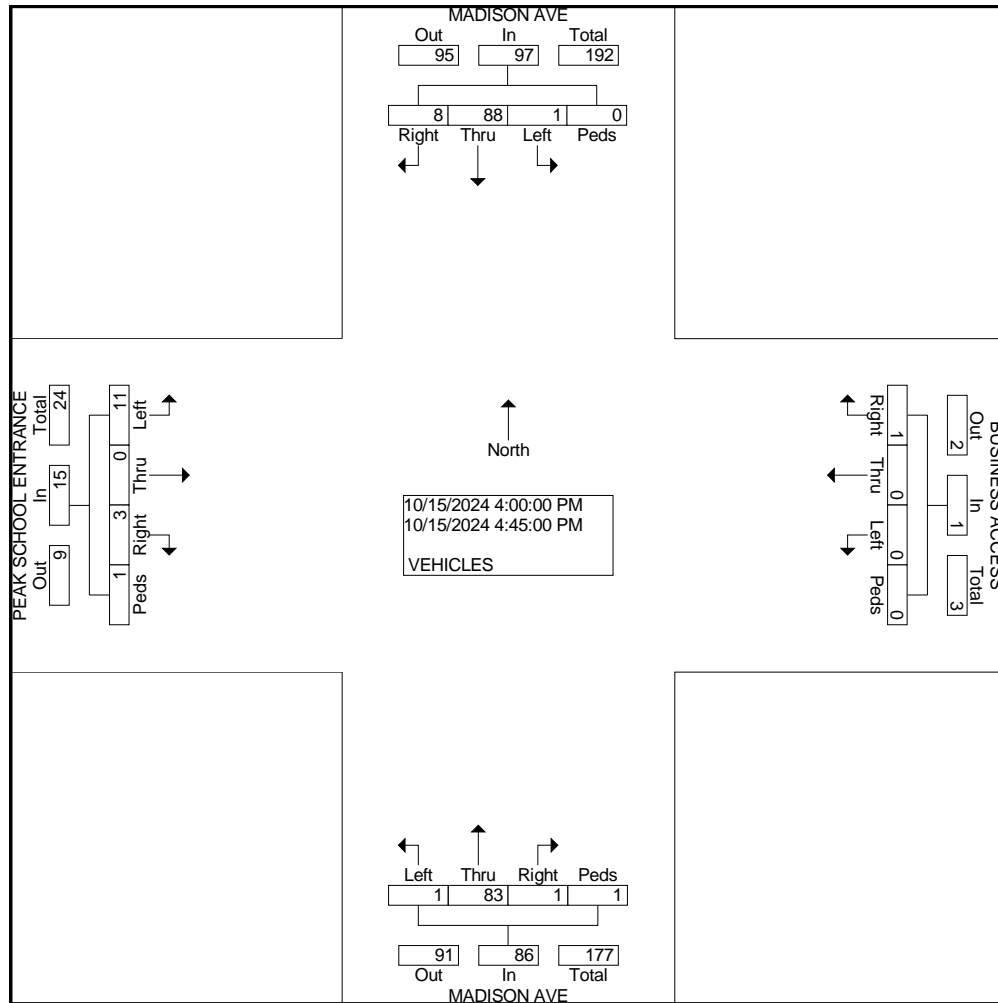
COUNTER MEASURES INC.

1889 YORK STREET
DENVER.COLORADO
303-333-7409

N/S STREET: MADISON AVE
E/W STREET: PEAK SCHOOL ENTRANCE
CITY: FRISCO
COUNTY: SUMMIT

File Name : MADPEAK
Site Code : 00000025
Start Date : 10/15/2024
Page No : 3

	MADISON AVE Southbound					BUSINESS ACCESS Westbound					MADISON AVE Northbound					PEAK SCHOOL ENTRANCE Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour From 04:00 PM to 04:45 PM - Peak 1 of 1																					
Intersection	04:00 PM																				
Volume	1	88	8	0	97	0	0	1	0	1	1	83	1	1	86	11	0	3	1	15	199
Percent	1.0	90.7	8.2	0.0		0.0	0.0	100.0	0.0		1.2	96.5	1.2	1.2		73.3	0.0	20.0	6.7		
04:00 Volume	1	22	1	0	24	0	0	1	0	1	1	29	0	1	31	3	0	2	1	6	62
Peak Factor	0.802																				
High Int. Volume	04:30 PM					04:00 PM					04:00 PM					04:00 PM					
Peak Factor	0	25	4	0	29	0	0	1	0	1	1	29	0	1	31	3	0	2	1	6	
	0.836					0.250					0.694					0.625					



COUNTER MEASURES INC.

1889 YORK STREET
DENVER.COLORADO
303-333-7409

N/S STREET: MADISON AVE
E/W STREET: W. MAIN ST
CITY: FRISCO
COUNTY: SUMMIT

File Name : MADIMAIN
Site Code : 00000008
Start Date : 10/15/2024
Page No : 1

Groups Printed- VEHICLES

	MADISON AVE Southbound				W. MAIN ST Westbound				MADISON AVE Northbound				W. MAIN ST Eastbound				Int. Total
Start Time	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	
Factor	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	
06:30 AM	2	1	1	0	0	13	2	1	2	1	0	0	1	13	2	0	39
06:45 AM	1	0	0	0	1	18	1	0	4	0	0	0	0	19	3	0	47
Total	3	1	1	0	1	31	3	1	6	1	0	0	1	32	5	0	86
07:00 AM	0	4	0	1	1	14	1	4	12	0	2	2	0	21	3	1	66
07:15 AM	0	3	2	0	1	11	4	1	2	1	1	0	0	40	7	0	73
07:30 AM	3	0	1	0	3	17	2	3	9	0	2	3	1	55	35	5	139
07:45 AM	1	3	1	1	8	24	3	3	15	2	6	1	2	55	41	0	166
Total	4	10	4	2	13	66	10	11	38	3	11	6	3	171	86	6	444
08:00 AM	3	8	1	0	0	24	2	0	9	2	2	4	2	46	18	4	125
08:15 AM	3	2	1	0	1	16	1	1	14	1	1	1	0	44	19	0	105
Total	6	10	2	0	1	40	3	1	23	3	3	5	2	90	37	4	230
04:00 PM	8	12	1	1	1	43	15	19	27	7	4	3	6	37	12	0	196
04:15 PM	10	6	3	4	3	41	8	4	23	4	0	0	4	37	10	2	159
04:30 PM	3	8	4	1	2	47	6	0	19	4	4	4	3	51	15	4	175
04:45 PM	18	4	1	3	1	34	8	9	15	3	0	0	0	45	21	0	162
Total	39	30	9	9	7	165	37	32	84	18	8	7	13	170	58	6	692
05:00 PM	11	6	0	0	5	63	4	11	16	3	1	3	2	41	24	2	192
05:15 PM	6	4	1	1	2	35	4	4	22	8	2	0	4	28	18	2	141
05:30 PM	5	4	4	0	2	34	6	0	14	2	1	0	1	41	5	3	122
05:45 PM	3	4	1	6	1	35	4	1	10	4	4	1	0	28	11	4	117
Total	25	18	6	7	10	167	18	16	62	17	8	4	7	138	58	11	572
Grand Total	77	69	22	18	32	469	71	61	213	42	30	22	26	601	244	27	2024
Apprch %	41.4	37.1	11.8	9.7	5.1	74.1	11.2	9.6	69.4	13.7	9.8	7.2	2.9	66.9	27.2	3.0	
Total %	3.8	3.4	1.1	0.9	1.6	23.2	3.5	3.0	10.5	2.1	1.5	1.1	1.3	29.7	12.1	1.3	

