



ARCHITECTURE

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April 25th, 2025

To: Town of Frisco

PO Box 4100 Frisco, CO 80443

Re: Prime Sauce

20 East Main Street . Frisco . CO

DESCRIPTION OF PROJECT

Prime Sauce is a new project on the southeast corner of Madison Avenue and Frisco's Main Street. The Granite Street Alley forms the southern boundary, with the Beaver Plaza Condos to the east. The current building which houses Frisco Prime will be removed, along with the current dirt parking lot. The large trees along Granite Street Alley will be protected wherever possible.

With distinctive styling and highly efficient floor plans, this project looks to continue the evolution of "Frisco Style" by picking up many of the cues in the surrounding architecture while introducing some new uses for classic materials. The use of outdoor decks on multiple levels not only keeps the street facades at the pedestrian level, it also allows the occupants to take in the full outdoor experience. With direct views of Mount Royal, Wichita, Chief and Buffalo Peaks as well as some distant views of Grey & Torreys, there are decks on all sides of the project, bringing the outside in and reducing project mass at the same time.

The project will consist of nine units, ranging from 785 sf to 2,156 sf of living space on the upper two levels. Five units are permitted by the base zoning with 4 bonus units, 2 of which will be deed restricted per the Town's standard restriction. The ground floor proposes a 3,555 sf restaurant & bar space, capable of seating 140 customers inside, and another 36 on the covered outdoor patio. A 7,542 sf parking garage containing 25 stalls provides all of the required parking for the residential units above. The units will have 2-3 bedrooms and 1-3.5 bathrooms, with an open office space also shown in Unit 1, 6 & 7. Window trim that blends with the surrounding siding, asymmetrical window patterns and metal siding all serve to give the architecture a current feel, while gable roofs, angle braces, real wood siding, and a brick base help the buildings connect with commercial buildings on Main Street and the homes traditionally found in the area.

The units are arranged on the site to provide the most view and sun exposure possible, while maintaining privacy between the different owners. Window placement along the walls has been designed to allow for great views of Mount Royal to the west, and Grays & Torreys Peaks to the east

There are a few bulk plane encroachments, totaling 342 cubic feet, but they are exclusively made up of the brick "fins" at the deck edges and the overframed gable roof elements. No living space is contained in any bulk plane encroachment, and the project is under the 350 CF limit for these encroachments.

Two elements that have been added to the project since the Planning Commission saw the Sketch Plan back in November, are the snow storage system on the upper-level decks, and the revitalization of the Right of Way space between the northern property line and the Main Street sidewalk. On the decks, a unique use of planter boxes as snow storage elements solves the problem of where snow on the upper-level decks will be shoveled. These boxes are 18" deep, and equipped with internal drainage systems to route the melting snow to the storm water system. Each deck has at least one dedicated snow storage area, meeting the required square footage for snow storage as outlined in the Town code.

In the space between the northern property line and the Main Street sidewalk, the project proposes to add four more trees, in addition to the required number of trees on site, along with a park bench on a small brick patio and a bike rack, all satisfying the Town's requirement to provide a community benefit. This area will also receive irrigation and native seeding, to help beautify a currently unused space that will be much improved with the proposed landscaping. A revocable license agreement has been discussed with the Town Engineer, who has indicated that the Town will be in favor of approval. This agreement will be in place prior to certificate of occupancy of the building.

This project is near the Madison Avenue bike path, is within easy walking distance of Main Street and several bus stops and makes perfect sense as a location for additional housing. The developers are excited to begin the redevelopment of this lot and look forward to incorporating the Planning Commission's comments into a final vision for the project.

Sincerely,

Andrew Stabile, AIA.
Allen-Guerra Architecture

PRIME SAUCE

EXTERIOR MATERIALS SCHEDULE DATE: 6 SEPTEMBER, 2024

| LABEL | ITEM | COLOR | DESCRIPTION |
|-------|---------------------------------|-------|--|
| МІ | ROOF - MEMBRANE | | SINGLE PLY ROOFING MEMBRANE EVERGUARD TPO COLOR "SLATE GRAY" |
| M2 | ROOF – METAL \$ CAP FLASHING | | STANDING SEAM METAL ROOF COLOR "DARK BRONZE" NON-REFLECTIVE |
| МЗ | FASCIA | | 2X CEDAR PER DETAIL – STAIN WITH SUPERDECK 2320 "CAPE BLACKWOOD" |
| M4 | SOFFIT | | Ix4, T&G CEDAR - STAIN WITH SUPERDECK 2 3 "GRANITE" |
| M5 | EXPOSED STEEL POSTS & BEAMS | | PAINTED STEEL PER DETAILS, COLOR TO BE GRAPHITE |
| MG | VERTICAL SIDING | | IxG, I/8" REVEAL T&G CEDAR – SPECIALTY WOOD PRODUCTS COLOR - "WELLSHIRE" |
| M7 | HORIZONTAL SIDING | | Ix8, 3/8" REVEAL T&G CEDAR – SPECIALTY WOOD PRODUCTS COLOR - "RINO RUSTIC" |

NOTE: ALL EXPOSED METAL INCLUDING, BUT NOT LIMITED TO, TYPICAL FLASHING, DOWNSPOUTS, GUTTERS, DRIP EDGE, VENT STACKS, FLUE PIPES, ETC, SHALL BE PAINTED TO BLEND UNABTRUSIVELY WITH ADJACENT SURFACES. $\begin{array}{c} \text{ALLEN-GLIERR} \\ \end{array}$

PRIME SAUCE

EXTERIOR MATERIALS SCHEDULE DATE: 6 SEPTEMBER, 2024

| LABEL | ITEM | COLOR | DESCRIPTION |
|-------|---------------------|-------|---|
| M8 | DOOR/WINDOW TRIM | | 2X CEDAR PER DETAIL – STAIN WITH SUPERDECK 2320 "CAPE BLACKWOOD" |
| M9 | DOORS/ WINDOWS | | JEL WEN WINDOW COMPANY ALUMINUM CLADDING COLOR – BLACK |
| MIO | EXPOSED TIMBER | | DOUG FIR # I STAIN WITH SUPERDECK 3504 "WOODRIDGE" |
| MII | DECKING | | BISON PORCELAIN TILE ON PEDESTAL SYSTEM COLOR-LIGHT GRAY |
| MI2 | GARAGE DOORS | | CEDAR STYLES & RAILS PER DETAILS — STAIN TO MATCH TRIM (M8) — WALLS PANELS TO MATCH (M I 5) FROSTED GLASS PANELS |
| MI3 | BRICK VENEER | | 2.5" TRUE BRICK SMOOTH FINISH DARK BROWN BRICK WITH LIGHT GREY GROUT |
| MI4 | STONE VENEER | | KANSAS PRAIRIE STONE PRAIRIE GREY DRY STACKED |

NOTE: ALL EXPOSED METAL INCLUDING, BUT NOT LIMITED TO, TYPICAL FLASHING, DOWNSPOUTS, GUTTERS, DRIP EDGE, VENT STACKS, FLUE PIPES, ETC, SHALL BE PAINTED TO BLEND UNABTRUSIVELY WITH ADJACENT SURFACES.

BUILDING TYPE AND OCCUPANCY DATA

ZONE DISTRICT: CENTRAL CORE ALLOWABLE DENSITY: 16 DU/ACRE MAXIMUM LOT COVERAGE:

FRONT YARD SETBACK: (PROVIDED=3.62') SIDE YARD SETBACK: (PROVIDED: 0.5' \$ 1.0') (PROVIDED: 0.83') REAR YARD SETBACK: MAXIMUM BUILDING HEIGHT: 40' (PITCHED) (PROPOSED: 39.0') 35' (FLAT) (PROPOSED: 34.9')

RESTAURANT SEATING 2,334 SF

RESTAURANT KITCHEN 1,221 SF RESIDENTIAL UNITS 13,399 SF GROUP S-2 PARKING GARAGE

CONSTRUCTION TYPE: TYPE VB CONSTRUCTION AUTOMATIC FIRE SPRINKLERS - NFPA 13

OCCUPANT LOADS
FIRST LEVEL RESTAURANT SEATING: (15 SF / OCC) 154 OCCUPANTS RESTAURANT KITCHEN: (200 SF / OCC) 7 OCCUPANTS (200 SF / OCC) 67 OCCUPANTS SECOND LEVEL UNITS: THIRD LEVEL UNITS: (200 SF / OCC) 65 OCCUPANTS

NOTES:

I. ALL WORK TO CONFORM TO CURRENT APPLICABLE CODES UNDER THE JURISDICTION OF THIS WORK INCLUDING:

2018 INTERNATIONAL BUILDING CODE (IBC) 2018 INTERNATIONAL PLUMBING CODE (IPC) 2018 INTERNATIONAL MECHANICAL CODE (IMC)

2018 INTERNATIONAL FUEL GAS CODE (IFGC) 202 I INTERNATIONAL ENERGY CONSERVATION CODE (IECC)

2017 NATIONAL ELECTRICAL CODE (NEC)

2017 ICC/ANSI AII7. I ACCESSIBILITY STANDARD 2. BUILDING IS TO BE EQUIPPED WITH AN AUTOMATIC SPRINKLER SYSTEM THROUGHOUT INSTALLED OER CHAPTER 9 OF THE

3. CHAPTER 7: FIRE RESISTANT RATED CONSTRUCTION

A. SECTION 708 CORRIDOR WALLS SHALL BE ONE-HOUR RATED PER WALL TYPE #6, SHEET A2.2.

B. SECTION 708 WALLS SEPARATING DWELLING UNITS IN THE SAME BUILDING SHALL BE ONE-HOUR RATED PER WALL TYPE C. SECTION 711 FLOOR-CEILING ASSEMBLIES SEPARATING DWELLING UNITS AND SEPARATING DWELLING UNITS FROM OTHER OCCUPANCIES IN THE SAME BUILDING SHALL BE ONE-HOUR RATED PER FLOOR-CEILING ASSEMBLY #12, SHEET

D. SECTION 717.5.3 SHAFT ENCLOSURES THAT ARE PERMITTED TO BE PENETRATED BY DUCTS AND AIR TRANSFER

OPENINGS SHALL BE PROTECTED WITH LISTED FIRE AND SMOKE DAMPERS - SEE MECHANICAL DRAWINGS. E. SECTION 7 | 8.2.4 STAIRWAYS FIRE BLOCKING SHALL BE PROVIDED IN CONCEALED SPACES BETWEEN STRINGERS AND AT THE TOP AND THE BOTTOM OF RUN

SITE CALCULATIONS

13,887 SF (0.32 ACRES) PROPOSED FOOTPRINT WITH DECKS & OVERHANGS: 11,841 SF (85.3% OF TOTAL SITE AREA) 775 SF (5.6% OF TOTAL SITE AREA) PROPOSED TOTAL DRIVES & SIDEWALKS PROPOSED SNOW STORAGE AREA FOR SITE 510 SF (65% OF DRIVE & SIDEWALK AREA) 12,616 SF (90.1% OF TOTAL SITE AREA) 2,049 SF 1,203 SF (59% OF TOTAL DECK AREA) PROPOSED UNCOVERED DECK AREA: PROPOSED DEDICATED DECK SNOW STORAGE:

BUILDING AREA CALCULATIONS

| UNIT # | BR | ВА | OFFICE | SF |
|---|-----------------------|------------------|---|--|
| SECOND LEVEL 201 202 203 204 205 | 3 3 3 2 2 | 3 3 3 2 2 | I | 2,080 1,900 2,036 785 785 |
| THIRD LEVEL 301 302 303 304 | 3 3 2 2 | 3 3 2 2 | 1 | 2,177 2,066 785 785 |
| TOTAL UNIT RESIDENTIAL: | 23 | | | 13,399 SF |
| COMMON RESIDENTIAL (V | | M PARKII | HALLWAYS: E CLOSETS: ECHANICAL: NG GARAGE: TRASH/REC) | 1,250 SF 646 SF 284 SF 7,542 SF |
| TOTAL COMMON: | | | | 9,722 SF |
| RESTAURANT | | | DINING: BAR: KITCHEN: | 1,328 SF 1,006 SF 964 SF |

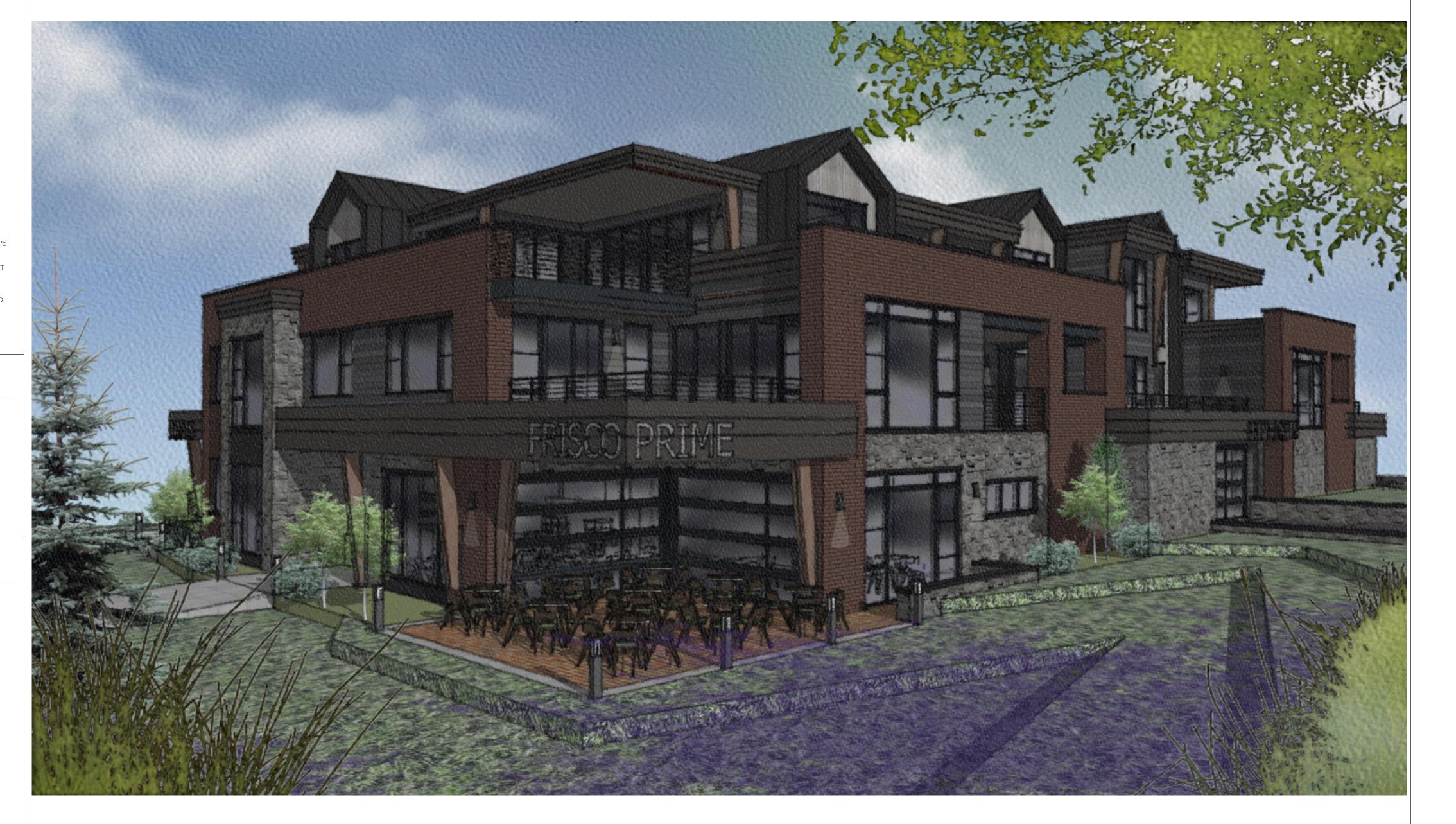
| | BATHROOMS: | 257 SF |
|----------------------|------------|-----------|
| TOTAL RESTAURANT: | | 3,555 SF |
| TOTAL BUILDING AREA: | | 26,676 SF |

PARKING CALCULATIONS

| BEDROOMS: VISITOR: (I PER 5 UNITS) | 23 |
|--|----|
| TOTAL REQUIRED: | 24 |

PRIME SAUCE

LOTS 9-12. BLOCK 2. KING SOLOMON SUBDIVISION #2 20 EAST MAIN STREET. TOWN OF FRISCO. COLORADO



PROJECT DIRECTORY

SILVERTHORNE . COLORADO . 80498

T: 970.468.6281

| OWNER 20EMAIN LLC PO BOX 2577 FRISCO . COLORADO . 80443 T: 970.406.1669 | GENERAL CONTRACTOR MOUNTAIN BUILDING SOLUTIONS PO BOX 2577 FRISCO . COLORADO 80443 T: 970.406.1669 |
|--|--|
| ARCHITECT ALLEN-GUERRA ARCHITECTURE 7 B GRANITE ST PO BOX 5540 FRISCO . COLORADO . 80443 T: 970.453.7002 | CIVIL ENGINEER TEN MILE ENGINEERING INC. PO BOX 1785 FRISCO . COLORADO . 80443 T: 970.485.5773 |
| STRUCTURAL ENGINEER G3 COSULTING LLC PO BOX 2933 BRECKENRIDGE . COLORADO . 80424 T: 970.485.2073 | MEP ENGINEER TIMBERLAKE ENGINEERING LLC 35 W. MAIN ST. #5657 FRISCO . COLORADO . 80443 T: 573.881.5684 |
| SURVEYOR RANGE WEST ENGINEERS & SURVEYORS, INC PO BOX 589 | |

LOCATION MAP



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| ISSUE: | DATE: | | | |
|-----------------|-------------|--|--|--|
| PRELIM | 28 APR 2022 | | | |
| DESIGN | 15 FEB 2024 | | | |
| DRC | 2 APR 2024 | | | |
| UPDATE | 8 MAY 2024 | | | |
| SKETCH | 6 SEP 2024 | | | |
| REVIEW | 6 DEC 2024 | | | |
| PLANNING | 24 JAN 2025 | | | |
| UPDATE | 24 APR 2025 | | | |
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| PROJECT #: 2233 | | | | |