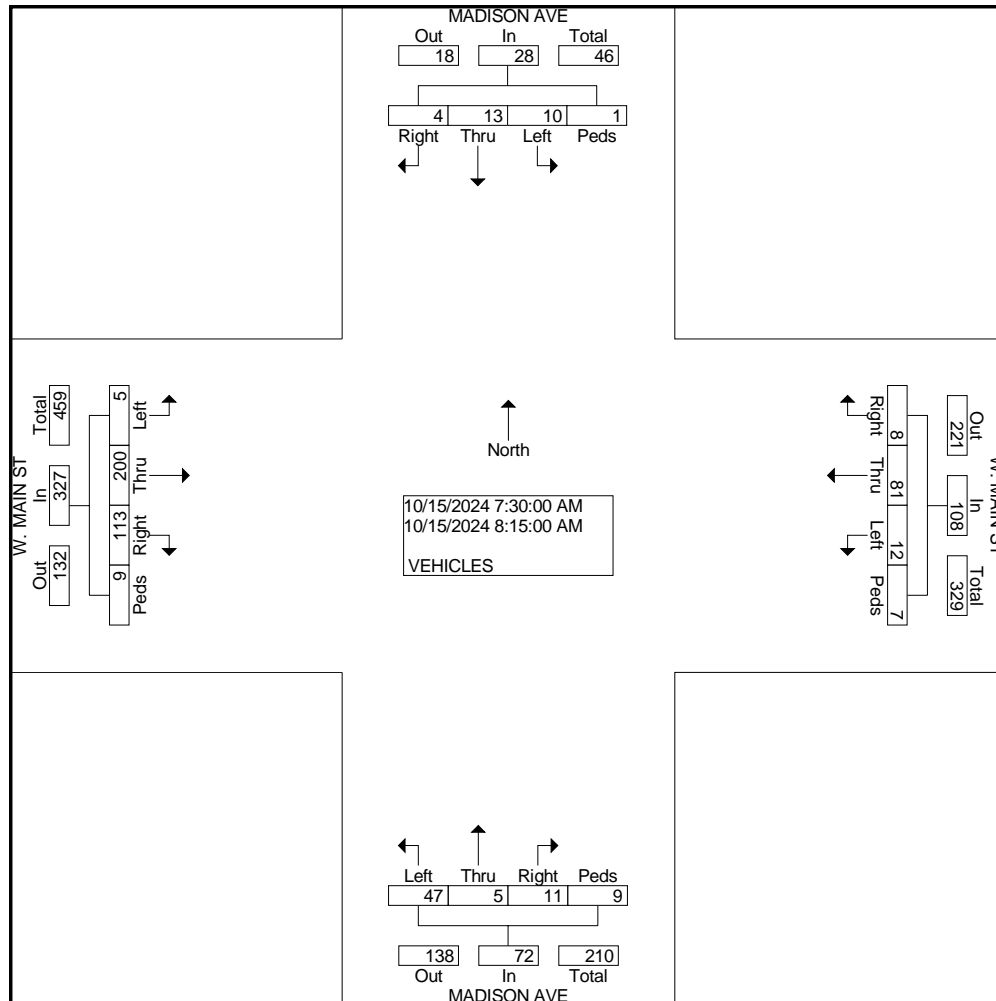
COUNTER MEASURES INC.

1889 YORK STREET
DENVER.COLORADO
303-333-7409

N/S STREET: MADISON AVE
E/W STREET: W. MAIN ST
CITY: FRISCO
COUNTY: SUMMIT

File Name : MADIMAIN
Site Code : 00000008
Start Date : 10/15/2024
Page No : 2

	MADISON AVE Southbound					W. MAIN ST Westbound					MADISON AVE Northbound					W. MAIN ST Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour	From 06:30 AM to 08:15 AM - Peak 1 of 1																				
Intersection	07:30 AM																				
Volume	10	13	4	1	28	12	81	8	7	108	47	5	11	9	72	5	200	113	9	327	535
Percent	35.7	46.4	14.3	3.6		11.1	75.0	7.4	6.5		65.3	6.9	15.3	12.5		1.5	61.2	34.6	2.8		
07:45																					
Volume	1	3	1	1	6	8	24	3	3	38	15	2	6	1	24	2	55	41	0	98	166
Peak Factor																					0.806
High Int.	08:00 AM					07:45 AM					07:45 AM					07:45 AM					
Volume	3	8	1	0	12	8	24	3	3	38	15	2	6	1	24	2	55	41	0	98	
Peak Factor	0.58					0.71					0.75					0.83					
	3					1					0					4					



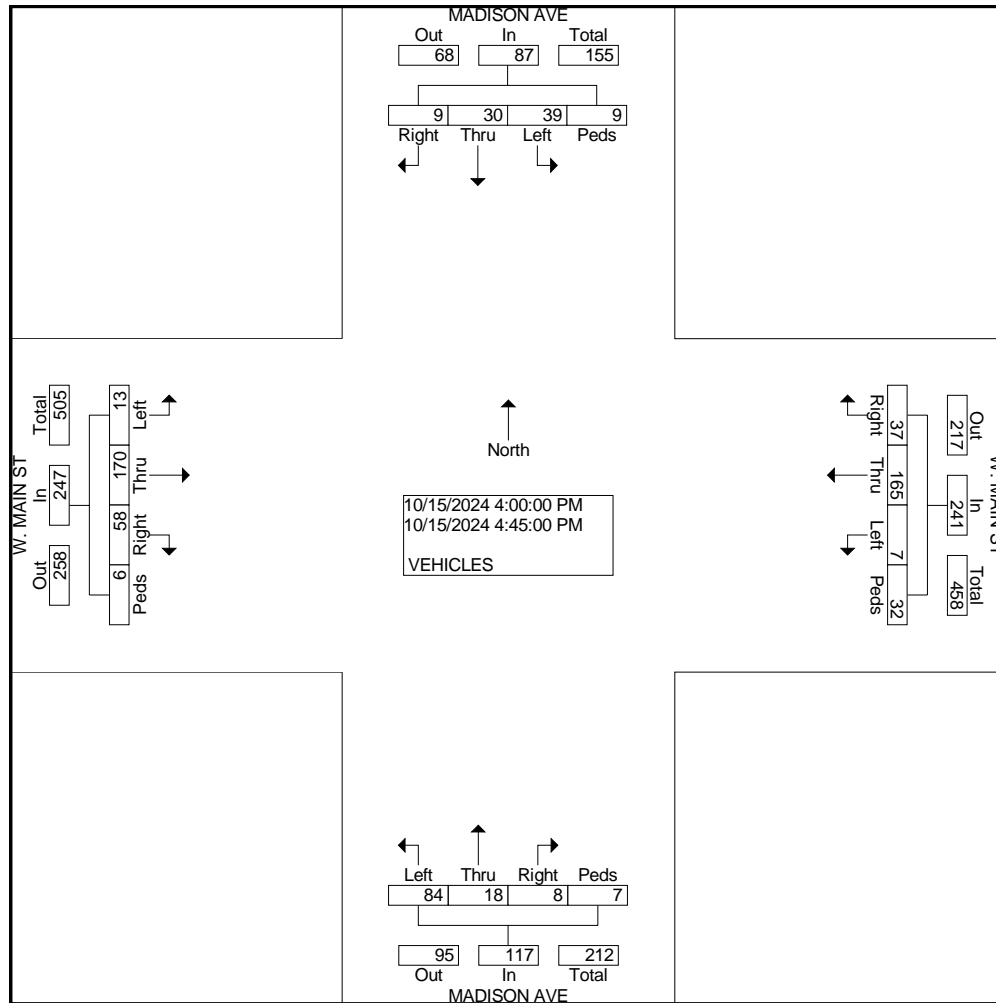
COUNTER MEASURES INC.

1889 YORK STREET
DENVER.COLORADO
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N/S STREET: MADISON AVE
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Start Date : 10/15/2024
Page No : 3

	MADISON AVE Southbound					W. MAIN ST Westbound					MADISON AVE Northbound					W. MAIN ST Eastbound					
Start Time	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Int. Total
Peak Hour From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Intersection	04:00 PM																				
Volume	39	30	9	9	87	7	165	37	32	241	84	18	8	7	117	13	170	58	6	247	692
Percent	44.	34.	10.	10.		2.9	68.	15.	13.		71.	15.	6.8	6.0		5.3	68.	23.	2.4		
	8	5	3	3			5	4	3		8	4					8	5			
04:00																					
Volume	8	12	1	1	22	1	43	15	19	78	27	7	4	3	41	6	37	12	0	55	196
Peak																					0.883
Factor																					
High Int.	04:45 PM					04:00 PM					04:00 PM					04:30 PM					
Volume	18	4	1	3	26	1	43	15	19	78	27	7	4	3	41	3	51	15	4	73	
Peak																					
Factor	0.83					0.77					0.71					0.84					6
	7					2					3										



PRIME SAUCE
Location: W. MAIN ST W-O MADISON AVE
City: FRISCO
County: SUMMIT
Direction: EAST/WEST



Site Code:2415306 2415306
Site Code:2415306
Start Date: 10152024 10/15/2024
End Date: 10162024 10/16/2024
Longitude: 0.000000
Longitude: 0.000000

10/15/2024	EAST	WEST	Total
Time			
12:00 AM	*	*	0
1:00	*	*	0
2:00	*	*	0
3:00	*	*	0
4:00	*	*	0
5:00	23	14	37
6:00	56	59	115
7:00	254	109	363
8:00	258	153	411
9:00	216	146	362
10:00	182	171	353
11:00	229	162	391
12:00 PM	204	186	390
1:00	191	165	356
2:00	216	186	402
3:00	238	233	471
4:00	237	259	496
5:00	194	230	424
6:00	59	127	186
7:00	34	67	101
8:00	26	44	70
9:00	37	28	65
10:00	30	18	48
11:00	23	16	39
Total	2707	2373	5080
Percent	53.3%	46.7%	
AM Peak	8:00	10:00	8:00
Volume	258	171	411
PM Peak	3:00	4:00	4:00
Volume	238	259	496

PRIME SAUCE
Location: W. MAIN ST W-O MADISON AVE
City: FRISCO
County: SUMMIT
Direction: EAST/WEST



Site Code:2415306 2415306
Site Code:2415306
Start Date: 10152024 10/15/2024
End Date: 10162024 10/16/2024
Longitude: 0.000000
Longitude: 0.000000

10/16/2024	EAST	WEST	Total
Time			
12:00 AM	16	11	27
1:00	11	8	19
2:00	9	6	15
3:00	6	5	11
4:00	12	5	17
5:00	19	16	35
6:00	51	48	99
7:00	267	118	385
8:00	256	146	402
9:00	205	151	356
10:00	0	0	0
11:00	0	0	0
12:00 PM	0	0	0
1:00	0	0	0
2:00	*	*	0
3:00	*	*	0
4:00	*	*	0
5:00	*	*	0
6:00	*	*	0
7:00	*	*	0
8:00	*	*	0
9:00	*	*	0
10:00	*	*	0
11:00	*	*	0
Total	852	514	1366
Percent	62.4%	37.6%	
AM Peak	7:00	9:00	8:00
Volume	267	151	402
PM Peak			
Volume			
Grand Total	3559	2887	6446
Percent	55.2%	44.8%	
ADT		ADT: 5,164	AADT: 5,164

LEVEL OF SERVICE DEFINITIONS

From *Highway Capacity Manual*, Transportation Research Board

UNSIGNALIZED INTERSECTION LEVEL OF SERVICE (LOS)

Applicable to Two-Way Stop Control, All-Way Stop Control, and Roundabouts

LOS	Average Vehicle Control Delay	Operational Characteristics
A	<10 seconds	Normally, vehicles on the stop-controlled approach only have to wait up to 10 seconds before being able to clear the intersection. Left-turning vehicles on the uncontrolled street do not have to wait to make their turn.
B	10 to 15 seconds	Vehicles on the stop-controlled approach will experience delays before being able to clear the intersection. <u>The delay could be up to 15 seconds.</u> Left-turning vehicles on the uncontrolled street may have to wait to make their turn.
C	15 to 25 seconds	Vehicles on the stop-controlled approach can expect delays in the range of 15 to 25 seconds before clearing the intersection. Motorists may begin to take chances due to the long delays, thereby posing a safety risk to through traffic. <u>Left-turning vehicles on the uncontrolled street will now be required to wait to make their turn causing a queue to be created in the turn lane.</u>
D	25 to 35 seconds	<u>This is the point at which a traffic signal may be warranted for this intersection.</u> The delays for the stop-controlled intersection are not considered to be excessive. The length of the queue may begin to block other public and private access points.
E	35 to 50 seconds	The delays for all critical traffic movements are considered to be unacceptable. The length of the queues for the stop-controlled approaches as well as the left-turn movements are extremely long. <u>There is a high probability that this intersection will meet traffic signal warrants.</u> The ability to install a traffic signal is affected by the location of other existing traffic signals. Consideration may be given to restricting the accesses by eliminating the left-turn movements from and to the stop-controlled approach.
F	>50 seconds	The delay for the critical traffic movements are probably in excess of 100 seconds. The length of the queues are extremely long. Motorists are selecting alternative routes due to the long delays. <u>The only remedy for these long delays is installing a traffic signal or restricting the accesses.</u> The potential for accidents at this intersection are extremely high due to motorist taking more risky chances. If the median permits, motorists begin making two-stage left-turns.