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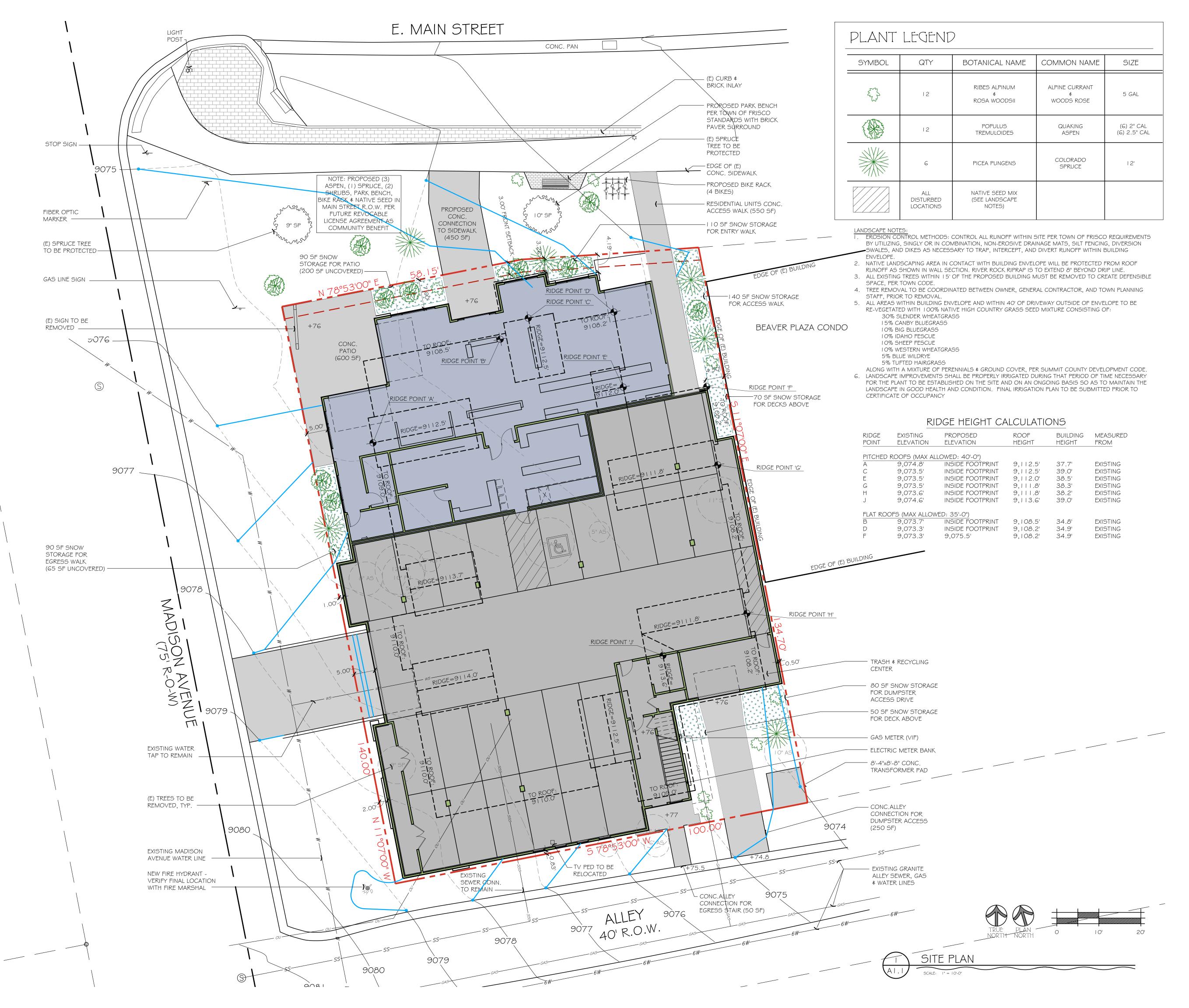
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PKII IL JAO, L LOT 9, 10, II, 12 BLOCK 2. KING SOLOMON SUBDIVISION #2 20 FAST MAIN STREET. TOWN OF FRISCO. COLORADO

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LOT 9, 10, 11, 12 BLOCK 2. KING SOLOMON SUBDIVISION #2
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CONSTRUCTION MANAGEMENT PL

| ISSUE: | DATE: |
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| PRELIM | 28 APR 2022 |
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PROJECT #: 2233



OVERALL GENERAL NOTES:

I. THE CONTRACTOR SHALL OBTAIN, AT HIS EXPENSE, ALL PERMITS WHICH ARE NECESSARY TO PERFORM THE PROPOSED WORK.

2. TRENCHES SHALL BE EXCAVATED AND THE PIPE EXPOSED FOR INSPECTION AT ANY LOCATION ON THE PROJECT IF SO ORDERED.

3. ALL STREET STATIONING IS ALONG THE CENTERLINE OF THE ROADWAY UNLESS OTHERWISE NOTED. FOR SEPARATE WATER & SANITARY SEWER PLANS THE STATIONING IS ALONG THE CENTERLINE OF THE PIPE

4. THE PROFILE GRADE ON THE PLANS IS ALONG THE ROADWAY CENTERLINE UNLESS OTHERWISE NOTED.

5. THE CONTRACTOR SHALL HAVE ON HIS POSSESSION AT THE SITE A COPY OF THE APPROVED CONSTRUCTION PLANS.

6. LIMITS OF WORK: NO AREAS SHALL BE DISTURBED OUTSIDE OF THE TEMPORARY CONSTRUCTION EASEMENTS AND THE ROADWAY DISTURBANCE LIMITS.

7. ALL CONSTRUCTION SHALL CONFORM TO THE TOWN OF FRISCO STANDARDS AND SPECIFICATIONS AS APPLICABLE. ALL WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE DEVELOPER, SUMMIT COUNTY, OR THEIR REPRESENTATIVES. ONE OR ALL OF THE PARTIES HAS THE RIGHT TO REJECT MATERIALS AND WORKMANSHIP WHICH DO NOT CONFORM TO SPECIFICATIONS.

8. THE CONTRACTOR SHALL NOTIFY THE TOWN OF FRISCO AND THE PUBLIC UTILITY COMPANIES PRIOR TO PROCEEDING WITH ANY EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ANY EXISTING UTILITY (INCLUDING DEPTHS) WHICH MAY CONFLICT WITH THE PROPOSED CONSTRUCTION. ALL EXISTING UTILITIES SHALL BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. DAMAGED UTILITIES SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE. ALL ITEMS SHOWN ON THE PLANS AS EXISTING ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. THE ACTUAL LOCATIONS MAY VARY FROM THE PLANS, ESPECIALLY IN THE CASE OF UNDERGROUND UTILITIES. WHENEVER THE CONTRACTOR DISCOVERS A DISCREPANCY IN LOCATIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY. ALL WORK PERFORMED IN THE AREA OF THE PUBLIC UTILITIES SHALL BE PERFORMED ACCORDING TO THE REQUIREMENTS OF THESE AGENCIES

9. CONTRACTOR SHALL GIVE 48 HOURS NOTICE TO TOWN OF FRISCO PERSONNEL TO PERFORM REQUIRED NSPECTIONS AND PRIOR TO ANY CONSTRUCTION ON THIS SITE. 10. ALL EXCAVATION SHALL COMPLY WITH OSHA SAFETY REGULATIONS.

11. CONTRACTOR SHALL OBTAIN APPROVAL FOR ALL TRAFFIC CONTROL AND ROAD/ALLEY REQUIREMENTS NECESSARY FROM THE TOWN OF FRISCO. NO ROAD/ALLEY CLOSURES MAY OCCUR WITHOUT APPROVAL AND NOTIFICATION OF TOWN OF FRISCO AND THE FIRE DEPARTMENT. 12. CONTRACTOR SHALL OBTAIN APPROVAL FOR ALL CONSTRUCTION STAGING REQUIREMENTS OFF THE PROPERTY NECESSARY FROM THE TOWN OF FRISCO.

DISTURBED AREA SEEDING NOTES:

- All areas to be seeded will be properly prepared to provide a friable soil surface in the upper 6 inches, minimum.
- Areas to be seeded will be drill seeded with the appropriate mix (Table 2 or 3) at the rates specified. Seed may be broadcast or hydroseeded on steep slopes. The specified seeding rate will be doubled for broadcast seeding or increased by 50 percent for hydroseeding.
- seeded areas will be mulched at a rate of at least two tons per acre of certified, weed-free straw mulch, or one ton per acre of wood cellulose, if hydromulching is completed. Hydromulching will be completed as a separate step after seeding. Straw mulch will be secured by use of m-binder tackifier at a rate of 3 pounds/1,000 square feet on slopes flatter than 2:1. Mulch will be secured with netting on slopes steeper than 3:1.

| SEED MIX TYPE I | | | | | |
|--------------------|----------------------|-------|-----------------|--|--|
| COMMON NAME | SCIENTIFIC NAME | % MIX | POUNDS PLS/ACRE | | |
| IDAHO FESCUE | FESTUCA IDAHOENSIS | 20 | 3.9 | | |
| ALPINE BLUEGRASS | POA ALPINA | 20 | 1.7 | | |
| WESTERN WHEATGRASS | PASCOPYRUM SMITHII | 20 | 15.8 | | |
| JUNE GRASS | KOELERIA CRISTATA | 15 | 0.6 | | |
| ARIZONA FESCUE | FESTUCA ARIZONICA | 20 | 3.2 | | |
| WHITE YARROW | ACHILLEA MILLEFOLIUM | 5 | 0.2 | | |
| TOTAL | | | 25.4 | | |

- 1. Mix should be drill seeded, except on steep slopes where broadcast or hydroseeding are acceptable at 200 and 150
- percent of rate shown, respectively. 2. The following wildflowers may also be seeded in certain areas. 0.8 Pounds PLS/Acre -Blanket Flower
- 4.4 Pounds PLS/Acre -Firecracker Penstemon 0.2 Pounds PLS/Acre -California Poppy 0.4 Pounds PLS/Acre
- 3. Divide Pounds PLS/Acre by 43.5 to obtain Pounds PLS/1,000 SQ.

| SEED MIX TYPE II | | | | | |
|--------------------|------------------------|-----------------|------|--|--|
| COMMON NAME | % MIX | POUNDS PLS/ACRE | | | |
| WESTERN WHEATGRASS | PASCOPYRUM SMITHII | 20 | 15.8 | | |
| REDTOP | AGROSTIS ALBA | 15 | 0.3 | | |
| TUFTED HAIRGRASS | DESCHAMPSIA CAESPITOSA | 15 | 0.5 | | |
| IDAHO FESCUE | FESTUCA IDAHOENSIS | 30 | 5.8 | | |
| ALPINE BLUEGRASS | POA ALPINA | 20 | 1.7 | | |
| TOTAL | | | 24.1 | | |

1. Mix should be drill seeded, except on steep slopes where broadcast or hydroseeding are acceptable at 200 and 150 percent of rate shown, respectively.

2. Divide Pounds PLS/Acre by 43.5 to obtain Pounds PLS/1,000 SQ

ROADWAY GENERAL NOTES:

 EARTHWORK OPERATIONS SHALL BE IN ACCORDANCE WITH GEOTECHNICAL REPORT FOR THE PROJECT.

2. PAVING SHALL NOT START UNTIL SUBGRADE COMPACTING TESTS ARE TAKEN AND MEET THE REQUIREMENTS OF THE PLANS AND SPECS AND FINAL PAVEMENT DESIGN BY GEOTECHINCAL ENGINEER AND/OR TOWN OF FRISCO STANDARDS, WHICHEVER ARE MORE STRINGENT. THE PAVEMENT SECTION SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL REPORT FOR THS PROJECT. THE MINIMUM DEPTH OF ASPHALT SHALL BE 3 INCHES.

3. THE CONTRACTOR SHALL SAW-CUT ALL EXISTING PAVEMENT WHERE MATCH LINES WITH EXISTING EDGE OF PAVEMENT OCCUR.

4. PORTLAND CEMENT CONCRETE SHALL MEET THE FOLLOWING REQUIREMENTS: SECTION TO END SECTION. THEREFORE, DISTANCES SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND COULD VARY. END SECTIONS ARE INCLUDED IN THE PIPE LENGTH SHOWN ON THE A. COMPRESSIVE STRENGTH OF 4000 PSI AFTER 28 DAYS OF CURE TIME;

B. AIR CONTENT OF $6.5\% \pm 1.5\%$; C. MAXIMUM SLUMP OF 3";

D. "FIBER MESH" FIBERS SHALL BE ADDED TO CONCRETE FOR STRENGTH, AT A RATE OF 1.5 POUNDS OF FIBER PER CUBIC YARD OF CONCRETE.

5. ROADWAY RETAINING WALL VERTICAL AND HORIZONTAL INFORMATION HAVE BEEN ESTABLISHED AS PART OF THESE ROADWAY PLANS. STRUCTURAL, GEOTECHNICAL, AND DRAINAGE ENGINEERING FOR THE WALLS IS BY OTHERS (SEE SEPARATE DESIGN DOCUMENTS). 6. COMPACTION TESTING FOR THE BASE COURSE IN THE ROADWAY SHALL MEET 95% OF MODIFIED PROCTOR (ASTM D-1557) THE MATERIAL BEING WITHIN 2.0 PERCENT OF OPTIMUM MOISTURE. EACH LIFT OF ASPHALT SHALL MEET THE MINIMUM DENSITY OF 92-96 PERCENT MAXIMUM THEORETICAL DENSITY AS DETERMINED BY THE RICE DENSITY METHOD (ASTM D-2041), TESTS SHALL BE MADE AT A FREQUENCY OF EVERY 200 LINEAR FEET AND AT EVERY 12" COMPACTED LIFT OF FILL PLACED, AND FOR EVERY LIFT OF ASPHALT PLACED OR ROLLED. ASPHALT DENSITY TESTING SHALL BE PERFORMED ON EACH LIFT AT INTERVALS OF ONE TEST PER EVERY 250 LINEAR FEET PER LANE. TEST LOCATIONS ON EACH LIFT AND EACH LANE SHALL BE STAGGERED.

7. DURING EARTHWORK OPERATION GEOTECHNICAL ENGINEER SHALL ASSESS ACTUAL SUB-SURFACE CONDITIONS AND REQUEST ADDITIONAL REQUIREMENTS IF NECESSARY.

STORM SEWER GENERAL NOTES

1. LOCATION AND ELEVATION OF EXISTING STORM SEWER AND CULVERTS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO START OF CONSTRUCTION. ANY DIFFERENCES FROM DESIGN PLAN SHALL BE REPORTED TO DESIGN ENGINEER.

2. STORM SEWER SHALL BE HDPE (HIGH DENSITY POLYETHYLENE).

3. ALL CULVERTS SHALL HAVE END SECTIONS ON BOTH THE UPSTREAM AND DOWNSTREAM ENDS OF THE PIPE UNLESS OTHERWISE NOTED ON THE PLANS AND SHALL EXTEND 1 TO 3 FEET BEYOND EACH EDGE OF SHOULDERED PAVED DRIVE. 4. STORM SEWER BEDDING AND PIPE ZONE BACKFILL SHALL BE 3/4" TO 1" ROAD BASE OR

APPROVED ALTERNATE. 5. PIPE LENGTHS FOR STORM SEWER ARE APPROXIMATE HORIZONTAL DISTANCES FROM END PLANS. FINAL LENGTH OF STORM SEWER SHALL BE SUFFICIENT TO PROVIDE THE ROAD SHOULDERS AND SIDE SLOPES TO NOT BE STEEPER THAN SHOWN ON THE TYPICAL ROAD

SANITARY SEWER GENERAL NOTES:

1. ALL SANITARY SEWER CONSTRUCTION SHALL CONFORM TO FRISCO SANITATION DISTRICT "DESIGN STANDARDS AND SPECIFICATIONS FOR SEWER CONSTRUCTION"

2. ALL SEWER MAINS AND SERVICES SHALL BE SDR 35 (UNLESS OTHERWISE NOTED). 3. ALL MANHOLE RIMS WITHIN THE 100-YEAR FLOOD PLAIN SHALL BE SET AT THE 100-YEAR FLOOD PLAIN ELEVATION AND SHALL HAVE GASKETTED BOLT DOWN LIDS.

4. MANHOLES SHALL BE WRAPPED WITH BITUTHENE. 5. SANITARY SEWER BEDDING AND PIPE ZONE BACKFILL GRADATION SHALL BE 1/4" TO 3/4" OR APPROVED ALTERNATE.

6. PIPELINE FLUSHING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIRING A CLEANING COMPANY THAT WILL HIGH-PRESSURE JET CLEAN THE LINES TO INSURE THAT SAND, ROCKS, OR OTHER FOREIGN MATERIAL ARE NOT LEFT IN ANY OF THE PIPELINES. WHEN FLUSHING, CARE SHOULD BE TAKEN TO PREVENT DAMAGE TO PROPERTY OR ROADWAYS OR EROSION OF SURROUNDING SOILS. FLUSHING WATER AND FLUSHED DEBRIS SHALL NOT BE ALLOWED TO ENTER THE EXISTING SEWER SYSTEM.

7. SEWER LINE ALIGNMENT, AND GRADE VERIFICATION. ONCE THE SEWER PIPELINES HAVE BEEN FLUSHED, THE SEWER PIPELINES SHALL BE INSPECTED BY MEANS OF CLOSED CIRCUIT TELEVISION (CCTV). DOCUMENTATION SHALL CONSIST OF A COLOR. VHS-FORMAT VIDEOTAPE, LOG SHEETS, AND A WRITTEN REPORT DETAILING THE CONDITION OF THE PIPELINE AND LATERAL CONNECTIONS/OPENINGS. THE REPORT SHALL NOTE THE TIME AND DATE OF VIDEO INSPECTION. STREET NAME, UPSTREAM AND DOWNSTREAM MANHOLE, DIRECTION OF VIEW, DIRECTION OF FLOW, SURFACE MATERIAL, PIPELINE LENGTH, PIPE SECTION LENGTH, PIPE SIZE, PIPE MATERIAL, LATERAL CONNECTIONS, VIDEO TAPE NUMBER, COUNTER NUMBER, AND A DETAILED LOGGING OF DEFECTS ENCOUNTERED. ANY REJECTED WORK SHALL BE REPAIRED, THEN RE-TELEVISED. 8. LEAKAGE. ALL PIPELINES SHALL BE TESTED FOR LEAKAGE BY MEANS OF AN AIR PRESSURE TEST. THE TEST SHALL BE PERFORMED AS FOLLOWS:

- PREPARATION FOR TESTS: FLUSH AND CLEAN THE PIPELINE PRIOR TO TESTING IN ORDER TO WET THE PIPE SURFACES AND PRODUCE MORE CONSISTENT RESULTS. PLUG AND BRACE ALL OPENINGS IN THE PIPELINE AND THE UPPER CONNECTIONS. CHECK ALL PIPE PLUGS WITH A SOAP SOLUTION TO DETECT ANY AIR LEAKAGE. IF LEAKS ARE FOUND, RELEASE THE AIR PRESSURE, ELIMINATE THE LEAKS, AND START THE TEST PROCEDURE OVER
- PROCEDURE OF TEST: ADD AIR UNTIL THE INTERNAL PRESSURE OF THE PIPELINE IS RAISED TO APPROXIMATELY 4.0 PSI, AT WHICH TIME THE FLOW OF AIR SHALL BE REDUCED AND THE PRESSURE MAINTAINED BETWEEN 3.5 AND 4.5 PSI FOR A SUFFICIENT TIME TO ALLOW THE AIR TEMPERATURE TO COME TO EQUILIBRIUM WITH THE TEMPERATURE OF THE PIPE.
- AFTER THE TEMPERATURE HAS STABILIZED, PERMIT THE PRESSURE TO DROP TO 3.5 PSIG IN EXCESS OF THE GROUND WATER PRESSURE ABOVE THE TOP OF THE SEWER, AT WHICH TIME A STOP WATCH OR SWEEP SECOND HAND WATCH SHALL BE USED TO DETERMINE THE TIME LAPSE REQUIRED FOR THE AIR PRESSURE TO DROP TO 3.0 PSIG.
- D. THE TIME ELAPSED SHALL NOT BE LESS THAN THE FOLLOWING:

PIPE SIZE TIME (INCHES) (MINUTES)

E. BRACE ALL PLUGS SUFFICIENTLY TO PREVENT BLOWOUTS AND VENT THE PIPELINE COMPLETELY REFORE ATTEMPTING TO REMOVE PLUGS

F. PROVIDE PRESSURIZING EQUIPMENT WITH A RELIEF VALVE SET AT 5 PSI TO AVOID OVER-PRESSURIZING AND DAMAGING AN OTHERWISE ACCEPTABLE LINE.