HCM 6th AWSC
1: Madison Avenue & Main Street

Existing
AM Peak

Intersection	
Intersection Delay, s/veh	11.6
Intersection LOS	B





Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔			↔	
Traffic Vol, veh/h	7	270	151	16	110	11	65	7	15	13	17	5
Future Vol, veh/h	7	270	151	16	110	11	65	7	15	13	17	5
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	8	307	172	18	125	13	74	8	17	15	19	6
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	13.1	9.1	9.5	8.9
HCM LOS	B	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	75%	2%	12%	37%
Vol Thru, %	8%	63%	80%	49%
Vol Right, %	17%	35%	8%	14%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	87	428	137	35
LT Vol	65	7	16	13
Through Vol	7	270	110	17
RT Vol	15	151	11	5
Lane Flow Rate	99	486	156	40
Geometry Grp	1	1	1	1
Degree of Util (X)	0.149	0.579	0.208	0.061
Departure Headway (Hd)	5.443	4.285	4.799	5.491
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	653	841	744	646
Service Time	3.519	2.324	2.855	3.575
HCM Lane V/C Ratio	0.152	0.578	0.21	0.062
HCM Control Delay	9.5	13.1	9.1	8.9
HCM Lane LOS	A	B	A	A
HCM 95th-tile Q	0.5	3.8	0.8	0.2

Intersection

Int Delay, s/veh 2.8

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	32	1	21	4	1	1	31	54	3	1	135	48
Future Vol, veh/h	32	1	21	4	1	1	31	54	3	1	135	48
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	43	1	28	5	1	1	41	72	4	1	180	64




Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	371	372	212	385	402	74	244	0	0	76	0	0
Stage 1	214	214	-	156	156	-	-	-	-	-	-	-
Stage 2	157	158	-	229	246	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	586	558	828	573	537	988	1322	-	-	1523	-	-
Stage 1	788	725	-	846	769	-	-	-	-	-	-	-
Stage 2	845	767	-	774	703	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	570	540	828	539	519	988	1322	-	-	1523	-	-
Mov Cap-2 Maneuver	570	540	-	539	519	-	-	-	-	-	-	-
Stage 1	763	724	-	819	744	-	-	-	-	-	-	-
Stage 2	815	742	-	746	702	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	11.2		11.3		2.8		0	
HCM LOS	B		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1322	-	-	648	579	1523	-
HCM Lane V/C Ratio	0.031	-	-	0.111	0.014	0.001	-
HCM Control Delay (s)	7.8	0	-	11.2	11.3	7.4	0
HCM Lane LOS	A	A	-	B	B	A	A
HCM 95th %tile Q(veh)	0.1	-	-	0.4	0	0	-

HCM 6th TWSC
4: Madison Avenue & Granite Alley





Existing
AM Peak

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	2	86	1	1	159
Future Vol, veh/h	1	2	86	1	1	159
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	3	115	1	1	212
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	330	116	0	0	116	0
Stage 1	116	-	-	-	-	-
Stage 2	214	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	665	936	-	-	1473	-
Stage 1	909	-	-	-	-	-
Stage 2	822	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	664	936	-	-	1473	-
Mov Cap-2 Maneuver	664	-	-	-	-	-
Stage 1	909	-	-	-	-	-
Stage 2	821	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	9.4	0	0			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	824	1473	-	
HCM Lane V/C Ratio	-	-	0.005	0.001	-	
HCM Control Delay (s)	-	-	9.4	7.4	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0	0	-	

HCM 6th AWSC
1: Madison Avenue & Main Street

Existing
PM Peak

Intersection	
Intersection Delay, s/veh	12.8
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	17	230	78	9	220	50	115	24	11	52	40	12
Future Vol, veh/h	17	230	78	9	220	50	115	24	11	52	40	12
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	19	261	89	10	250	57	131	27	13	59	45	14
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	14	12.9	11.6	10.7
HCM LOS	B	B	B	B

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	77%	5%	3%	50%
Vol Thru, %	16%	71%	79%	38%
Vol Right, %	7%	24%	18%	12%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	150	325	279	104
LT Vol	115	17	9	52
Through Vol	24	230	220	40
RT Vol	11	78	50	12
Lane Flow Rate	170	369	317	118
Geometry Grp	1	1	1	1
Degree of Util (X)	0.287	0.531	0.465	0.201
Departure Headway (Hd)	6.069	5.18	5.283	6.113
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	590	694	678	584
Service Time	4.13	3.228	3.333	4.179
HCM Lane V/C Ratio	0.288	0.532	0.468	0.202
HCM Control Delay	11.6	14	12.9	10.7
HCM Lane LOS	B	B	B	B
HCM 95th-tile Q	1.2	3.2	2.5	0.7

Intersection

Int Delay, s/veh 1

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	15	1	4	1	1	2	2	133	2	1	115	11
Future Vol, veh/h	15	1	4	1	1	2	2	133	2	1	115	11
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	20	1	5	1	1	3	3	177	3	1	153	15




Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	350	349	161	351	355	179	168	0	0	180	0	0
Stage 1	163	163	-	185	185	-	-	-	-	-	-	-
Stage 2	187	186	-	166	170	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	605	575	884	604	571	864	1410	-	-	1396	-	-
Stage 1	839	763	-	817	747	-	-	-	-	-	-	-
Stage 2	815	746	-	836	758	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	601	573	884	598	569	864	1410	-	-	1396	-	-
Mov Cap-2 Maneuver	601	573	-	598	569	-	-	-	-	-	-	-
Stage 1	837	762	-	815	746	-	-	-	-	-	-	-
Stage 2	809	745	-	829	757	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	10.9		10.2		0.1		0.1	
HCM LOS	B		B					





Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1410	-	-	640 696	1396	-	-
HCM Lane V/C Ratio	0.002	-	-	0.042 0.008	0.001	-	-
HCM Control Delay (s)	7.6	0	-	10.9 10.2	7.6	0	-
HCM Lane LOS	A	A	-	B B	A A	-	-
HCM 95th %tile Q(veh)	0	-	-	0.1 0	0	-	-

HCM 6th TWSC
4: Madison Avenue & Granite Alley

Existing
PM Peak

Intersection						
Int Delay, s/veh	0.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	2	2	135	1	3	117
Future Vol, veh/h	2	2	135	1	3	117
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	3	180	1	4	156
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	345	181	0	0	181	0
Stage 1	181	-	-	-	-	-
Stage 2	164	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	652	862	-	-	1394	-
Stage 1	850	-	-	-	-	-
Stage 2	865	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	650	862	-	-	1394	-
Mov Cap-2 Maneuver	650	-	-	-	-	-
Stage 1	850	-	-	-	-	-
Stage 2	862	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.9	0		0.2		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	- 741		1394	-	
HCM Lane V/C Ratio	-	- 0.007		0.003	-	
HCM Control Delay (s)	-	- 9.9		7.6	0	
HCM Lane LOS	-	- A		A	A	
HCM 95th %tile Q(veh)	-	- 0		0	-	

Intersection	
Intersection Delay, s/veh	11.9
Intersection LOS	B





Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	7	275	154	16	112	11	66	7	15	13	17	5
Future Vol, veh/h	7	275	154	16	112	11	66	7	15	13	17	5
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	8	313	175	18	127	13	75	8	17	15	19	6
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	13.4	9.2	9.5	9
HCM LOS	B	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	75%	2%	12%	37%
Vol Thru, %	8%	63%	81%	49%
Vol Right, %	17%	35%	8%	14%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	88	436	139	35
LT Vol	66	7	16	13
Through Vol	7	275	112	17
RT Vol	15	154	11	5
Lane Flow Rate	100	495	158	40
Geometry Grp	1	1	1	1
Degree of Util (X)	0.152	0.591	0.211	0.061
Departure Headway (Hd)	5.472	4.293	4.816	5.52
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	650	837	741	643
Service Time	3.548	2.332	2.873	3.607
HCM Lane V/C Ratio	0.154	0.591	0.213	0.062
HCM Control Delay	9.5	13.4	9.2	9
HCM Lane LOS	A	B	A	A
HCM 95th-tile Q	0.5	4	0.8	0.2

Intersection

Int Delay, s/veh 2.6

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	33	0	21	0	0	0	32	55	0	0	139	48
Future Vol, veh/h	33	0	21	0	0	0	32	55	0	0	139	48
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	44	0	28	0	0	0	43	73	0	0	185	64




Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	376	376	217	390	408	73	249	0	0	73	0	0
Stage 1	217	217	-	159	159	-	-	-	-	-	-	-
Stage 2	159	159	-	231	249	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	581	555	823	569	533	989	1317	-	-	1527	-	-
Stage 1	785	723	-	843	766	-	-	-	-	-	-	-
Stage 2	843	766	-	772	701	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	566	536	823	535	515	989	1317	-	-	1527	-	-
Mov Cap-2 Maneuver	566	536	-	535	515	-	-	-	-	-	-	-
Stage 1	758	723	-	814	740	-	-	-	-	-	-	-
Stage 2	814	740	-	746	701	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	11.3		0		2.9		0	
HCM LOS	B		A					





Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1317	-	-	644	-	1527	-
HCM Lane V/C Ratio	0.032	-	-	0.112	-	-	-
HCM Control Delay (s)	7.8	0	-	11.3	0	0	-
HCM Lane LOS	A	A	-	B	A	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0.4	-	0	-

HCM 6th TWSC
4: Madison Avenue & Granite Alley

2026 Background
AM Peak





Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	2	85	1	1	159
Future Vol, veh/h	1	2	85	1	1	159
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	3	113	1	1	212
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	328	114	0	0	114	0
Stage 1	114	-	-	-	-	-
Stage 2	214	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	666	939	-	-	1475	-
Stage 1	911	-	-	-	-	-
Stage 2	822	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	665	939	-	-	1475	-
Mov Cap-2 Maneuver	665	-	-	-	-	-
Stage 1	911	-	-	-	-	-
Stage 2	821	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.4	0		0		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	826	1475	-	
HCM Lane V/C Ratio	-	-	0.005	0.001	-	
HCM Control Delay (s)	-	-	9.4	7.4	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0	0	-	

Intersection	
Intersection Delay, s/veh	13.1
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	17	235	80	9	225	51	117	24	11	53	41	12
Future Vol, veh/h	17	235	80	9	225	51	117	24	11	53	41	12
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	19	267	91	10	256	58	133	27	13	60	47	14
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	14.4	13.2	11.8	10.9
HCM LOS	B	B	B	B

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	77%	5%	3%	50%
Vol Thru, %	16%	71%	79%	39%
Vol Right, %	7%	24%	18%	11%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	152	332	285	106
LT Vol	117	17	9	53
Through Vol	24	235	225	41
RT Vol	11	80	51	12
Lane Flow Rate	173	377	324	120
Geometry Grp	1	1	1	1
Degree of Util (X)	0.294	0.547	0.479	0.207
Departure Headway (Hd)	6.127	5.215	5.322	6.174
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	584	690	674	578
Service Time	4.195	3.27	3.379	4.248
HCM Lane V/C Ratio	0.296	0.546	0.481	0.208
HCM Control Delay	11.8	14.4	13.2	10.9
HCM Lane LOS	B	B	B	B
HCM 95th-tile Q	1.2	3.3	2.6	0.8

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	16	0	4	0	0	0	3	136	0	0	119	11
Future Vol, veh/h	16	0	4	0	0	0	3	136	0	0	119	11
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	21	0	5	0	0	0	4	181	0	0	159	15
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	356	356	167	358	363	181	174	0	0	181	0	0
Stage 1	167	167	-	189	189	-	-	-	-	-	-	-
Stage 2	189	189	-	169	174	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	599	570	877	597	565	862	1403	-	-	1394	-	-
Stage 1	835	760	-	813	744	-	-	-	-	-	-	-
Stage 2	813	744	-	833	755	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	598	568	877	592	563	862	1403	-	-	1394	-	-
Mov Cap-2 Maneuver	598	568	-	592	563	-	-	-	-	-	-	-
Stage 1	832	760	-	811	742	-	-	-	-	-	-	-
Stage 2	811	742	-	828	755	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	10.9		0		0.2		0					
HCM LOS	B		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1403	-	-	639	-	1394	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.042	-	-	-	-				
HCM Control Delay (s)	7.6	0	-	10.9	0	0	-	-				
HCM Lane LOS	A	A	-	B	A	A	-	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	-	0	-	-				

HCM 6th TWSC
4: Madison Avenue & Granite Alley





2026 Background
PM Peak

Intersection						
Int Delay, s/veh	0.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	2	2	137	1	3	120
Future Vol, veh/h	2	2	137	1	3	120
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	3	183	1	4	160
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	352	184	0	0	184	0
Stage 1	184	-	-	-	-	-
Stage 2	168	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	646	858	-	-	1391	-
Stage 1	848	-	-	-	-	-
Stage 2	862	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	644	858	-	-	1391	-
Mov Cap-2 Maneuver	644	-	-	-	-	-
Stage 1	848	-	-	-	-	-
Stage 2	859	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.9	0		0.2		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	736	1391	-	
HCM Lane V/C Ratio	-	-	0.007	0.003	-	
HCM Control Delay (s)	-	-	9.9	7.6	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0	0	-	

HCM 6th AWSC
1: Madison Avenue & Main Street





2026 Total
AM Peak

Intersection	
Intersection Delay, s/veh	12.2
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	7	275	159	27	112	11	71	8	25	13	18	5
Future Vol, veh/h	7	275	159	27	112	11	71	8	25	13	18	5
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	8	313	181	31	127	13	81	9	28	15	20	6
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	14	9.5	9.8	9.1
HCM LOS	B	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	68%	2%	18%	36%
Vol Thru, %	8%	62%	75%	50%
Vol Right, %	24%	36%	7%	14%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	104	441	150	36
LT Vol	71	7	27	13
Through Vol	8	275	112	18
RT Vol	25	159	11	5
Lane Flow Rate	118	501	170	41
Geometry Grp	1	1	1	1
Degree of Util (X)	0.18	0.607	0.232	0.065
Departure Headway (Hd)	5.471	4.361	4.903	5.711
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	649	822	726	631
Service Time	3.561	2.412	2.975	3.711
HCM Lane V/C Ratio	0.182	0.609	0.234	0.065
HCM Control Delay	9.8	14	9.5	9.1
HCM Lane LOS	A	B	A	A
HCM 95th-tile Q	0.7	4.2	0.9	0.2