9. MANHOLE VISUAL EXAMINATION. THE ENGINEER SHALL VISUALLY CHECK EACH MANHOLE. BOTH EXTERIOR AND INTERIOR, FOR FLAWS, CRACKS, HOLES, OR OTHER INADEQUACIES, WHICH INADEQUACIES BE FOUND, THE CONTRACTOR, AT ITS OWN EXPENSE, SHALL MAKE ANY REPAIRS DEEMED NECESSARY BY THE ENGINEER. CONTRACTOR TO NOTIFY ENGINEER 48 HOURS PRIOR TO

10. MANHOLE LEAKAGE TEST (VACUUM). ALL MANHOLES SHALL BE TESTED FOR LEAKAGE AND ALL TESTS SHALL BE WITNESSED BY THE ENGINEER. THE LEAKAGE TEST SHALL BE CONDUCTED PRIOR TO BACK-FILLING AROUND THE MANHOLE AND SHALL BE CARRIED OUT IN THE FOLLOWING

A. MANHOLES SHALL BE VACUUM TESTED AFTER ASSEMBLY AND PRIOR TO BACKFILLING.

B. CARE SHALL BE TAKEN LO EFFECT A SEAL BETWEEN THE VACUUM BASE AND THE MANHOLE RIM. PIPE PLUGS SHALL BE SECURED TO PREVENT MOVEMENT WHILE THE VACUUM IS

C. A VACUUM OF 10 INCHES OF MERCURY SHALL BE DRAWN. THE TIME FOR THE VACUUM TO DROP TO 9 INCHES OF MERCURY SHALL BE RECORDED.

D. ACCEPTANCE SHALL BE DEFINED AS WHEN THE TIME TO DROP TO 9 INCHES MEETS OR EXCEEDS THE FOLLOWING:

120 SECONDS

E. IF THE MANHOLE FAILS THE TEST, MAKE NECESSARY REPAIRS. REPAIRS AND REPAIR PROCEDURES MUST BE ACCEPTABLE TO TOWN.

IF PREFORMED PLASTIC GASKETS ARE PULLED OUT DURING THE VACUUM TEST, THE MANHOLE SHALL BE DISASSEMBLED AND THE GASKETS SHALL BE REPLACED. 11. ALL SEWER LINE WORK SHALL BE INSPECTED BY THE DESIGN ENGINEER DURING CONSTRUCTION.

12. AS BUILT DRAWINGS SHALL BE PROVIDED BY A PROFESSIONAL ENGINEER. 13. EXISTING SEWER MAIN ELEVATIONS MUST BE FIELD VERIFIED.

WATER GENERAL NOTES:

1. ALL MATERIALS AND WORKMANSHIP SHALL BE IN CONFORMANCE WITH THE TOWN OF FRISCO WATER DISTRICT CURRENT RULES AND REGULATIONS. WATER SYSTEM SPECIFICATIONS AND TESTING PROCEDURES SHALL BE IN CONFORMANCE WITH TOWN OF FRISCO WATER DISTRICT STANDARDS.

2. ALL WATER MAINS SHALL BE AWWA, CLASS 52, PUSH ON JOINT, DUCTILE IRON PIPE (DIP) WITH RUBBER GASKET..

3. SERVICE LINES SHALL BE 1" K COPPER. ALL SERVICE LINES SHALL HAVE A BACKFLOW

PREVENTION DEVICE INSTALLED UPSTREAM OF THE WATER METER CONSISTING OF A DOUBLE CHECK VALVE ASSEMBLY SIMILAR OR EQUAL TO A WATTS REGULATOR NO. 7. 4. MINIMUM COVER WITHIN STREETS IS 9.5 FEET AND 8.5 FEET IN UNPAVED LOCATIONS. INSULATION REQUIRED AT DEPTHS BELOW 8.5'.

5. THE CONTRACTOR IS RESPONSIBLE FOR: A. NOTIFYING ALL CUSTOMERS POSSIBLY AFFECTED BY OUTAGE OF WATER DURING CONSTRUCTION. B. THE CONTRACTOR SHALL OBTAIN, AT HIS EXPENSE, ALL

APPLICABLE LICENSES, PERMITS, BONDS, ETC. REQUIRED FOR THE MAIN INSTALLATION/SYSTEM MODIFICATION. C. CONTACTING TOWN OF FRISCO WATER DISTRICT FOR PRE-CONSTRUCTION

MEETING AT LEAST 48 HOURS PRIOR TO CONSTRUCTION. NOTE: BE ADVISED THAT OCCASIONALLY VALVES IN OUR SYSTEM MAY BE INOPERABLE. ON SUCH OCCASIONS IT MAY BECOME NECESSARY TO BACK UP AN ADDITIONAL BLOCK FOR THE SHUT OUT. IT WILL THEN BE NECESSARY TO MAKE THE ADDITIONAL NOTIFICATIONS TO GIVE THE AFFECTED CUSTOMERS THE

MANDATORY 24 HOURS ADVANCE NOTICE. ALSO BE ADVISED THAT

WHEN VALVE MAINTENANCE IS REQUIRED, A DELAY OF SEVERAL

DAYS SHOULD BE EXPECTED. 6. ALL WATER LINE WORK SHALL BE INSPECTED BY THE DESIGN ENGINEER DURING CONSTRUCTION

7. AS BUILT DRAWINGS SHALL BE PREPARED BY A COLORADO PROFESSIONAL ENGINEER PER THE TOWN OF FRISCO WATER DISTRICT REQUIREMENTS. 8. FOR DETAILS OF IRRIGATION REQUIREMENTS AND METER REQUIREMENTS SEE

9. CONTRACTOR IS RESPONSIBLE FOR VERIFING THE MECHINICAL DESIGN ACCOUNTS FOR FIRE PROTECTION AND CONFIRMING THE 4" WATER SERVICE SPECIFIED IS SIZE APPROPRIATELY.

WATER GENERAL NOTES (CONTINUED):

10. VALVES SHALL BE RESILIENT SEAT NRS GATE VALVES AND SHALL OPEN-LEFT (MUELLER, US, WATEROUS OR CLOW BRAND RESILIENT WEDGE VALVES ONLY). CHECK WITH WATER SUPT. FOR VERIFICATION OF SPECIFIC MODEL NUMBERS. 11. VALVE BOXES SHALL BE OVAL BASE BOTTOM TYPE. CHECK WITH WATER SUPT. FOR VERIFICATION OF SPECIFIC MODEL NUMBERS.

12. ALL FIRE HYDRANTS SHALL BE WATEROUS "PACER" WITH 34-INCH MOUNTAIN STANDARD FLANGE MEETING THE FOLLOWING REQUIREMENTS: NOZZLE

6 INCH FOR MECHANICAL JOINT 9'-6" OR 8'-6" (AS REQUIRED TO MEET THE WATERLINE COVER) DEPTH OF BURY 1 INCH PENTAGON OPERATING NUT1 LEFT(CCW

OUTLETS TWO 2-1/2 INCH, ONE 5-1/4 INCH PUMPER NOZZLE (THREADS TO MATCH EXISTING) THREADS NATIONAL STANDARD CAPS CAP WITH PENTAGON NU

INLET

COLOR

RED (ALL ABOVE GROUND PARTS) BOTTOM THRUST BLOCK AND 2-3/4" TIE RODS FROM MAIN TEE THRUST RESTRAINT TO HYDRANT BOTTOM. **ELEVATION OF NOZZLE** 42" ± 3" OPERATING NUT ABOVE FINISHED GROUND SURFACE AT TRAFFIC FLANGE

ALL HYDRANTS TO BE SHOP PRIMED AND PAINTED RED. BOLLARDS AS SPECIFIED BY TOWN. 13. WATER METER KIT WILL BE PROVIDED BY TOWN. THE CHARGE FOR THE WATER METER KIT WILL BE PAID BY THE DEVELOPER AT THE TIME OF THE BUILDING PERMIT ISSUANCE. THE METER KIT WILL HAVE REMOTE READOUT. 14. AIR RELEASE VALVES (ARV'S) SHALL BE APCO MODEL NO. 143 C COMBINATION AIR/VACUUM VALVE OR APPROVED EQUAL

15. MECHANICAL JOINT RESTRAINT DEVICES SHALL BE: FOR DUCTILE IRON PIPE: FOR C900 PVC PIPE: IBEE IRON INC. SERIES 1500 MEGALUG 1700 SERIES ROMAL ROM GRIP

UNI-FLANGE 1400 SERIES STAR GRIP 3000 SERIES

16. PIPE JOINT RESTRAINT DEVICES. TIE RODS AND THRUST BLOCKS SHALL BE INSTALLED PER DETAILS. ALL RESTRAINT RODS AND HARDWARE ARE TO BE STAINLESS STEEL OR CORTEN. 17. CHLORINATION

ALL MAIN EXTENSIONS AND PRIVATE PIPE EXTENSIONS SHALL BE CHLORINATED IN ACCORDANCE WITH AWWA C651. THE CHLORINATING AGENT AND METHOD OF APPLICATION, SHALL BE APPROVED BY THE TOF. THE CHLORINATION OF THE FINISHED PIPELINE SHALL BE DONE PRIOR TO THE HYDROSTATIC TESTING. BEFORE FILLING THE PIPE WITH WATER, THE PIPE SHALL BE CLEAN AND FREE OF DEBRIS TO THE SATISFACTION OF THE TOWN. TOS WILL NOT PROVIDE LABOR OR MATERIAL FOR

DISINFECTION TO APPLICANT'S INSTALLING MAINS UNDER PRIVATE CONTRACT. CHLORINE TABLETS MAY BE USED FOR DISINFECTION IN 12-INCH AND SMALLER PIPE. SIXTEEN INCH AND LARGER PIPE REQUIRES A CHLORINE SLURRY FED INTO THE WATER USED IN FILLING THE PIPE. CHLORINE TABLETS SHALL BE ATTACHED TO THE INSIDE TOP OF THE PIPE WITH AN APPROVED ADHESIVE CERTIFIED TO NSF STANDARD 61 PRIOR TO THE PIPE INSTALLATION IN THE TRENCH. AN APPROVED ADHESIVE IS DOW CORNING 732 MULTI-PURPOSE SEALANT. NUMBER OF HYPOCHLORITE TABLETS OF 5 GRAM STRENGTH

REQUIRED FOR A DOSE OF 50 MILLIGRAMS/LITER* PIPE LENGTH PIPE DIAMETER (INCHES)

<u>6 8 12</u> 13 OR LESS 18

SIGMA-LOCK

*BASED ON 3 3/4" GRAM AVAILABLE CHLORINE PER TABLET

AFTER THE PIPE IS FILLED WITH WATER AND CHLORINE, THE CHLORINATED WATER SHALL BE HELD IN CONTACT WITH THE PIPE FOR 24 HOURS. AT THE END OF THE 24 HOUR PERIOD, THE WATER IN THE PIPELINE SHALL BE TESTED BY THE TOWN OF FRISCO TO INSURE A RESIDUAL CHLORINE CONTENT OF NOT LESS THAN 25 MILLIGRAMS PER LITTER. THE PIPE LINE THEN SHALL BE THOROUGHLY FLUSHED TO REMOVE THE HEAVILY CHLORINATED WATER. $\,$ THE CONTRACTOR SHALL $\,$ TAKE CARE IN FLUSHING THE PIPELINE TO PREVENT PROPERTY, ENVIRONMENTAL OR DANGER TO

SAMPLES OF WATER WILL BE COLLECTED FOR BACTERIOLOGICAL EXAMINATION AND RESIDUAL CHLORINE CONTENT TESTING BEFORE THE PIPE IS PUT INTO SERVICE. TESTING OF RESIDUAL CHLORINE AND SAMPLING WILL BE DONE BY THE LOCAL HEALTH AUTHORITY OR THEIR DESIGNATED REPRESENTATIVE.

18. HYDROSTATIC TESTING

NO HYDROSTATIC TESTS SHALL BE MADE ON ANY PORTION OF THE PIPELINE UNTIL FIELD PLACED CONCRETE HAS HAD ADEQUATE CURING TIME, DEFINED AS FOLLOWS: CONCRETE SHALL BE CURED BY A METHOD RECOMMENDED BY ACI 308. WHEN THE DAILY MEAN AMBIENT TEMPERATURE IS ABOVE 40°F, THE FINISHED CONCRETE SHALL BE CURED CONTINUOUSLY FOR A MINIMUM OF 7 DAYS OR FOR THE TIME NECESSARY TO ATTAIN 70% OF THE SPECIFIED COMPRESSIVE STRENGTH, WHICHEVER PERIOD IS LESS. WHEN THE MEAN DAILY AMBIENT TEMPERATURE IS 40°F OR LOWER. THE FINISHED CONCRETE SHALL BE CONTINUALLY CURED AT A MINIMUM TEMPERATURE OF 55° F FOR THE PERIOD RECOMMENDED BY ACI 306 TO PREVENT DAMAGE FROM EARLY-AGE FREEZING AND PROVIDE THE SERVICE CATEGORY STRENGTHS REQUIRED FOR EACH PLACEMENT. TOF SHALL BE NOTIFIED 24 HOURS IN ADVANCE OF TESTING. ALL TESTING SHALL BE MADE

IN THE PRESENCE OF TOF WATER DEPARTMENT STAFF ONLY THE FOLLOWING METHODS ARE ACCEPTABLE FOR SUPPLYING POTABLE WATER FOR HYDROSTATIC TESTING:

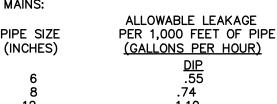
WATER MAY BE TAKEN FROM A NEARBY PRESSURIZED WATER SOURCE WHICH HAS BEEN PREVIOUSLY CHLORINATED, TESTED AND ACCEPTED, SUCH AS A FIRE HYDRANT. WATER MAY BE DELIVERED TO THE SITE IN A CHLORINATED WATER TRUCK HAVING A MINIMUM CAPACITY OF 300 GALLONS. THE WATER TRUCK SHALL BE USED EXCLUSIVELY FOR THE TRANSPORTATION OF POTABLE WATER.

3. ANY PREVIOUSLY TESTED, CHLORINATED AND ACCEPTED WATER MAIN, WHICH IS PRESSURIZED AND IS TO SERVE THE NEW MAIN EXTENSION, MAY BE TAPPED ON THE PRESSURIZED SIDE OF THE

IN ANY EVENT, THE METHOD OF SUPPLYING WATER AS WELL AS THE SOURCE OF WATER FOR HYDROSTATIC TESTING MUST BE CERTIFIED AND APPROVED BY TOB. USE OF BARRELS, SANITARY OR OTHERWISE. TO SUPPLY WATER FOR HYDROSTATIC TESTING IS STRICTLY PROHIBITED. TOF WILL FURNISH ONLY THE CALIBRATED METER BUT NOT THE PUMP FOR TESTING. THE PIPELINE SHALL BE PROPERLY BACKFILLED AND SHALL BE IN A STATE OF READINESS FOR TESTING. ALL BULKHEADS, PUMPS, TAPS, AND APPURTENANCES NECESSARY TO FILL THE PIPELINE AND MAINTAIN THE REQUIRED PRESSURE SHALL BE IN PLACE. THE PIPELINE SHALL BE FILLED WITH WATER AND THE TEST PRESSURE OF 150 POUNDS PER SQUARE INCH SHALL BE APPLIED TO THE PIPELINE BY MEANS OF A CONTINUOUSLY OPERATING PUMP, EQUIPPED WITH A BYPASS VALVE FOR REGULATING PRESSURE. WHEN FILLING THE PIPELINE, IT SHALL BE FILLED AT A RATE, WHICH WILL NOT CAUSE ANY SURGES, NOR WILL IT EXCEED THE RATE AT WHICH THE AIR CAN BE RELEASED.