Intersection												
Int Delay, s/veh	2.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	33	0	21	0	0	0	32	71	0	0	156	48
Future Vol, veh/h	33	0	21	0	0	0	32	71	0	0	156	48
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	44	0	28	0	0	0	43	95	0	0	208	64
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	421	421	240	435	453	95	272	0	0	95	0	0
Stage 1	240	240	-	181	181	-	-	-	-	-	-	-
Stage 2	181	181	-	254	272	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	543	524	799	531	503	962	1291	-	-	1499	-	-
Stage 1	763	707	-	821	750	-	-	-	-	-	-	-
Stage 2	821	750	-	750	685	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	528	506	799	499	485	962	1291	-	-	1499	-	-
Mov Cap-2 Maneuver	528	506	-	499	485	-	-	-	-	-	-	-
Stage 1	736	707	-	792	724	-	-	-	-	-	-	-
Stage 2	792	724	-	724	685	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	11.7		0			2.4			0			
HCM LOS	B		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1291	-	-	608	-	1499	-	-				
HCM Lane V/C Ratio	0.033	-	-	0.118	-	-	-	-				
HCM Control Delay (s)	7.9	0	-	11.7	0	0	-	-				
HCM Lane LOS	A	A	-	B	A	A	-	-				
HCM 95th %tile Q(veh)	0.1	-	-	0.4	-	0	-	-				




HCM 6th TWSC
3: Madison Avenue & West Site Access (South)

2026 Total
AM Peak

Intersection						
Int Delay, s/veh	1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	16	87	1	17	160
Future Vol, veh/h	1	16	87	1	17	160
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	21	116	1	23	213
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	376	117	0	0	117	0
Stage 1	117	-	-	-	-	-
Stage 2	259	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	625	935	-	-	1471	-
Stage 1	908	-	-	-	-	-
Stage 2	784	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	614	935	-	-	1471	-
Mov Cap-2 Maneuver	614	-	-	-	-	-
Stage 1	908	-	-	-	-	-
Stage 2	770	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.1	0		0.7		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	907	1471	-	
HCM Lane V/C Ratio	-	-	0.025	0.015	-	
HCM Control Delay (s)	-	-	9.1	7.5	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

HCM 6th TWSC
4: Madison Avenue & Granite Alley





2026 Total
AM Peak

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	2	86	1	1	160
Future Vol, veh/h	1	2	86	1	1	160
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	3	115	1	1	213
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	331	116	0	0	116	0
Stage 1	116	-	-	-	-	-
Stage 2	215	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	664	936	-	-	1473	-
Stage 1	909	-	-	-	-	-
Stage 2	821	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	663	936	-	-	1473	-
Mov Cap-2 Maneuver	663	-	-	-	-	-
Stage 1	909	-	-	-	-	-
Stage 2	820	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	9.4	0	0			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	823	1473	-	
HCM Lane V/C Ratio	-	-	0.005	0.001	-	
HCM Control Delay (s)	-	-	9.4	7.4	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0	0	-	

HCM 6th AWSC
1: Madison Avenue & Main Street

2026 Total
PM Peak

Intersection	
Intersection Delay, s/veh	13.8
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	17	235	86	22	225	51	121	25	19	53	42	12
Future Vol, veh/h	17	235	86	22	225	51	121	25	19	53	42	12
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	19	267	98	25	256	58	138	28	22	60	48	14
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	15.1	14.1	12.2	11.1
HCM LOS	C	B	B	B

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	73%	5%	7%	50%
Vol Thru, %	15%	70%	76%	39%
Vol Right, %	12%	25%	17%	11%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	165	338	298	107
LT Vol	121	17	22	53
Through Vol	25	235	225	42
RT Vol	19	86	51	12
Lane Flow Rate	188	384	339	122
Geometry Grp	1	1	1	1
Degree of Util (X)	0.322	0.566	0.51	0.213
Departure Headway (Hd)	6.183	5.305	5.422	6.3
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	578	676	660	565
Service Time	4.26	3.368	3.487	4.385
HCM Lane V/C Ratio	0.325	0.568	0.514	0.216
HCM Control Delay	12.2	15.1	14.1	11.1
HCM Lane LOS	B	C	B	B
HCM 95th-tile Q	1.4	3.6	2.9	0.8

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	16	0	4	0	0	0	3	149	0	0	139	11
Future Vol, veh/h	16	0	4	0	0	0	3	149	0	0	139	11
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	21	0	5	0	0	0	4	199	0	0	185	15
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	400	400	193	402	407	199	200	0	0	199	0	0
Stage 1	193	193	-	207	207	-	-	-	-	-	-	-
Stage 2	207	207	-	195	200	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	560	538	849	559	533	842	1372	-	-	1373	-	-
Stage 1	809	741	-	795	731	-	-	-	-	-	-	-
Stage 2	795	731	-	807	736	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	559	536	849	554	531	842	1372	-	-	1373	-	-
Mov Cap-2 Maneuver	559	536	-	554	531	-	-	-	-	-	-	-
Stage 1	807	741	-	793	729	-	-	-	-	-	-	-
Stage 2	793	729	-	802	736	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	11.3		0			0.2			0			
HCM LOS	B		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1372	-	-	600	-	1373	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.044	-	-	-	-				
HCM Control Delay (s)	7.6	0	-	11.3	0	0	-	-				
HCM Lane LOS	A	A	-	B	A	A	-	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	-	0	-	-				




HCM 6th TWSC
3: Madison Avenue & West Site Access (South)

2026 Total
PM Peak

Intersection						
Int Delay, s/veh	1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	13	139	1	20	123
Future Vol, veh/h	1	13	139	1	20	123
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	17	185	1	27	164
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	404	186	0	0	186	0
Stage 1	186	-	-	-	-	-
Stage 2	218	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	603	856	-	-	1388	-
Stage 1	846	-	-	-	-	-
Stage 2	818	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	590	856	-	-	1388	-
Mov Cap-2 Maneuver	590	-	-	-	-	-
Stage 1	846	-	-	-	-	-
Stage 2	801	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.4	0		1.1		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	829	1388	-	
HCM Lane V/C Ratio	-	-	0.023	0.019	-	
HCM Control Delay (s)	-	-	9.4	7.6	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0.1	-	

HCM 6th TWSC
4: Madison Avenue & Granite Alley

2026 Total
PM Peak

Intersection						
Int Delay, s/veh	0.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	2	2	138	1	3	121
Future Vol, veh/h	2	2	138	1	3	121
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	3	184	1	4	161

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	354	185	0	0	185
Stage 1	185	-	-	-	-
Stage 2	169	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	644	857	-	-	1390
Stage 1	847	-	-	-	-
Stage 2	861	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	642	857	-	-	1390
Mov Cap-2 Maneuver	642	-	-	-	-
Stage 1	847	-	-	-	-
Stage 2	858	-	-	-	-





Approach	WB	NB	SB
HCM Control Delay, s	9.9	0	0.2
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	734	1390
HCM Lane V/C Ratio	-	-	0.007	0.003
HCM Control Delay (s)	-	-	9.9	7.6
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0	0

HCM 6th AWSC
1: Madison Avenue & Main Street





2045 Background
AM Peak

Intersection	
Intersection Delay, s/veh	15.8
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	9	330	186	20	135	14	80	9	18	16	21	6
Future Vol, veh/h	9	330	186	20	135	14	80	9	18	16	21	6
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	375	211	23	153	16	91	10	20	18	24	7
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	19.2	10.1	10.4	9.6
HCM LOS	C	B	B	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	75%	2%	12%	37%
Vol Thru, %	8%	63%	80%	49%
Vol Right, %	17%	35%	8%	14%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	107	525	169	43
LT Vol	80	9	20	16
Through Vol	9	330	135	21
RT Vol	18	186	14	6
Lane Flow Rate	122	597	192	49
Geometry Grp	1	1	1	1
Degree of Util (X)	0.201	0.738	0.276	0.082
Departure Headway (Hd)	5.939	4.451	5.169	6.05
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	607	805	699	594
Service Time	3.952	2.535	3.169	4.068
HCM Lane V/C Ratio	0.201	0.742	0.275	0.082
HCM Control Delay	10.4	19.2	10.1	9.6
HCM Lane LOS	B	C	B	A
HCM 95th-tile Q	0.7	6.7	1.1	0.3