ALL AIR IN THE LINE SHALL BE PROPERLY PURGED. WHERE BLOWOFFS OR HYDRANTS ARE NOT AVAILABLE OR ARE NOT EFFECTIVE IN PURGING AIR FROM THE LINE, TOF SHALL REQUIRE A TAP TO PURGE THE LINE. THE LOCATION AND SIZE OF TAP SHALL BE AT TOF'S DISCRETION. WHILE THE TEST PRESSURE IS MAINTAINED, AN EXAMINATION SHALL BE MADE OF THE PIPELINE IN GENERAL, AND ANY LEAKS SHALL BE REPAIRED. ANY PIPE OR FITTING FOUND TO BE FAULTY SHALL BE REMOVED AND REPLACED. NO LEAKAGE IS ALLOWED THROUGH THE BONNET OF THE LINE VALVE. ANY VALVE LEAKING THROUGH THE BONNET SHALL BE REPAIRED IN PLACE OR REMOVED AND REPLACED. CUTTING AND REPLACING PAVEMENT. EXCAVATING. AND BACKFILLING MAY ALL BE NECESSARY PARTS OF LOCATING AND REPAIRING LEAKS DISCOVERED BY PRESSURE TESTING OF

AFTER ALL VISIBLE LEAKS HAVE BEEN STOPPED, THE FULL TEST-PRESSURE SHALL BE MAINTAINED FOR 2 CONTINUOUS HOURS. ALLOWABLE LEAKAGE FOR EACH SECTION BETWEEN LINE VALVES SHALL NOT EXCEED THE FOLLOWING LEAKAGE RATES FOR 4-INCH THROUGH 20-INCH DISTRIBUTION AND TRANSMISSION MAINS:



SHOULD TESTING SHOW A LEAKAGE RATE IN EXCESS OF THE RATES SHOWN, THE PIPELINE SHALL NOT BE ACCEPTED. THE PIPELINE SHALL BE REPAIRED, RECHLORINATED AS DESCRIBED IN NOTE 12, AND RETESTED UNTIL IT MEETS THE TEST REQUIREMENTS. 19. THE CONTRACTOR IS RESPONSIBLE FOR:

A. NOTIFYING ALL CUSTOMERS POSSIBLY AFFECTED BY OUTAGE OF WATER DURING CONSTRUCTION.
B. THE CONTRACTOR SHALL OBTAIN, AT HIS EXPENSE, ALL APPLICABLE LICENSES, PERMITS, BONDS, ETC. REQUIRED FOR THE MAIN INSTALLATION/SYSTEM MODIFICATION. C. CONTACTING TOWN OF FRISCO FOR PRE-CONSTRUCTION MEETING AND INSPECTION, 970-XXX-XXXX, AT LEAST 48 HOURS PRIOR TO COMMENCING CONSTRUCTION. D. IN CASE OF AN EMERGENCY AFTER WORKING HOURS, CALL TOWN OF FRISCO AT 970-668-0836 (JEFF GOBLE)

NOTE: BE ADVISED THAT OCCASIONALLY VALVES IN OUR SYSTEM MAY BE INOPERABLE. ON SUCH OCCASIONS IT MAY BECOME NECESSARY TO BACK UP AN ADDITIONAL BLOCK FOR THE SHUT OUT. IT WILL THEN BE NECESSARY TO MAKE THE ADDITIONAL NOTIFICATIONS TO GIVE THE AFFECTED CUSTOMERS THE MANDATORY 24 HOURS ADVANCE NOTICE. ALSO BE ADVISED THAT WHEN VALVE MAINTENANCE IS REQUIRED. A DELAY OF SEVERAL DAYS SHOULD BE EXPECTED.

WATER GENERAL NOTES (CONTINUED):

PLASTIC CAP, TYP.

CONSTRUCTION FENCE

OR APPROVED EQUA

STUDDED STEEL

20. WATER TRENCH BEDDING AND PIPE ZONE BACKFILL SHALL BE GRADED AS FOLLOWS:

SIEVE SIZE TOTAL PASSING BY SIZE (% BY WEIGHT) NO. 200 0 - 3

— CF —— CF —— CF -

' MIN.

1' MIN.

OR TOWN OF FRISCO APPROVED CONTRACTOR ALTERNATE. 21. IRRIGATION VAULT TO BE CONSTRUCTED PER TOWN OF FRISCO DETAILS. 22. CLAY CHECK DAMS MAY BE REQUIRED IF GROUNDWATER IS ENCOUNTERED CALL UTILITY NOTIFICATION CENTER OF COLORADO \bigcirc

CALL 2 BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

CONSTRUCTION FENCE INSTALLATION NOTES

CONSTRUCTION FENCE MAINTENANCE NOTES

EROSION, AND PERFORM NECESSARY MAINTENANCE

-LOCATION OF CONSTRUCTION FENCE.

2. CONSTRUCTION FENCE SHOWN SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING

3. CONSTRUCTION FENCE SHALL BE COMPOSED OF ORANGE, CONTRACTOR-GRADE MATERIAL

4. STUDDED STEEL TEE POSTS SHALL BE UTILIZED TO SUPPORT THE CONSTRUCTION FENCE. MAXIMUM SPACING FOR STEEL TEE POSTS SHALL BE 10'.

1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE

2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.

. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON

NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARIA FROM GOLD CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN

4. CONSTRUCTION FENCE SHALL BE REPAIRED OR REPLACED WHEN THERE ARE SIGNS OF DAMAGE SUCH AS RIPS OR SAGS. CONSTRUCTION FENCE IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS STABILIZED AND APPROVED BY THE LOCAL JURISDICTION.

6. WHEN CONSTRUCTION FENCES ARE REMOVED, ALL DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE FENCE SHALL BE COVERED WITH TOPSOIL, SEEDED AND MULCHED, OR OTHERWISE STABILIZED AS APPROVED BY LOCAL

MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS.

THAT IS AT LEAST 4' HIGH. METAL POSTS SHOULD HAVE A PLASTIC CAP FOR SAFETY.

5. CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO THE TOP, MIDDLE, AND

1. SEE PLAN VIEW FOR:

BOTTOM OF EACH POST.



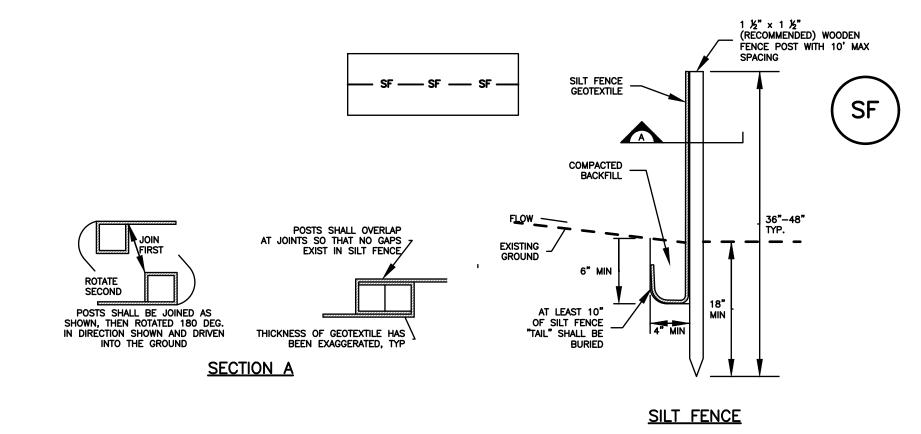
| | | 5/15/25 REVISED GARAGE RAMP PER TOF INPUT | TOF FINAL PLAN SUBMITTAL | TOF SKETCH PLAN SUBMITTAL | Description |
|--|--|---|------------------------------|------------------------------|----------------|
| | | 5/12/52 | 4/25/25 | 9/7/24 | Date |
| | | REVISED | FINAL PLAN SUBMITTAL 4/25/25 | SKETCH PLAN SUBMITTAL 9/7/24 | Revision/Issue |
| | | 3 | 2 | - | No. |
| | | | | | |

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CF PLASTIC MESH CONSTRUCTION FENCE

STUDDED STEEL



SF SILT FENCE

SILT FENCE INSTALLATION NOTES

I. SILT FENCE MUST BE PLACED AWAY FROM THE TOE OF THE SLOPE TO ALLOW FOR WATER PONDING. SILT FENCE AT THE TOE OF A SLOPE SHOULD BE INSTALLED IN A FLAT LOCATION AT LEAST SEVERAL FEET (2-5 FT) FROM THE TOE OF THE SLOPE TO ALLOW ROOM FOR PONDING AND DEPOSITION. 2. A UNIFORM 6" X 4" ANCHOR TRENCH SHALL BE EXCAVATED USING TRENCHER OR SILT FENCEINSTALLATION DEVICE. NO ROAD GRADERS, BACKHOES, OR SIMILAR EQUIPMENT SHALL BE USED. 5. COMPACT ANCHOR TRENCH BY HAND WITH A "JUMPING JACK" OR BY WHEEL ROLLING. COMPACTION SHALL BE SUCH THAT SILT FENCE RESISTS BEING PULLED OUT OF ANCHOR TRENCH BY HAND. . SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD BE NO NOTICEABLE SAG BETWEEN STAKES AFTER IT HAS BEEN ANCHORED TO THE STAKES. 5. SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES USING 1" HEAVY DUTY STAPLES OR NAILS WITH 1" HEADS. STAPLES AND NAILS SHOULD BE PLACED 3" ALONG THE FABRIC DOWN THE 6. AT THE END OF A RUN OF SILT FENCE ALONG A CONTOUR, THE SILT FENCE SHOULD BE TURNED PERPENDICULAR TO THE CONTOUR TO CREATE A "J-HOOK." THE "J-HOOK" EXTENDING PERPENDICULAR TO THE CONTOUR SHOULD BE OF SUFFICIENT LENGTH TO KEEP RUNOFF FROM FLOWING AROUND THE END OF THE SILT FENCE (TYPICALLY 10' - 20'). 7. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.

SILT FENCE MAINTENANCE NOTES

1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION. AND PERFORM NECESSARY MAINTENANCE 2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED 3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE

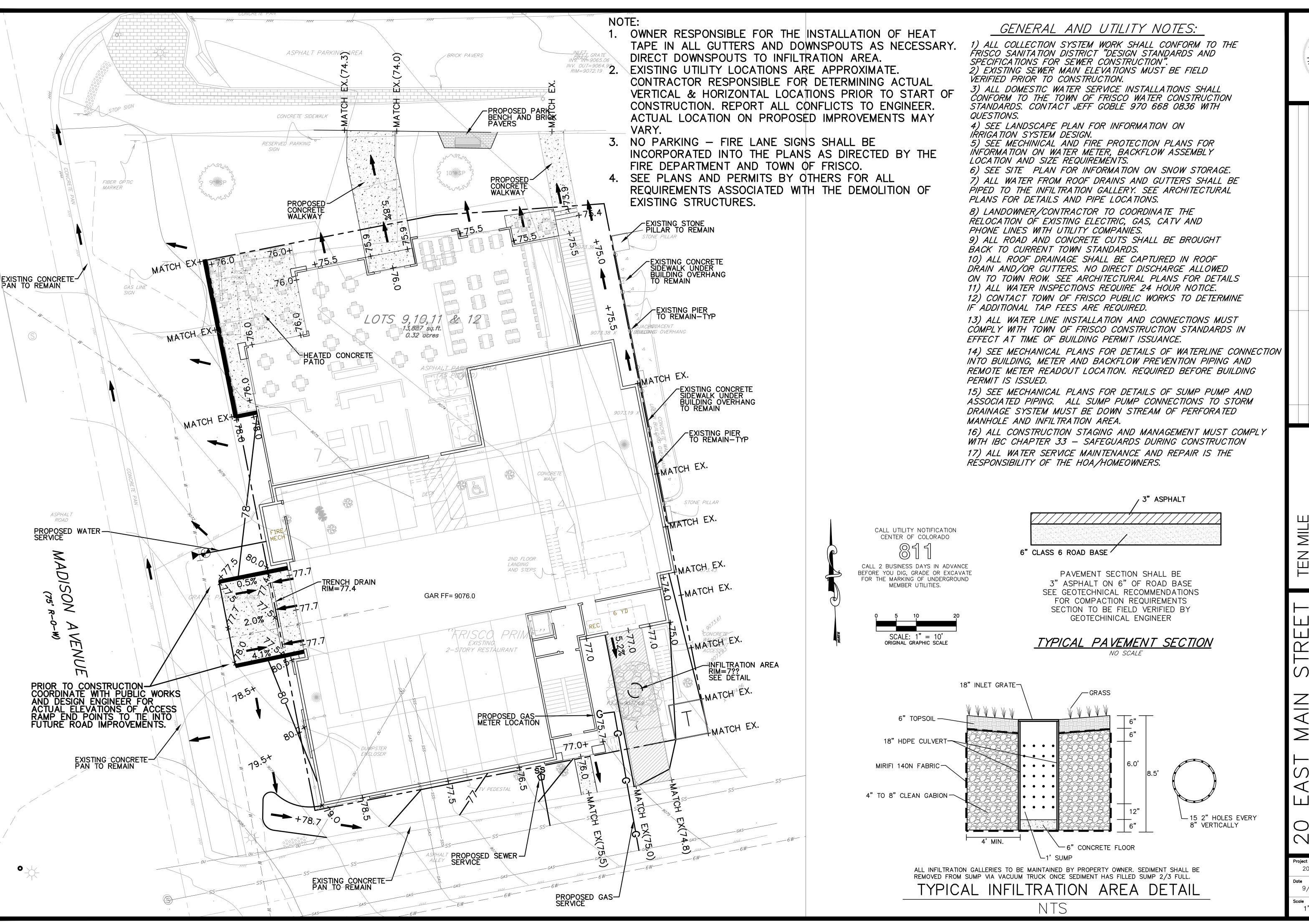
4. SEDIMENT ACCUMULATED UPSTREAM OF THE SILT FENCE SHALL BE REMOVED AS NEEDED TO MAINTAIN THE FUNCTIONALITY OF THE BMP, TYPICALLY WHEN DEPTH OF ACCUMULATED SEDIMENTS IS APPROXIMATELY 6". 5. REPAIR OR REPLACE SILT FENCE WHEN THERE ARE SIGNS OF WEAR, SUCH AS SAGGING, TEARING, OR COLLAPSE. 6. SILT FENCE IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS STABILIZED AND APPROVED BY THE LOCAL JURISDICTION, OR IS REPLACED BY AN EQUIVALENT PERIMETER SEDIMENT 7. WHEN SILT FENCE IS REMOVED, ALL DISTURBED AREAS SHALL BE COVERED WITH TOPSOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED AS APPROVED BY LOCAL JURISDICTION.

NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS. CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN DIFFERENCES ARE NOTED.

20 EAST MAIN(PRIME)

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9/7/24 NTS



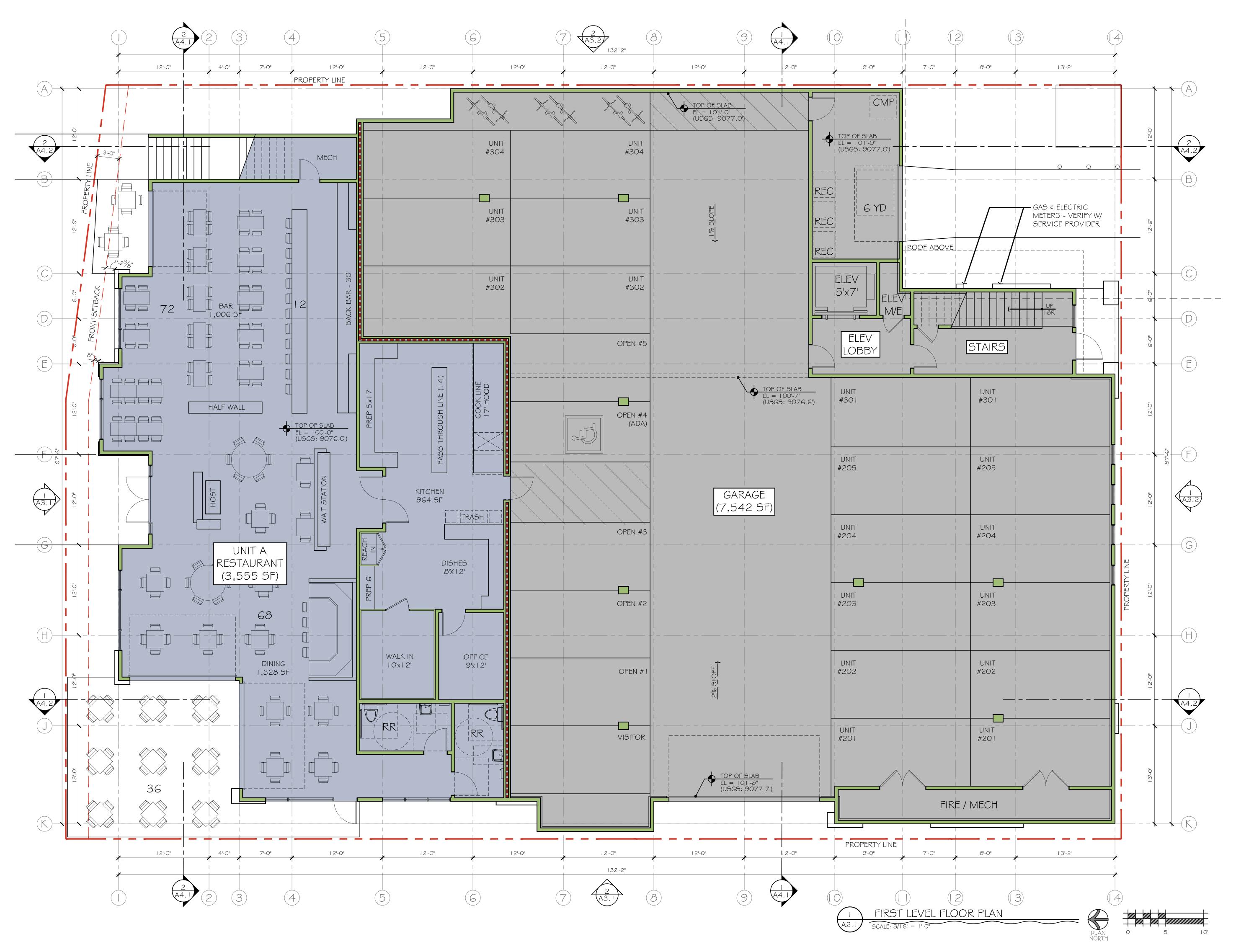


2 2

RING TEN MILE ENGINEEF

20 EAST MAIN(PRIME)

9/7/24 1"=10'





ALLEN-GUERRA ARCHITECTURE
711 B GRANITE STREET
PO BOX 5540
FRISCO . COLORADO . 80443
PH: 970453.7002 . FAX: 970.453.7040
E-MAIL: INFO@ALLEN-GUERRA.COM
WEBSITE: WWW.ALLEN-GUERRA.COM

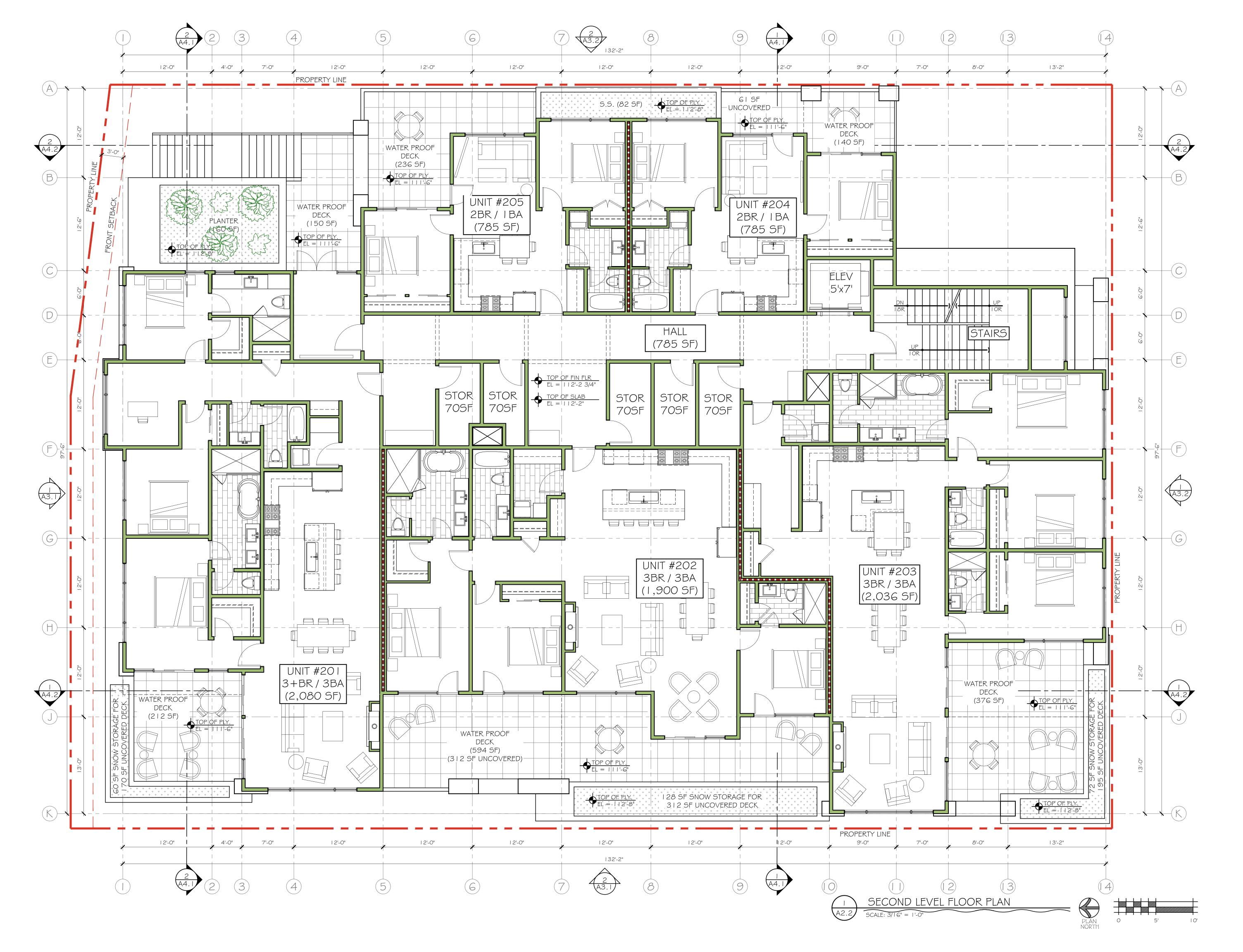
PLOCK 2. KING SOLOMON SUBDIVISION #2

N STREET. TOWN OF FRISCO. COLORADO

LEVEL FLOOR PLAN

DSUE: DATE: PRELIM 28 APR 2022 DESIGN 15 FEB 2024 DRC 2 APR 2024 UPDATE 8 MAY 2024 SKETCH 6 SEP 2024 REVIEW 6 DEC 2024 PLANNING 24 JAN 2025 UPDATE 24 APR 2025 PROJECT #: 2233

A2.1





ALLEN-GUERRA ARCHITECTURE
711 B GRANITE STREET
PO BOX 5540
FRISCO . COLORADO . 80443
PH: 970453,7002 . FAX: 970453,7040
E-MAIL: INFO@ALLEN-GUERRACOM
WEDSITE: WWW.ALLEN-GUERRACOM

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PRIME SAUCE
LOT 9, 10, 11, 12 BLOCK 2. KING SOLOMON SUBDIVISION #
20 FAST MAIN STREET. TOWN OF FRISCO. COLORADO

ISSUE:

PRELIM

DESIGN

DRC

UPDATE

SKETCH

REVIEW

PLANNING

UPDATE

DATE:

28 APR 2022

15 FEB 2024

2 APR 2024

8 MAY 2024

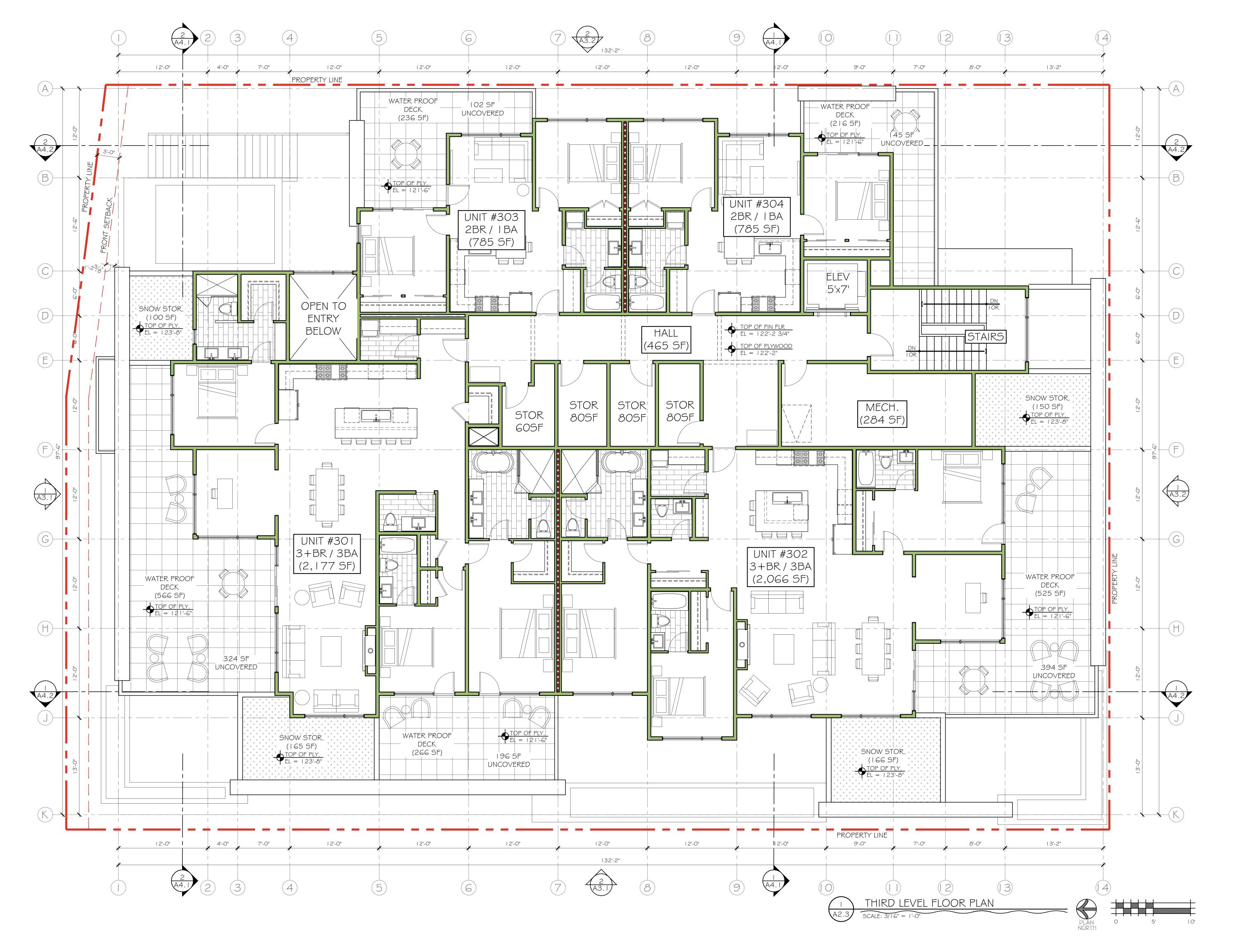
6 SEP 2024

6 DEC 2024

24 JAN 2025

24 APR 2025

PROJECT #: 2233



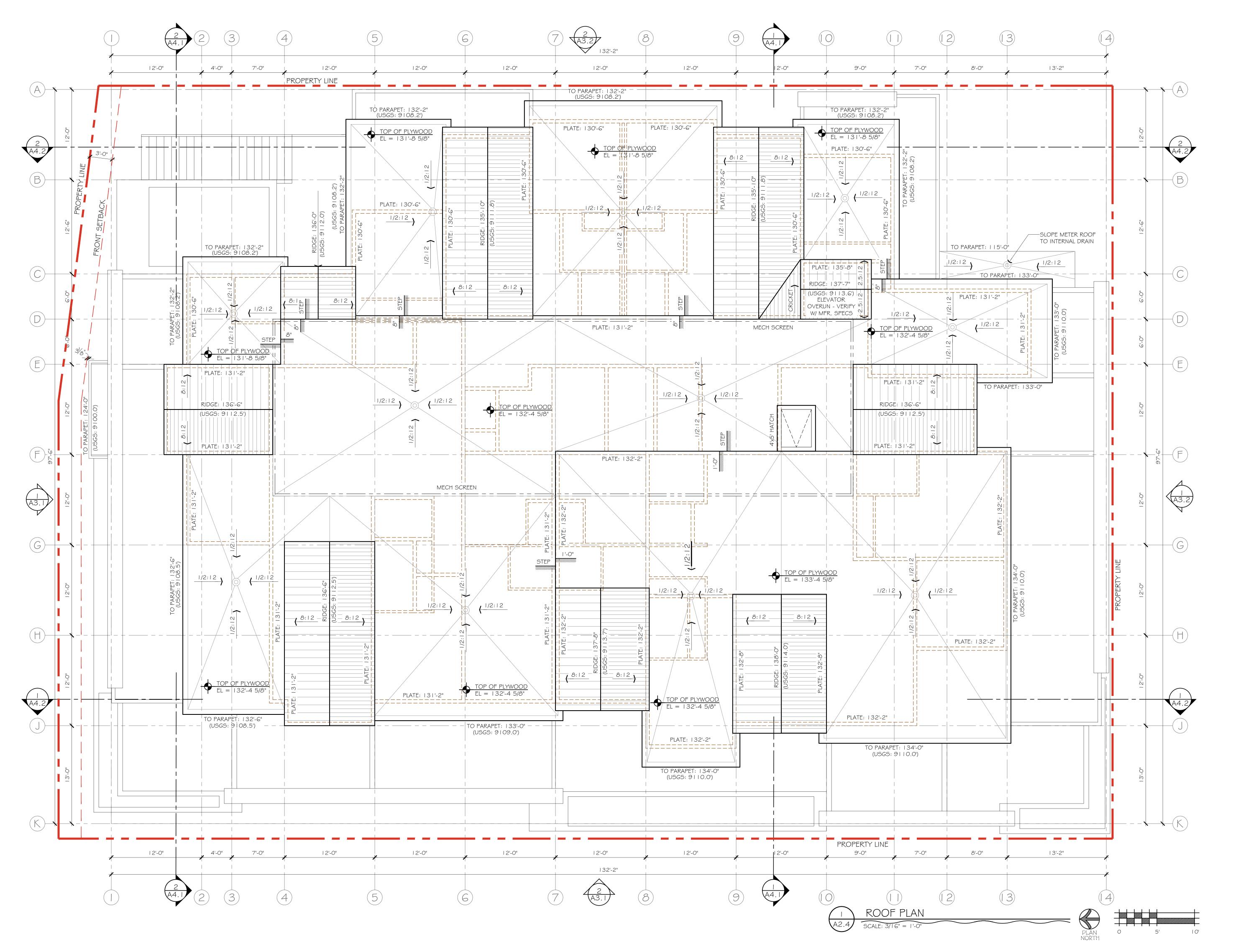


ALLEN-GUERRA ARCHITECTURE
711 D GRANITE STREET
PO BOX 5540
FRISCO . COLORADO . 80443
PH: 970.453.7002 . FAX: 970.453.7040
E-MAIL: INFO@ALLEN-GUERRA.COM
WEDSITE: WWW.ALLEN-GUERRA.COM

PRIME SAUCE
LOT 9, 10, 11, 12 BLOCK 2. KING SOLOMON SUBDIVISION
20 EAST MAIN STREET. TOWN OF FRISCO. COLORAD
THIRD LEVEL FLOOR PLA

| ISSUE: | DATE: |
|----------|-------------|
| PRELIM | 28 APR 2022 |
| DESIGN | 15 FEB 2024 |
| DRC | 2 APR 2024 |
| UPDATE | 8 MAY 2024 |
| SKETCH | 6 SEP 2024 |
| REVIEW | 6 DEC 2024 |
| PLANNING | 24 JAN 2025 |
| UPDATE | 24 APR 2025 |
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| PROJECT | T #: 2233 |

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ALLEN-GUERRA ARCHITECTURE
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LOT 9, 10, 11, 12 BLOCK 2. KING SOLOMON SUBDIVISIO
20 EAST MAIN STREET. TOWN OF FRISCO. COLORAI

ISSUE:

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28 APR 2022

15 FEB 2024

2 APR 2024

8 MAY 2024

6 SEP 2024

6 DEC 2024

24 JAN 2025

24 APR 2025

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PROJECT #: 2233





ALLEN-GUERRA ARCHITECTURE
P.O. BOX 7488
BRECKENRIDGE: COLORADO: 80424
PH: 970453,7002
FAX: 970453,7040
E-MAIL: INFO@ALLEN-GUERRACOM
WEB SITE: WWWALLEN-GUERRACOM

PRIME SAUCE
LOTS 9, 10, 11 & 12. BLOCK 2. KING SOLOMON SUBDIVISION #2
20 EAST MAIN STREET. TOWN OF FRISCO. COLORADO

FYTERIOR FLEVATIONS

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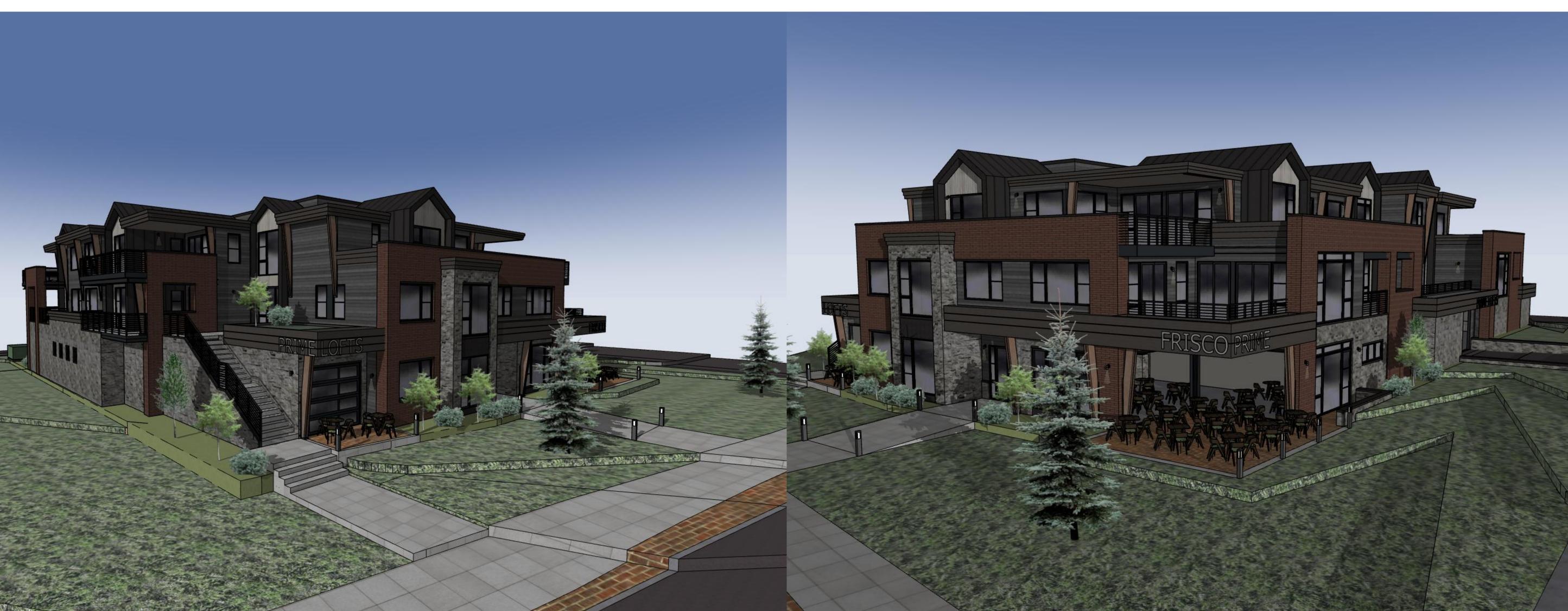
ALLEN-GUERRA ARCHITECTURE
P.O. BOX 7488
BRECKENRIDGE . COLORADO . 80424
PH: 9704537002
FAX: 9704537040
E-MAIL: INFO@ALLEN-GUERRACOM
WEB SITE: WWWALLEN-GUERRACOM

PRIME SAUCE
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20 EAST MAIN STREET. TOWN OF FRISCO. COLORADO

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NORTHEAST PERSPECTIVE

N.T.S.





4 SOUTHWEST PERSPECTIVE

A3.3 N.T.S.