Intersection												
Int Delay, s/veh	2.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	33	0	21	0	0	0	32	74	0	0	179	48
Future Vol, veh/h	33	0	21	0	0	0	32	74	0	0	179	48
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	44	0	28	0	0	0	43	99	0	0	239	64
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	456	456	271	470	488	99	303	0	0	99	0	0
Stage 1	271	271	-	185	185	-	-	-	-	-	-	-
Stage 2	185	185	-	285	303	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	515	501	768	504	480	957	1258	-	-	1494	-	-
Stage 1	735	685	-	817	747	-	-	-	-	-	-	-
Stage 2	817	747	-	722	664	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	501	483	768	472	463	957	1258	-	-	1494	-	-
Mov Cap-2 Maneuver	501	483	-	472	463	-	-	-	-	-	-	-
Stage 1	709	685	-	788	720	-	-	-	-	-	-	-
Stage 2	788	720	-	696	664	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	12.1		0			2.4			0			
HCM LOS	B		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1258	-	-	579	-	1494	-	-				
HCM Lane V/C Ratio	0.034	-	-	0.124	-	-	-	-				
HCM Control Delay (s)	8	0	-	12.1	0	0	-	-				
HCM Lane LOS	A	A	-	B	A	A	-	-				
HCM 95th %tile Q(veh)	0.1	-	-	0.4	-	0	-	-				

HCM 6th TWSC
4: Madison Avenue & Granite Alley

2045 Background
AM Peak

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	3	5	101	3	3	197
Future Vol, veh/h	3	5	101	3	3	197
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	4	7	135	4	4	263

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	408	137	0	0	139
Stage 1	137	-	-	-	-
Stage 2	271	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	599	911	-	-	1445
Stage 1	890	-	-	-	-
Stage 2	775	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	597	911	-	-	1445
Mov Cap-2 Maneuver	597	-	-	-	-
Stage 1	890	-	-	-	-
Stage 2	773	-	-	-	-





Approach	WB	NB	SB
HCM Control Delay, s	9.8	0	0.1
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	761	1445
HCM Lane V/C Ratio	-	-	0.014	0.003
HCM Control Delay (s)	-	-	9.8	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0	0

HCM 6th AWSC
1: Madison Avenue & Main Street

2045 Background
PM Peak

Intersection	
Intersection Delay, s/veh	19.4
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	21	285	96	11	270	62	142	30	14	64	49	15
Future Vol, veh/h	21	285	96	11	270	62	142	30	14	64	49	15
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	24	324	109	13	307	70	161	34	16	73	56	17
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	23.7	19.4	14.7	12.9
HCM LOS	C	C	B	B

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	76%	5%	3%	50%
Vol Thru, %	16%	71%	79%	38%
Vol Right, %	8%	24%	18%	12%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	186	402	343	128
LT Vol	142	21	11	64
Through Vol	30	285	270	49
RT Vol	14	96	62	15
Lane Flow Rate	211	457	390	145
Geometry Grp	1	1	1	1
Degree of Util (X)	0.406	0.739	0.646	0.285
Departure Headway (Hd)	6.907	5.827	5.97	7.049
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	520	621	604	508
Service Time	4.97	3.878	4.025	5.12
HCM Lane V/C Ratio	0.406	0.736	0.646	0.285
HCM Control Delay	14.7	23.7	19.4	12.9
HCM Lane LOS	B	C	C	B
HCM 95th-tile Q	2	6.4	4.7	1.2

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	16	0	4	0	0	0	3	170	0	0	145	11
Future Vol, veh/h	16	0	4	0	0	0	3	170	0	0	145	11
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	21	0	5	0	0	0	4	227	0	0	193	15
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	436	436	201	438	443	227	208	0	0	227	0	0
Stage 1	201	201	-	235	235	-	-	-	-	-	-	-
Stage 2	235	235	-	203	208	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	531	514	840	529	509	812	1363	-	-	1341	-	-
Stage 1	801	735	-	768	710	-	-	-	-	-	-	-
Stage 2	768	710	-	799	730	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	530	512	840	524	507	812	1363	-	-	1341	-	-
Mov Cap-2 Maneuver	530	512	-	524	507	-	-	-	-	-	-	-
Stage 1	799	735	-	766	708	-	-	-	-	-	-	-
Stage 2	766	708	-	794	730	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	11.6		0		0.1		0					
HCM LOS	B		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1363	-	-	572	-	1341	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.047	-	-	-	-				
HCM Control Delay (s)	7.6	0	-	11.6	0	0	-	-				
HCM Lane LOS	A	A	-	B	A	A	-	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	-	0	-	-				

HCM 6th TWSC
4: Madison Avenue & Granite Alley

2045 Background
PM Peak

Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	5	5	168	3	5	144
Future Vol, veh/h	5	5	168	3	5	144
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	7	224	4	7	192

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	432	226	0	0	228
Stage 1	226	-	-	-	-
Stage 2	206	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	581	813	-	-	1340
Stage 1	812	-	-	-	-
Stage 2	829	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	578	813	-	-	1340
Mov Cap-2 Maneuver	578	-	-	-	-
Stage 1	812	-	-	-	-
Stage 2	824	-	-	-	-





Approach	WB	NB	SB
HCM Control Delay, s	10.4	0	0.3
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	676	1340
HCM Lane V/C Ratio	-	-	0.02	0.005
HCM Control Delay (s)	-	-	10.4	7.7
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.1	0

HCM 6th AWSC
1: Madison Avenue & Main Street

2045 Total
AM Peak

Intersection	
Intersection Delay, s/veh	17.2
Intersection LOS	C





Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	9	330	191	31	135	14	85	10	28	16	22	6
Future Vol, veh/h	9	330	191	31	135	14	85	10	28	16	22	6
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	10	375	217	35	153	16	97	11	32	18	25	7
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	21.6	10.5	10.8	9.8
HCM LOS	C	B	B	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	69%	2%	17%	36%
Vol Thru, %	8%	62%	75%	50%
Vol Right, %	23%	36%	8%	14%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	123	530	180	44
LT Vol	85	9	31	16
Through Vol	10	330	135	22
RT Vol	28	191	14	6
Lane Flow Rate	140	602	205	50
Geometry Grp	1	1	1	1
Degree of Util (X)	0.232	0.774	0.299	0.086
Departure Headway (Hd)	5.964	4.625	5.261	6.168
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	601	789	683	580
Service Time	4.011	2.625	3.298	4.222
HCM Lane V/C Ratio	0.233	0.763	0.3	0.086
HCM Control Delay	10.8	21.6	10.5	9.8
HCM Lane LOS	B	C	B	A
HCM 95th-tile Q	0.9	7.6	1.3	0.3

Intersection

Int Delay, s/veh 2.2

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	33	0	21	0	0	0	32	90	0	0	196	48
Future Vol, veh/h	33	0	21	0	0	0	32	90	0	0	196	48
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	44	0	28	0	0	0	43	120	0	0	261	64




Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	499	499	293	513	531	120	325	0	0	120	0	0
Stage 1	293	293	-	206	206	-	-	-	-	-	-	-
Stage 2	206	206	-	307	325	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	482	473	746	472	454	931	1235	-	-	1468	-	-
Stage 1	715	670	-	796	731	-	-	-	-	-	-	-
Stage 2	796	731	-	703	649	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	469	455	746	441	437	931	1235	-	-	1468	-	-
Mov Cap-2 Maneuver	469	455	-	441	437	-	-	-	-	-	-	-
Stage 1	689	670	-	767	704	-	-	-	-	-	-	-
Stage 2	767	704	-	677	649	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	12.6		0		2.1		0	
HCM LOS	B		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1235	-	-	548	-	1468	-
HCM Lane V/C Ratio	0.035	-	-	0.131	-	-	-
HCM Control Delay (s)	8	0	-	12.6	0	0	-
HCM Lane LOS	A	A	-	B	A	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0.5	-	0	-




HCM 6th TWSC
3: Madison Avenue & West Site Access (South)

2045 Total
AM Peak

Intersection						
Int Delay, s/veh	0.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	16	106	1	17	200
Future Vol, veh/h	1	16	106	1	17	200
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	21	141	1	23	267
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	455	142	0	0	142	0
Stage 1	142	-	-	-	-	-
Stage 2	313	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	563	906	-	-	1441	-
Stage 1	885	-	-	-	-	-
Stage 2	741	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	552	906	-	-	1441	-
Mov Cap-2 Maneuver	552	-	-	-	-	-
Stage 1	885	-	-	-	-	-
Stage 2	727	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.2	0		0.6		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	873	1441	-	
HCM Lane V/C Ratio	-	-	0.026	0.016	-	
HCM Control Delay (s)	-	-	9.2	7.5	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

HCM 6th TWSC
4: Madison Avenue & Granite Alley





2045 Total
AM Peak

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	3	5	102	3	3	198
Future Vol, veh/h	3	5	102	3	3	198
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	4	7	136	4	4	264
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	410	138	0	0	140	0
Stage 1	138	-	-	-	-	-
Stage 2	272	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	598	910	-	-	1443	-
Stage 1	889	-	-	-	-	-
Stage 2	774	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	596	910	-	-	1443	-
Mov Cap-2 Maneuver	596	-	-	-	-	-
Stage 1	889	-	-	-	-	-
Stage 2	772	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.8	0		0.1		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	- 760		1443	-	
HCM Lane V/C Ratio	-	- 0.014		0.003	-	
HCM Control Delay (s)	-	- 9.8		7.5	0	
HCM Lane LOS	-	- A		A	A	
HCM 95th %tile Q(veh)	-	- 0		0	-	

HCM 6th AWSC
1: Madison Avenue & Main Street





2045 Total
PM Peak

Intersection	
Intersection Delay, s/veh	21.2
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	21	285	102	24	270	62	146	31	22	64	50	15
Future Vol, veh/h	21	285	102	24	270	62	146	31	22	64	50	15
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	24	324	116	27	307	70	166	35	25	73	57	17
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	26.1	21.6	15.5	13.3
HCM LOS	D	C	C	B

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	73%	5%	7%	50%
Vol Thru, %	16%	70%	76%	39%
Vol Right, %	11%	25%	17%	12%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	199	408	356	129
LT Vol	146	21	24	64
Through Vol	31	285	270	50
RT Vol	22	102	62	15
Lane Flow Rate	226	464	405	147
Geometry Grp	1	1	1	1
Degree of Util (X)	0.44	0.767	0.686	0.294
Departure Headway (Hd)	7.007	5.959	6.109	7.231
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	511	607	591	494
Service Time	5.082	4.017	4.171	5.317
HCM Lane V/C Ratio	0.442	0.764	0.685	0.298
HCM Control Delay	15.5	26.1	21.6	13.3
HCM Lane LOS	C	D	C	B
HCM 95th-tile Q	2.2	7	5.3	1.2

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	16	0	4	0	0	0	3	183	0	0	165	11
Future Vol, veh/h	16	0	4	0	0	0	3	183	0	0	165	11
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	75	75	75	75	75	75	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	21	0	5	0	0	0	4	244	0	0	220	15
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	480	480	228	482	487	244	235	0	0	244	0	0
Stage 1	228	228	-	252	252	-	-	-	-	-	-	-
Stage 2	252	252	-	230	235	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	496	485	811	495	481	795	1332	-	-	1322	-	-
Stage 1	775	715	-	752	698	-	-	-	-	-	-	-
Stage 2	752	698	-	773	710	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	495	484	811	491	480	795	1332	-	-	1322	-	-
Mov Cap-2 Maneuver	495	484	-	491	480	-	-	-	-	-	-	-
Stage 1	773	715	-	750	696	-	-	-	-	-	-	-
Stage 2	750	696	-	768	710	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	12.1		0		0.1		0					
HCM LOS	B		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1332	-	-	537	-	1322	-	-				
HCM Lane V/C Ratio	0.003	-	-	0.05	-	-	-	-				
HCM Control Delay (s)	7.7	0	-	12.1	0	0	-	-				
HCM Lane LOS	A	A	-	B	A	A	-	-				
HCM 95th %tile Q(veh)	0	-	-	0.2	-	0	-	-				




HCM 6th TWSC
3: Madison Avenue & West Site Access (South)

2045 Total
PM Peak

Intersection						
Int Delay, s/veh	0.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	13	173	1	20	149
Future Vol, veh/h	1	13	173	1	20	149
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	17	231	1	27	199
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	485	232	0	0	232	0
Stage 1	232	-	-	-	-	-
Stage 2	253	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	541	807	-	-	1336	-
Stage 1	807	-	-	-	-	-
Stage 2	789	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	529	807	-	-	1336	-
Mov Cap-2 Maneuver	529	-	-	-	-	-
Stage 1	807	-	-	-	-	-
Stage 2	771	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9.7	0		0.9		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	- 778		1336	-	
HCM Lane V/C Ratio	-	- 0.024		0.02	-	
HCM Control Delay (s)	-	- 9.7		7.7	0	
HCM Lane LOS	-	- A		A	A	
HCM 95th %tile Q(veh)	-	- 0.1		0.1	-	

HCM 6th TWSC
4: Madison Avenue & Granite Alley

2045 Total
PM Peak

Intersection						
Int Delay, s/veh	0.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	5	5	169	3	5	145
Future Vol, veh/h	5	5	169	3	5	145
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	7	225	4	7	193
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	434	227	0	0	229	0
Stage 1	227	-	-	-	-	-
Stage 2	207	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	579	812	-	-	1339	-
Stage 1	811	-	-	-	-	-
Stage 2	828	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	576	812	-	-	1339	-
Mov Cap-2 Maneuver	576	-	-	-	-	-
Stage 1	811	-	-	-	-	-
Stage 2	823	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	10.4	0		0.3		
HCM LOS	B					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	-	674	1339	-	
HCM Lane V/C Ratio	-	-	0.02	0.005	-	
HCM Control Delay (s)	-	-	10.4	7.7	0	
HCM Lane LOS	-	-	B	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	