ALLEN-GUERRA ARCHITECTURE
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BRECKENRIDGE . COLORADO . 80424
PH: 9704537002
FAX: 9704537040
E-MAIL: INFO@ALLEN-GUERRACOM
WEB SITE: WWWALLEN-GUERRACOM

BLOCK 2. KING SOLOMON SUBDIVISION #2

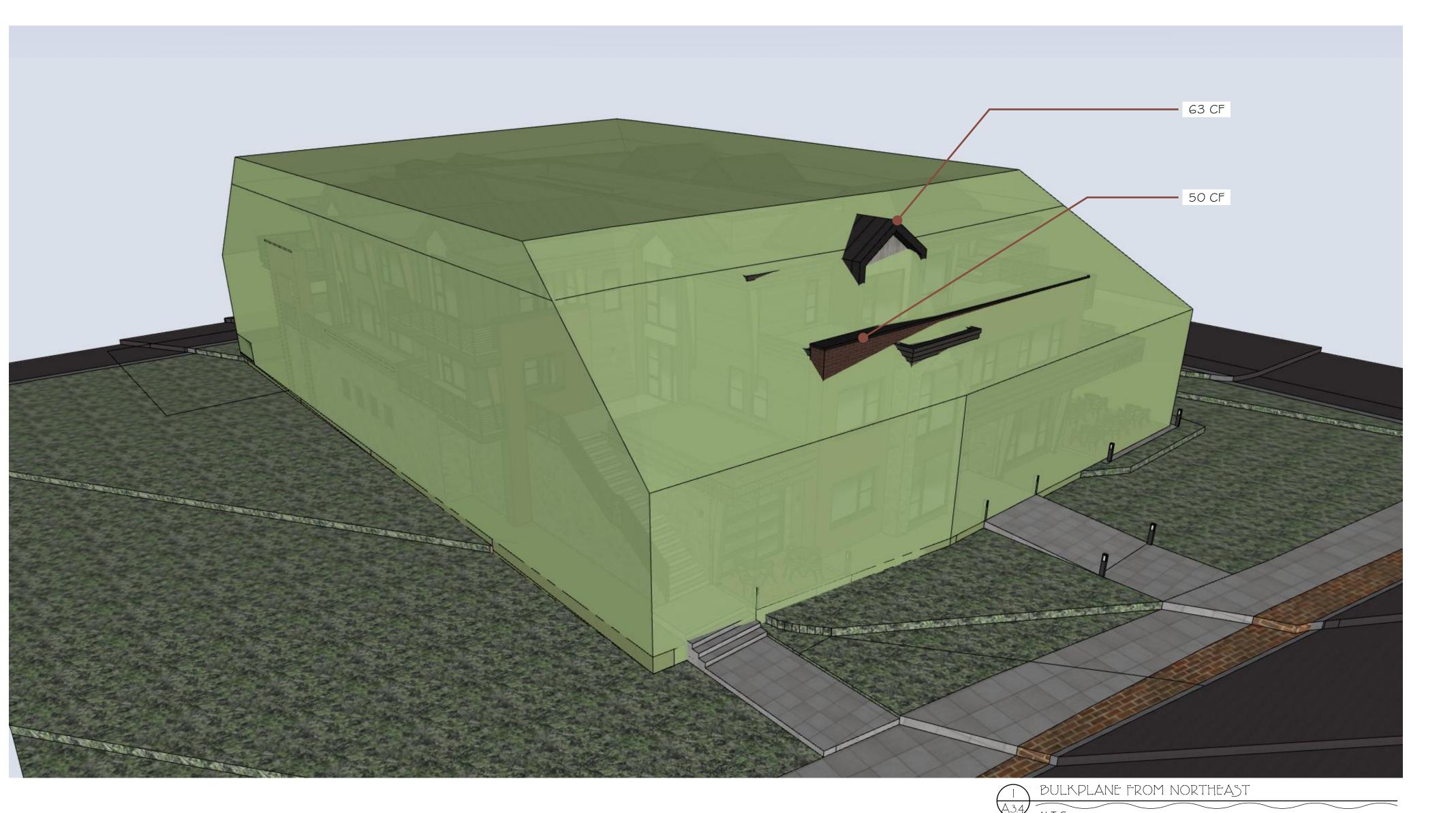
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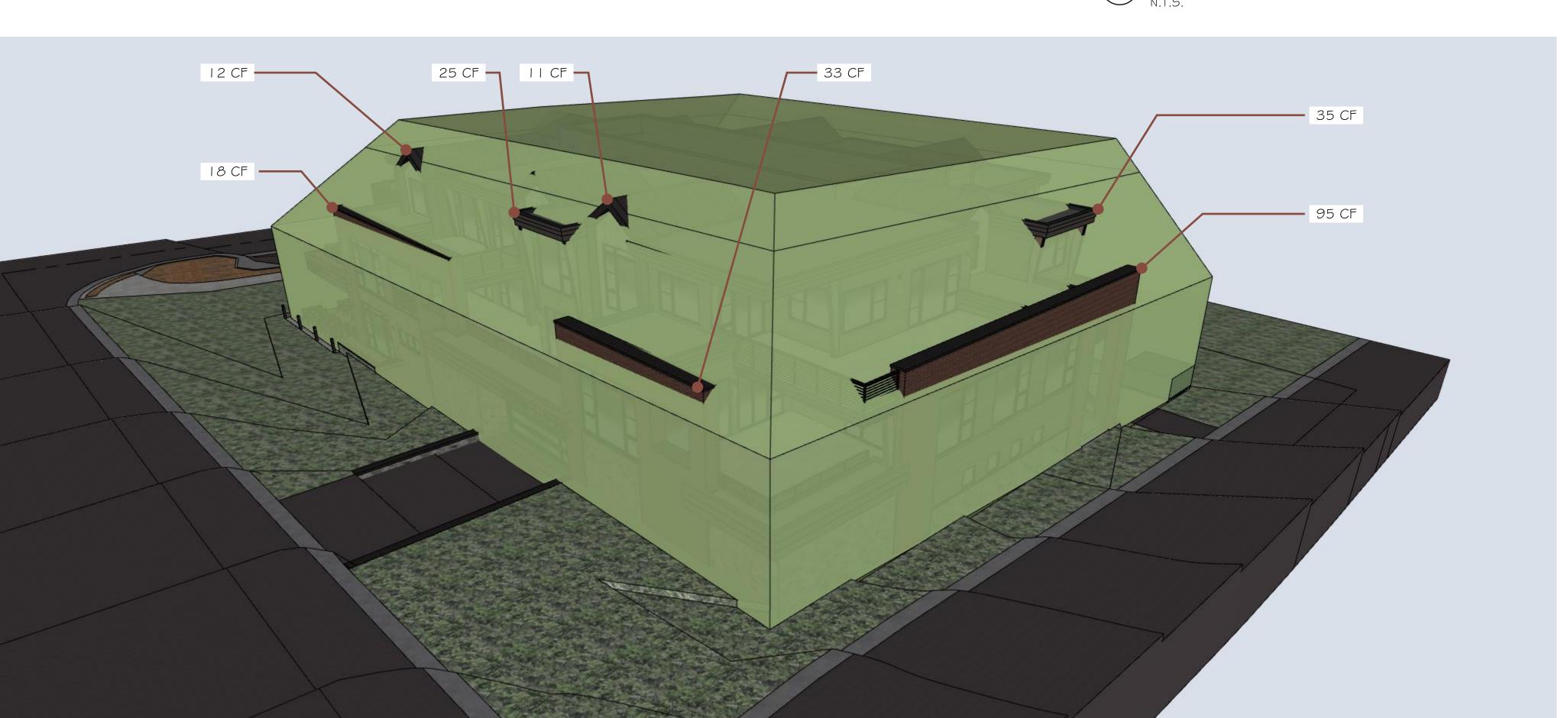
PROJECT# 2233





ALLEN-GUERRA ARCHITECTURE
P.O. BOX 7488
BRECKENRIDGE . (OLORADO . 80424
PH: 9704537002
FAX: 9704537040
E-MAIL: INFO@ALLEN-GUERRACOM
WEB SITE: WWWALLEN-GUERRACOM

NOTE: 342 CF TOTAL BULK PLANE ENCROACHMENT



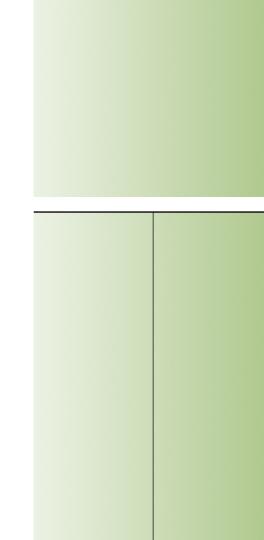
BULKPLANE FROM SOUTHWEST

ISSUE DATE PRELIM 2 MAY 2022 DRC 2 APR 2024 UPDATE 9 MAY 2024 SKETCH 6 SEP 2024 REVIEW 6 DEC 2024 PLANNING 24 JAN 2025 UPDATE 24 APR 2025 PROJECT# 2233

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ZIME SAUCE
3 9, 10, 11 & 12. BLOCK 2. KING SOLOMON SUBDIVISION #2
EAST MAIN STREET. TOWN OF FRISCO. COLORADO
UILDING SECTIONS

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| DRC | 2 APR 2024 |
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| SKETCH | 6 SEP 2024 |
| REVIEW | 6 DEC 2024 |
| PLANNING | 24 JAN 2025 |
| UPDATE | 24 APR 2025 |
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SLOCK 2. KING SOLOMON SUBDIVISION #2

FET. TOWN OF FRISCO. COLORADO

SECT. TOWN OF PRISCO.

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| ISSUE | DATE |
| PRELIM | 2 MAY 2022 |
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| REVIEW | 6 DEC 2024 |
| PLANNING | 24 JAN 2025 |
| UPDATE | 24 APR 2025 |
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| PROJEC" | T# 2233 |

A4.2





| | | | | | | CONCRETE SIDEWALK | < | | | | | | | | | | |
|------------------|--|--|--------------------|--------------------------|------------------------|----------------------|---------------|--|------------------|--|------------------|----------------------------|------------------|--------------------------------|---|------------------|-------------------------|
| 0.0 | $\sqrt{\begin{array}{c} \uparrow 0.0 \\ \hline 0.0 \end{array}}$ | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 0.0 | 0.0 | 0.0 | 0.0 | 0.0 0.0 | 0.0 0.0 | 0.0 | 0.0 0.0 | 0.0 | 0.0 | 0.0 | ⁺ 0 . |
| ⁺ 0.0 | 0.0 | 0.0 | [†] 0.0 | 0.0 5 9" | | [†] 0.0 | 0.1 | PROPOSTO CONCE CONNECTION TO SIDEWA | 0. F | 0" SP 0.1 | [†] 0.2 | 0.0 | 0.0 | CONC. ACCES (550 &F) 0.0 | 0.0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 | 0.0 | [†] 0. |
| 0.0 | 0.0 | 0.0 \.0 | 0.0 | [†] 0.1 | ⁺ 0.4 | O.7 D.6 MH: 3.5 | 3.0 B_0 | (450 3 . B | 3.9 | 3 | B 06.8 | 3.6 | 0.0 | 0.0 | 0.0 EDGE OF (E) B | UILDING | |
| ⁺ 0.0 | 0.0 | †o.o †o.o | 0.1 | 3.3 B | 6.3 6.1 4.3 4.3 | 3.8 | MH: 3 | 1.6 | MH W | | | W MH: 8 | 9.8 | EDG | BFAVER F | PLAZA COND |)() |
| 0.0 | 0.0 | 0.0 0.0 | 0.1 | ⁺ 2.7 B MH; | 3.5 3.5 2.5 4 | Q. 22 PATIO WH. 0 | | : 8 | MH: 8 | | | MH: 8 | 0.7 | OF (E) BUIL | | | |
| [†] 0.0 | ⁺ 0.0 | \\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\ | [†] 0.1 | [†] 1.3 B MH | 6.4 % + 1.6 | 5.9 5.2 V | RAN RAN PARAN | | STATION | | BACK BAR - 30' | | | 7.50 DING | | | |
| 0.0 | 0.0 | \\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\ | 0.0 | [†] 0.6 В | ∕/H: 3.5 \ 🛣 | | | PREP G' IN | | PREP 5'x17' | | | | 0.0 | | | |
| ⁺ 0.0 | ⁺ 0.0 | | [†] 0.0 | [†] 0.1 | 1.3 MI | H: 8 | A SERRY | | | THROUGH LINE (14) | 7' HOOD | * * * * * * * * * * | 0.4 | | FDG | | |
| ⁺ 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | TR. O | 9 Alet | SE S | | | | 302 | | | E OF (E) BUILDIN | | |
| 0.0 | 0.0 | 0.0 | 0.0 0.0 | 0.0 | 0.0 | W 22MH: 8 | | | | | | #80 0 2 | 33 | | - CO | EDGE OF (| E) BUII |
| 0.0 | 0.0 | \\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\ | 0.0 0.0 | 0.0 | 0.0 | W S | OPEN # | OPEN #2 | OPEN #3 | OPP #4 | PEN #5 | | | | 0.0 | 0.0 | ⁺ 0. |
| 0.0 | 0.0 | MAD 150N (75' F | 0.0 | 0.0 | 0.0 | MH: 8 | | | | | | m | REE REC | | 70 | 0.0 | ⁺ 0. |
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| | | | | | | 0.0 | | 0.0 | | | \$ 0' F | | | | | | |
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| LUMINAIRE | LUMINAIRE SCHEDULE | | | | | | | | | | | |
|-----------|--------------------|-------|---------------|--|-------|--------|----------|-------|--------|--|--|--|
| SYMBOL | QTY | LABEL | [MANUFAC] | CATALOG | WATTS | LUMENS | BUG | LLF | MTG HT | | | |
| 0 | 9 | В | McGRAW-EDISON | BRT6-A1-727-U-T5-42-BZ (DARK BRONZE) | 9.4 | 960 | B1-U0-G0 | 0.900 | 3.5 | | | |
| - | 1 | D | HALO | SMD4R6927WH | 9.5 | 690 | B0-U0-G0 | 0.900 | 8 | | | |
| Ю | 21 | W | LUMIERE | 9002-W1-RW-LED2797-W-BZ- L1-UNV-RSM (DARK BRONZE) | 10 | 685 | B1-U0-G0 | 0.900 | 8, 9 | | | |

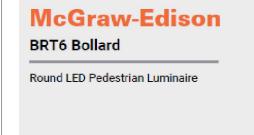
| CALCULATION STATISTICS | | |
|------------------------|--|--|
| REFLECTANCES | CALCULATION SPACING | CALCULATION HEIGHTS |
| DIRECT METHOD ONLY | OVERALL: 10'-0" x 10'-0" / DECK & PATIO: 5'-0" X 5'-0" | SITE: 0'-0" A.F.G. / PROPERTY LINE: 0'-0" A.F.G. |

| CALCULATION SUMMARY | | | | | | |
|---------------------|-------|------|------|-----|---------|---------|
| LABEL | UNITS | AVG | MAX | MIN | AVG/MIN | MAX/MIN |
| _OVERALL SITE | Fc | 0.17 | 22.3 | 0.0 | N.A. | N.A. |
| _PROPERTY LINE | Fc | 0.01 | 0.3 | 0.0 | N.A. | N.A. |
| DECK & STAIRS | Fc | 3.67 | 9.8 | 0.7 | 5.24 | 14.00 |
| PATIO | Fc | 4.76 | 8.2 | 1.0 | 4.76 | 8.20 |

FIXTURE 'B'







 Ordering Information page 2
 Product Specifications page 2 Optical Distributions page 2 • Energy and Performance Data page 3





HALO

SMD4 Series

4" Round and Square

 4 Optical Distributions
 Available in 30", 36", and 42" Lumen packages range from 560 - 4400 (5W - 49W)
Efficacy up to 122 lumens per watt
Zero uplight on all configurations

FIXTURE 'D'

Quick Facts



- Ultra-low profile surface luminaire with wide distribution
- Ceiling or wall mounting in compatible junction boxes
 Retrofit 4" recessed downlights with screwbase adapter included 2700K, 3000K, 3500K, 4000K or 5000K color temperature; 90 CRI
- Dimmable with 120V dimmer

FIXTURE 'W'





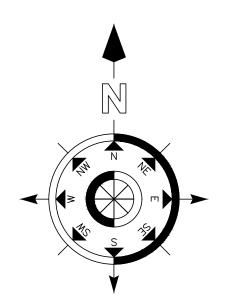
| | | COLOR MET | RIC SUMMARY | CANDLE | POWER DISTRIBUTION | CONE | OF LIGH | CANDELA TABLE | | | |
|--------------|-------------------------|-------------|-------------|--------|--|---------|-------------|---------------|---------|-------|-------|
| Test Number | P29422 | | Rf = 82.6 | | | Horizon | ıtal IIIumi | nance o | n Floor | Angle | 0-deg |
| | 9002-[W1]-X-FL-LED4080- | TM-30-15 | Rg = 93.8 | | | MH | FC | L | W | 0 | 5519 |
| Lumcat | M-BK-L1-UNV | | Ra = 84.2 | 0 | | 2' | 1379.8 | 0.6 | 0.6 | 5 | 4909 |
| | 2051 | CRI/CIE | | | The state of the s | 4' | 344.9 | 1.4 | 1.4 | 10 | 3391 |
| Lumens | 985 Lm | | R9 = 15.1 | 1,380 | THE STATE OF THE S | | | | | 15 | 1302 |
| Watts | 10 W | | | | | 6' | 153.3 | 2.2 | 2.2 | 20 | 457 |
| watts | 10 44 | | | | | 8. | 86.2 | 3 | 3 | 30 | 133 |
| LPW | 98.5 Lm/W | 2000K 3000K | 4000K 5000K | 2,760 | 50° | 10' | 55.2 | 3.8 | 3.8 | 40 | 22 |
| ССТ | 4000K | | 1 | | | 15' | 24.5 | 5.8 | 5.8 | 50 | 6 |
| 661 | 4000K | | | 4,139 | | 20' | 13.8 | 7.8 | 7.8 | 60 | 2 |
| SC (0/90/45) | 0.4/0.4/0.36 | | | 4,100 | 40° | 20 | 13.8 | 7.8 | 7.8 | 70 | 1 |
| Beam Angle | 23.1° | | | | | 30' | 6.1 | 11.8 | 11.8 | 80 | 0 |
| | | 2 | 5° | 5,519 | 10° \ 20° \ 30° | 40' | 3.4 | 15.8 | 15.8 | 90 | 0 |

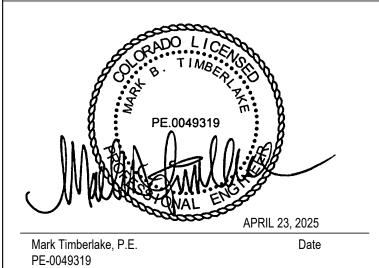
DRAWINGS ARE THE PROPERTY OF TIMBERLAKE ENGINEERING AND MAY NOT BE COPIED OR USED IN WHOLE OR PART WITHOUT THE WRITTEN PERMISSION OF TIMBERLAKE ENGINEERING.

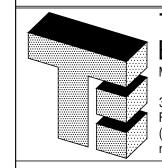
DO NOT SCALE THIS DRAWING. SOME DEVIATION FROM SCALE MAY OCCUR.

| REVISIONS: | DATE: |
|--|-----------|
| REVISED SITE BKGRD, ELIM. EAST 'W' FIXTURE | 4/23/2025 |
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TIMBERLAKE ENGINEERING, L.L.C.

Mark B. Timberlake P.E.

35 W.MAIN ST. #5657

FRISCO, CO 80443
(573) 881-5684

mark@teccengineering.com

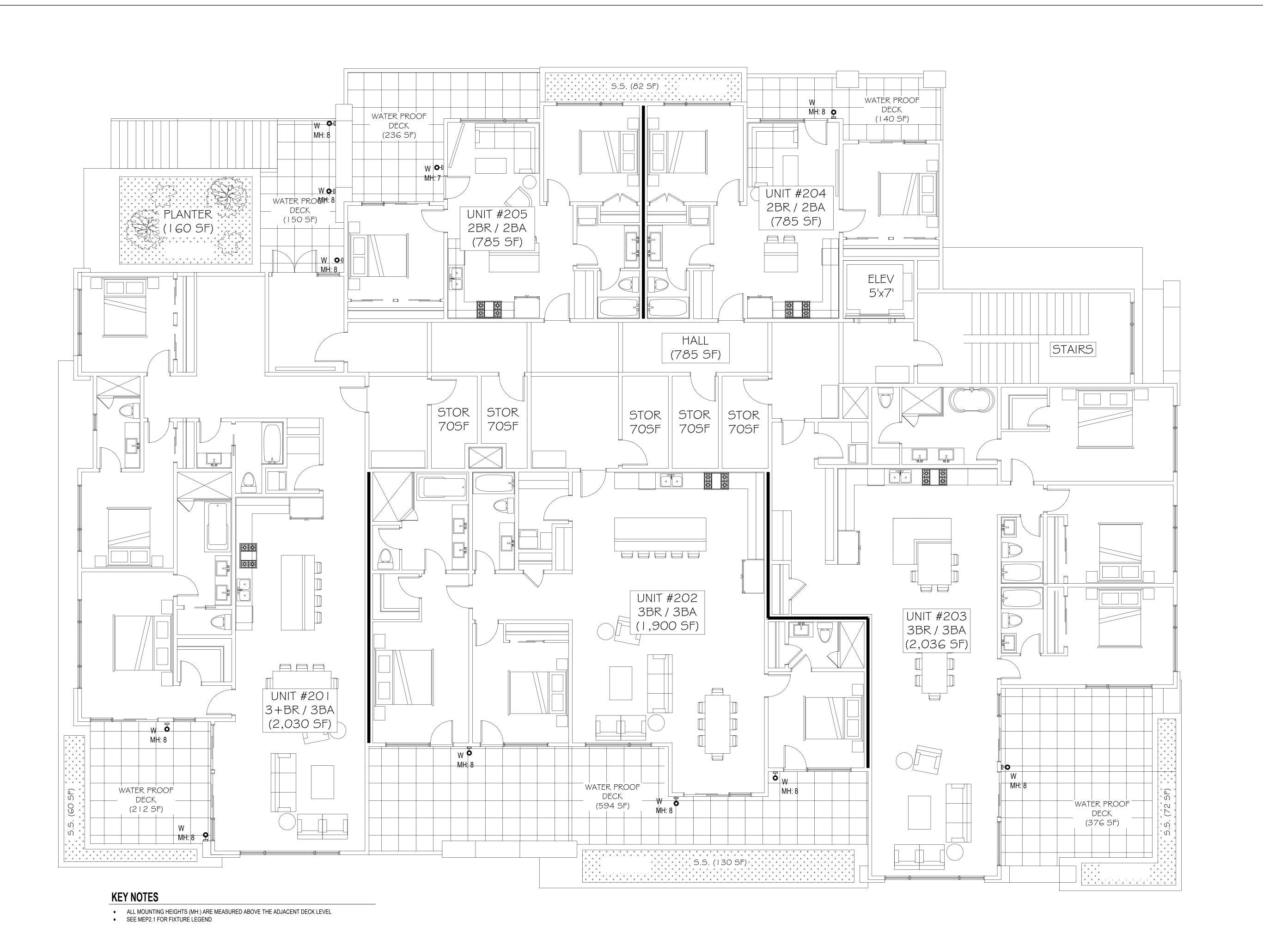
mark@tecoengineering.com

SITE LIGHTING PLAN

PRIME SAUCE

20 EAST MAIN STREET FEB. 27, 2025 | SHEET NO. DRAWN BY:

PROJECT NO:

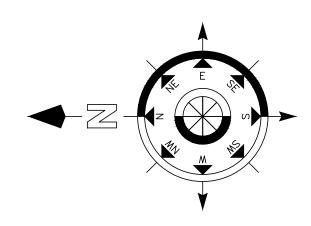


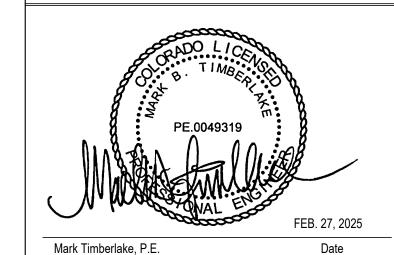
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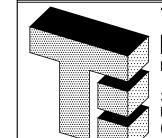
DO NOT SCALE THIS DRAWING. SOME DEVIATION FROM SCALE MAY OCCUR.

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PE-0049319

TIMBERLAKE ENGINEERING, L.L.C.

Mark B. Timberlake P.E.

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(573) 881-5684

mark@teccengineering.com

mark@tecoengineering.com

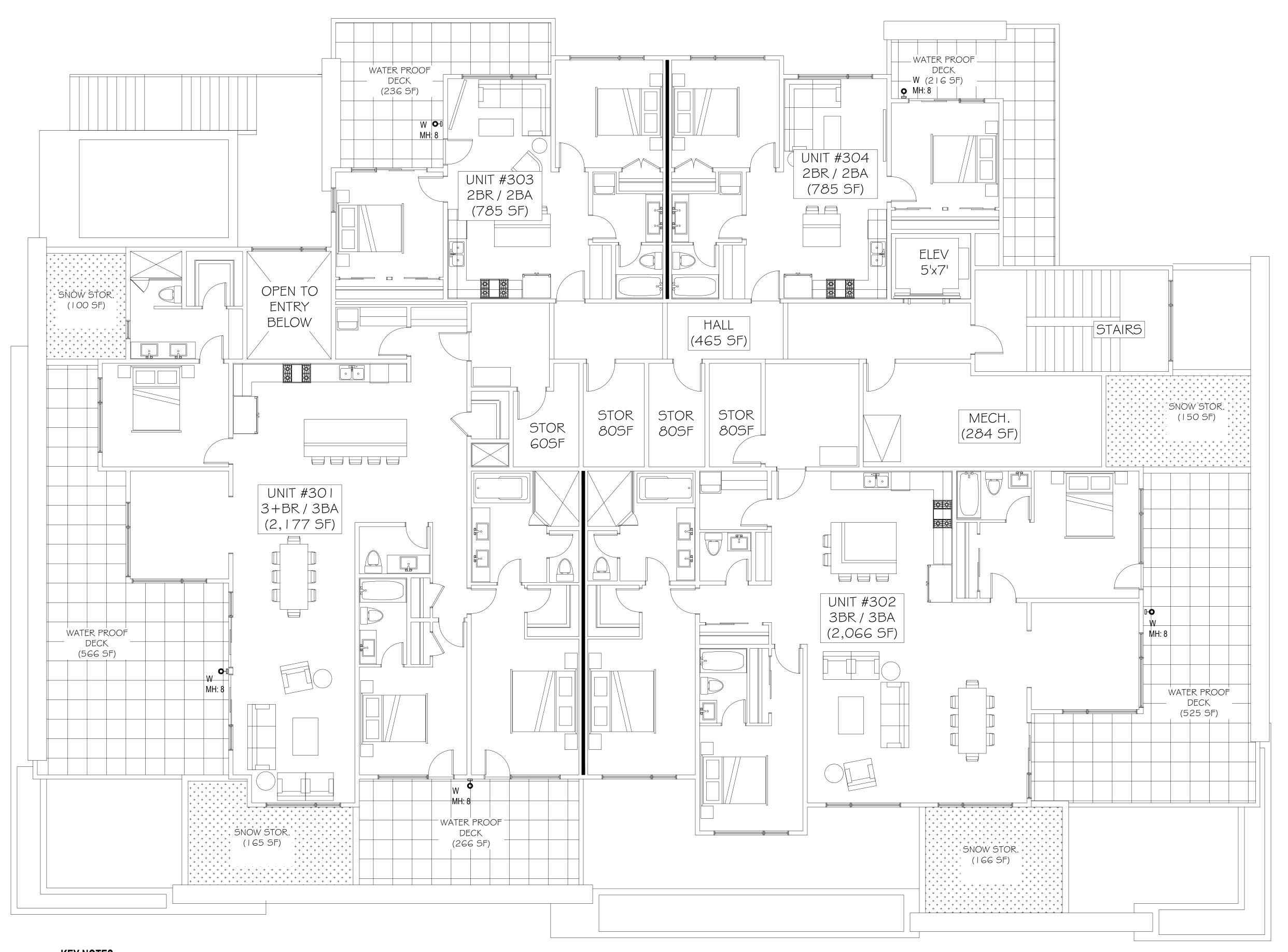
BUILDING LIGHTING PLAN - 2ND FL.

PRIME SAUCE

20 EAST MAIN STREET FEB. 27, 2025 | SHEET NO.

DRAWN BY: PROJECT NO: 3/16" = 1'- 0"

MEP2.2



KEY NOTES

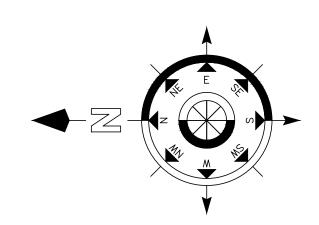
ALL MOUNTING HEIGHTS (MH:) ARE MEASURED ABOVE THE ADJACENT DECK LEVEL
 SEE MEP2.1 FOR FIXTURE LEGEND

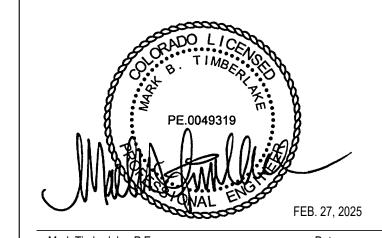
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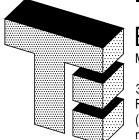
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Mark Timberlake, P.E. PE-0049319



TIMBERLAKE ENGINEERING, L.L.C.

Mark B. Timberlake P.E.

35 W.MAIN ST. #5657
FRISCO, CO 80443
(573) 881-5684
mark@tecoengineering.com

BUILDING LIGHTING PLAN - 3RD LEVEL

PRIME SAUCE

20 EAST MAIN STREET FEB. 27, 2025 | SHEET NO.

DRAWN BY: 3/16" = 1'- 0"

LSC TRANSPORTATION CONSULTANTS, INC.



January 22, 2025

Mr. Stephen Lunney Mountain Building Solutions 610 Main Street, #11 Frisco, CO 80443 1889 York Street Denver, CO 80206 (303) 333-1105 FAX (303) 333-1107 E-mail: lscdenver@lsctrans.com

Re: Prime Sauce Frisco, CO LSC #240670

Dear Mr. Lunney:

In response to your request, LSC Transportation Consultants, Inc. has prepared this traffic impact analysis for the proposed Prime Sauce development. As shown on Figure 1, the site is located south of W. Main Street and east of S. Madison Avenue in Frisco, Colorado.

REPORT CONTENTS

The report contains the following: the existing roadway and traffic conditions in the vicinity of the site including the lane geometries, traffic controls, posted speed limits, etc.; the existing weekday peak-hour traffic volumes; the typical weekday site-generated traffic volume projections for the site; the assignment of the projected traffic volumes to the area roadways; the projected background and resulting total traffic volumes on the area roadways; the site's projected traffic impacts; and any recommended roadway improvements to mitigate growth in background traffic or from the impact of the site.

LAND USE AND ACCESS

The site is proposed to include about 9 townhome dwelling units and a 3,324 square-foot restaurant. Access is proposed from S. Madison Avenue as shown in the conceptual site plan in Figure 2.

ROADWAY AND TRAFFIC CONDITIONS

Area Roadways

The major roadways in the site's vicinity are shown on Figure 1 and are described below.

- **W. Main Street** is an east-west, two-lane roadway north of the site. The intersection with S. Madison Street is all-way stop-sign controlled. The posted speed limit in the vicinity of the site is 25 mph west of S. Madison Street and 20 mph east of Madison Street.
- **S. Madison Avenue** is a north-south, two-lane roadway west of the site. The intersection with W. Main Street is all-way stop-sign controlled and the intersection with Granite Alley is two-way stop-sign controlled. The posted speed limit in the vicinity of the site is 20 mph.

Existing Traffic Conditions

Figure 3a shows the existing October traffic volumes, existing traffic control, and lane geometry in the site's vicinity on a typical weekday. The weekday peak-hour traffic volumes and daily traffic counts are from the attached traffic counts conducted by Counter Measures in October, 2024.

Figure 3b shows the estimated July, 2024 volumes, existing traffic control, and lane geometry in the site's vicinity on a typical weekday. A seasonal adjustment factor of 1.34 was applied to the October traffic counts conducted in October, 2024 based on CDOT continuous count locations on US 6 west of Swan Road and on SH 9 south of Tiger Road.

2026 and 2045 Background Traffic

Figure 4 shows the estimated 2026 background traffic and Figure 5 shows the estimated 2045 background traffic. The area is mostly built out but the background traffic estimates assume one percent annual growth to maintain a conservative analysis.

Existing, 2026, and 2045 Background Levels of Service

Level of service (LOS) is a quantitative measure of the level of congestion or delay at an intersection. Level of service is indicated on a scale from "A" to "F." LOS A is indicative of little congestion or delay and LOS F is indicative of a high level of congestion or delay. Attached are specific level of service definitions for unsignalized intersections.

The intersections in the study area were analyzed as appropriate to determine the existing, 2026, and 2045 background levels of service using Synchro Version 11. Table 1 shows the level of service analysis results. The level of service reports are attached.

- Madison Avenue/Main Street: This all-way stop controlled intersection currently operates at an overall LOS "B" during both morning and afternoon peak-hours and is expected to do so through 2026. The intersection is expected to operate at LOS "C" during both morning and afternoon peak-hours through 2045.
- Madison Avenue/Peak School Driveway: All movements at this stop-sign controlled intersection currently operate at LOS "B" or better and are expected to do so through 2045.
- 3. Madison Avenue/Site Access: This intersection was analyzed only in the total traffic scenarios.
- Madison Avenue/Granite Alley: All movements at this stop-sign controlled intersection currently operate at LOS "A" during both morning and afternoon peak-hours and are expected to operate at LOS "B" or better through 2045.

TRIP GENERATION

Table 2 shows the estimated average weekday, morning peak-hour, and afternoon peak-hour trip generation for the proposed site for three separate scenarios based on the rates from Trip Generation, 11th Edition, 2021 by the Institute of Transportation Engineers (ITE).

The site is projected to generate about 421 vehicle-trips on the average weekday, with about half entering and half exiting during a 24-hour period. During the morning peak-hour, which generally occurs for one hour between 6:30 and 8:30 a.m., about 18 vehicles would enter and about 17 vehicles would exit the site. During the afternoon peak-hour, which generally occurs for one hour between 4:00 and 6:00 p.m., about 21 vehicles would enter and about 14 vehicles would exit.

TRIP DISTRIBUTION

Figure 6 shows the estimated directional distribution of the site-generated traffic volumes on the area roadways. The estimates were based on the location of the site with respect to the regional population, employment, and activity centers; and the site's proposed land use.

TRIP ASSIGNMENT

Figure 7 shows the site-generated traffic volumes which are the directional distribution percentages (from Figure 6) applied to the trip generation estimate (from Table 2).

2026 AND 2045 TOTAL TRAFFIC

Figure 8 shows the 2026 total traffic which is the sum of the 2026 background traffic volumes (from Figure 4) and the site-generated traffic volumes (from Figure 7). Figure 8 also shows the 2026 total traffic lane geometry and traffic control.

Figure 9 shows the 2045 total traffic which is the sum of the 2045 background traffic volumes (from Figure 5) and the site-generated traffic volumes (from Figure 7). Figure 9 also shows the 2045 total traffic lane geometry and traffic control.

PROJECTED LEVELS OF SERVICE

The intersections in the study area were analyzed to determine the 2026 and 2045 total levels of service. Table 1 shows the level of service analysis results for each movement or lane group. The level of service reports are attached.

- 1. **Madison Avenue/Main Street:** This all-way stop controlled intersection is expected to operate at an overall LOS "B" during both morning and afternoon peak-hours through 2026 and at LOS "C" through 2045.
- 2. **Madison Avenue/Peak School Driveway:** All movements at this stop-sign controlled intersection are expected to operate at LOS "B" or better through 2045.
- **3. Madison Avenue/Site Access:** All movements at this stop-sign controlled intersection are expected to operate at LOS "A" through 2045.
- **4. Madison Avenue/Granite Alley:** All movements at this stop-sign controlled intersection are expected to operate at LOS "B" or better through 2045.

CONCLUSIONS AND RECOMMENDATIONS

Trip Generation

1. The site is projected to generate about 421 vehicle-trips on the average weekday, with about half entering and half exiting during a 24-hour period. During the morning peak-hour, about 18 vehicles would enter and about 17 vehicles would exit the site. During the afternoon peak-hour, about 21 vehicles would enter and about 14 vehicles would exit.

Projected Levels of Service

- 2. The all-way stop controlled Madison Avenue/Main Street intersection is expected to operate at an overall LOS "C" or better through 2045 with or without development of the site.
- 3. All movements at the unsignalized intersections analyzed are expected to operate at LOS "B" or better during both morning and afternoon peak-hours through 2045.

Conclusions

4. The impact of the Prime Sauce development can be accommodated by the existing roadway network.

Recommendations

5. The site access approach to Madison Avenue should be stop-sign controlled.

* * * * *

We trust our findings will assist you in gaining approval of the proposed Prime Sauce development. Please contact me if you have any questions or need further assistance.

Respectfully submitted,

LSC Transportation

1 8

By:_/

Christopher S. McGranahan, P.

Principal/Preside

CSM/wc

1-22-25

Enclosures: Tables 1 and 2

Figures 1 - 9

Traffic Count Reports Level of Service Definitions Level of Service Reports

Table 1
Intersection Levels of Service Analysis
Prime Sauce
Frisco, CO
LSC #240670; January, 2025

| | | | | 2026 | | 20 | 26 | 20 | 45 | 2045 | |
|--|---------|----------|----------|----------|------------|----------|----------|----------|-------------|----------|----------|
| | | Existing | Traffic | Backgrou | nd Traffic | Total | Traffic | Backgrou | ınd Traffic | Total | Traffic |
| | | Level of | Level of | Level of | Level of | Level of | Level of | Level of | Level of | Level of | Level of |
| | Traffic | Service | Service | Service | Service | Service | Service | Service | Service | Service | Service |
| Intersection No. and Location | Control | AM | PM | AM | PM | AM | PM | AM | PM | AM | PM |
| | | | | | | | | | | | _ |
| Madison Avenue/Main Street | AWSC | | | | | | | | | | _ |
| NB Approach | | Α | В | Α | В | Α | В | В | В | В | С |
| EB Approach | | В | В | В | В | В | С | С | С | С | D |
| WB Approach | | Α | В | Α | В | Α | В | В | С | В | С |
| SB Approach | | Α | В | Α | В | Α | В | Α | В | Α | В |
| Entire Intersection Delay (sec /veh) | | 11.6 | 12.8 | 11.9 | 13.1 | 12.2 | 13.8 | 15.8 | 19.4 | 17.2 | 21.2 |
| Entire Intersection LOS | | В | В | В | В | В | В | С | С | С | С |
| Madison Avenue/Peak School Driveway | TWSC | | | | | | | | | | |
| NB Approach | | Α | Α | Α | Α | Α | Α | Α | Α | Α | Α |
| EB Approach | | В | В | В | В | В | В | В | В | В | В |
| WB Approach | | В | B | | | | | | | | |
| SB Approach | | Ā | Ā | Α | Α | Α | Α | Α | Α | Α | Α |
| Critical Movement Delay | | 11.3 | 10.9 | 11.3 | 10.9 | 11.7 | 11.3 | 12.1 | 11.6 | 12.6 | 12.1 |
| 2) Madiaan Ayanya/Sita Aasaa | TWSC | | | | | | | | | | |
| 3) Madison Avenue/Site Access | TVVSC | | | | | ^ | Δ. | | | ^ | Δ. |
| WB Approach | | | | | | A | A | | | A | A |
| SB Approach | | | | | | A | A | | | A | Α |
| Critical Movement Delay | | | | | | 9.1 | 9.4 | | | 9.2 | 9.7 |
| 4) <u>Madison Avenue/Granite Alley</u> | TWSC | | | | | | | | | | |
| WB Approach | | Α | Α | Α | Α | Α | Α | Α | В | Α | В |
| SB Approach | | Α | Α | Α | Α | Α | Α | Α | Α | Α | Α |
| Critical Movement Delay | | 9.4 | 9.9 | 9.4 | 9.9 | 9.4 | 9.9 | 9.8 | 10.4 | 9.8 | 10.4 |
| | | | | | | | | | | | |

Table 2 ESTIMATED TRAFFIC GENERATION Prime Sauce Frisco, CO LSC #240670; January, 2025

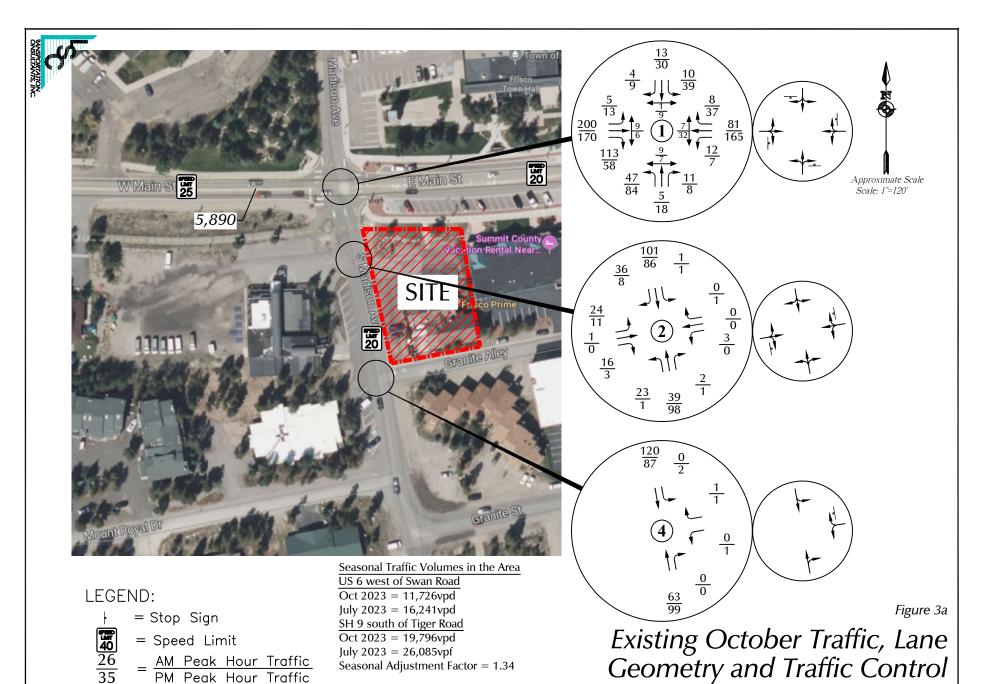
| | | Trip Generation Rates (1) | | | | | Total Trips Generated | | | | |
|----------------------------|---------------------|---------------------------|--------------|-------|--------------|---------|-----------------------|--------------|-----|--------------|-----|
| | | Average | AM Peak-Hour | | PM Peak-Hour | | Average | AM Peak-Hour | | PM Peak-Hour | |
| Trip Generating Category | Quantity | Weekday | ln | Out | ln | Out | Weekday | ln | Out | In | Out |
| Currently Proposed Land Us | se | | | | | | | | | | |
| Townhomes (2) | 9 DU ⁽³⁾ | 7.20 | 0.120 | 0.360 | 0.336 | 0.234 | 65 | 1 | 3 | 3 | 2 |
| Restaurant (4) | 3.324 KSF (5) | 107.20 | 5.264 | 4.306 | 5.521 | 3.529 | 356 | 17 | 14 | 18 | 12 |
| | | | | | | Total = | 421 | 18 | 17 | 21 | 14 |

Notes:

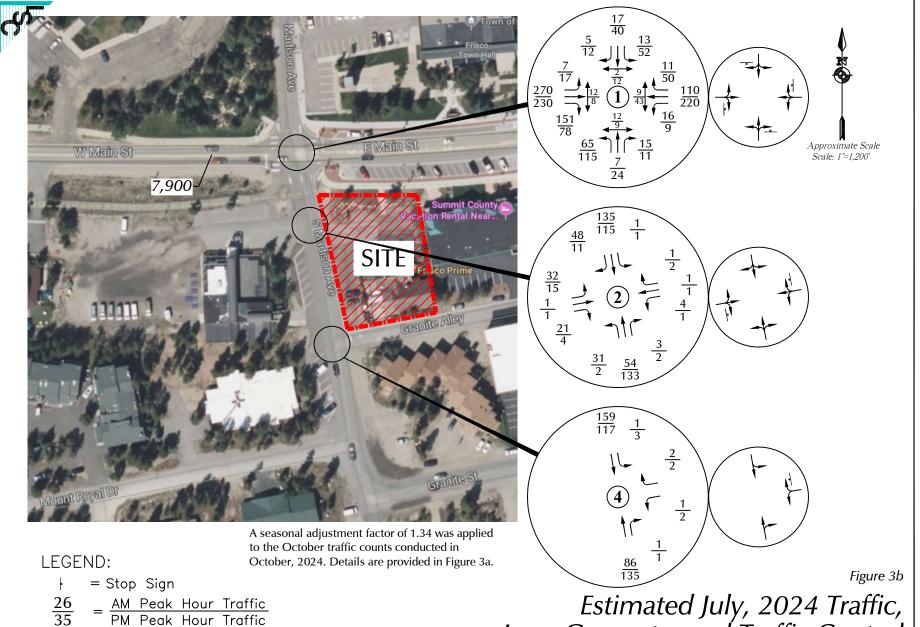
- (1) Source: Trip Generation, Institute of Transportation Engineers, 11th Edition, 2021
- (2) ITE Land Use No. 215 Single Family Attached Housing
- (3) DU = Dwelling Unit
- (4) ITE Land Use No. 932 High-Turnover Sit-Down Restaurant
- (5) KSF = 1,000 square feet





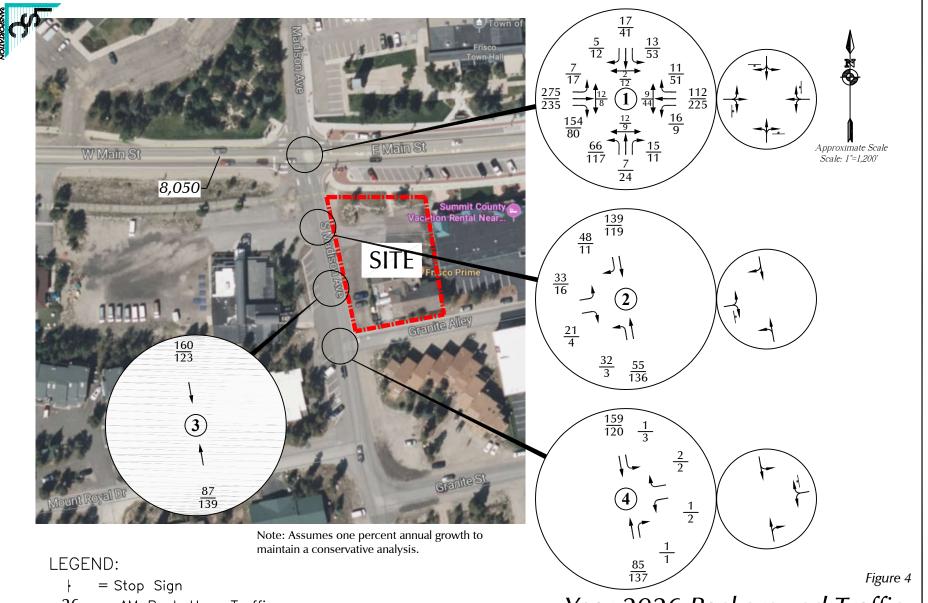


1,000 = Average Daily Traffic



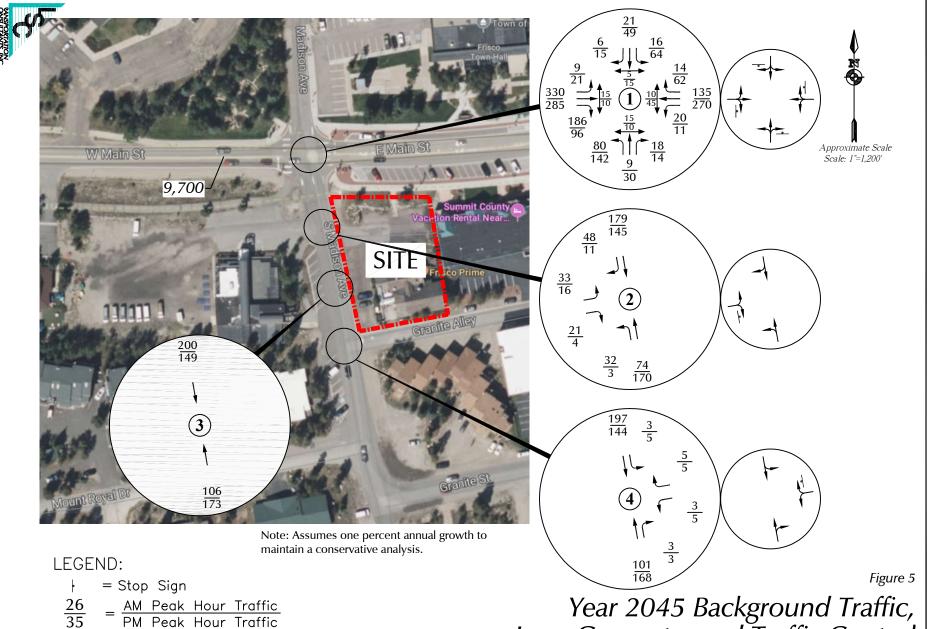
1,000 = Average Daily Traffic

Estimated July, 2024 Traffic, Lane Geometry and Traffic Control



 $\frac{26}{35} = \frac{\text{AM Peak Hour Traffic}}{\text{PM Peak Hour Traffic}}$ 1,000 = Average Daily Traffic

Year 2026 Background Traffic, Lane Geometry and Traffic Control



1,000 = Average Daily Traffic

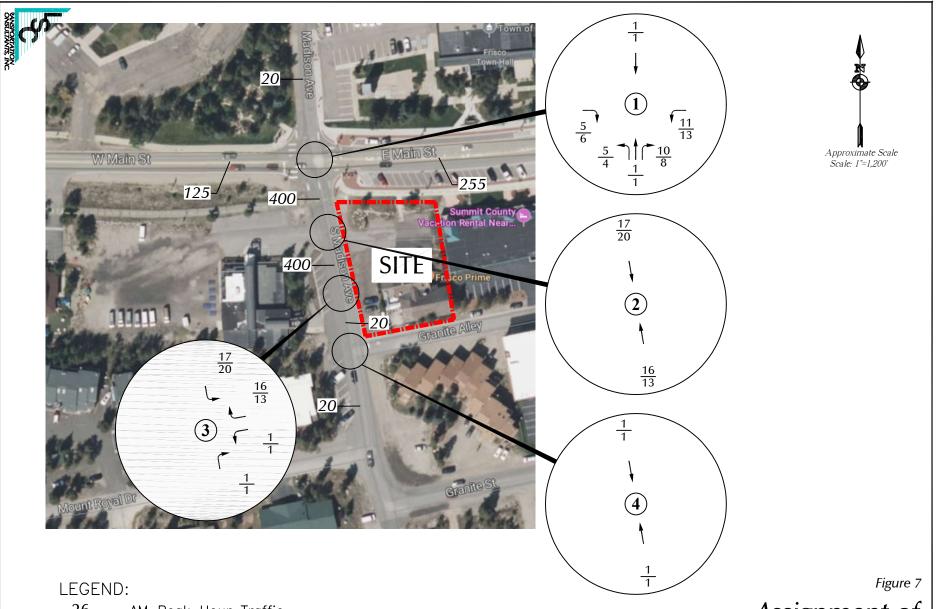
Year 2045 Background Traffic, Lane Geometry and Traffic Control



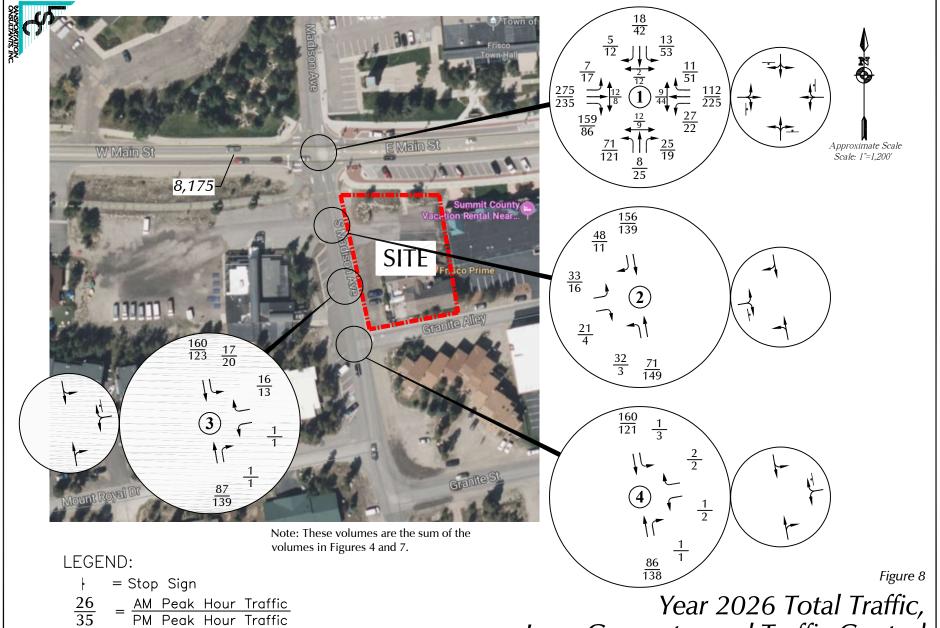
Figure 6

LEGEND: Percent Directional Distribution

Directional Distribution of Site-Generated Traffic

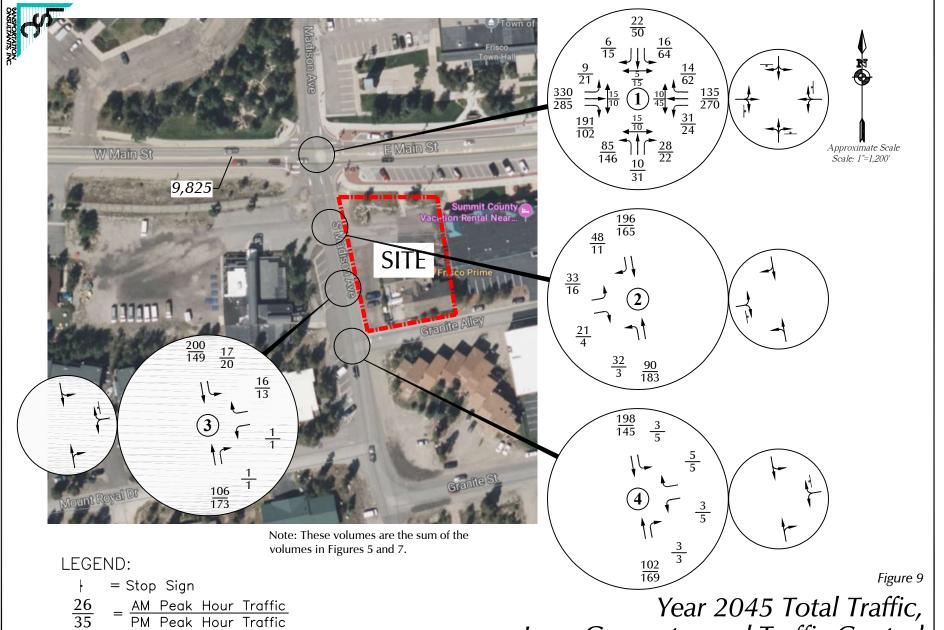


 $\frac{26}{35}$ = $\frac{AM \ Peak \ Hour \ Traffic}{PM \ Peak \ Hour \ Traffic}$ 1,000 = Average Daily Traffic Assignment of Site-Generated Traffic



1,000 = Average Daily Traffic

Year 2026 Total Traffic, Lane Geometry and Traffic Control



1,000 = Average Daily Traffic

Year 2045 Total Traffic, Lane Geometry and Traffic Control

1889 YORK STREET DENVER.COLORADO

N/S STREET: MADISON AVE E/W STREET: GRANITE ALLEY

CITY: FRISCO COUNTY: SUMMIT

File Name: MADIGRANITEALLEY 303-333-7409 Site Code : 00000025

Start Date : 10/15/2024

Page No : 1

Groups Printed- Not Used

| | N | MADISC | ON AVE | | G | RANIT | E ALLE | Y | | MADISC | ON AVE | | | NO AC | CESS | | |
|-------------|------|--------|--------|------|------|-------|--------|------|------|--------|--------|------|------|-------|-------|------|---------------|
| | | South | bound | | | West | oound | | | North | oound | | | Eastb | ound | | |
| Start Time | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Int. Total |
| Factor | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | |
| 06:30 AM | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 6 |
| 06:45 AM | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 9 |
| Total | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 15 |
| | | | | 1 | ì | | | | | | | | | | | 1 | |
| 07:00 AM | 1 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 20 |
| 07:15 AM | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 14 |
| 07:30 AM | 0 | 22 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 34 |
| 07:45 AM | 0 | 41 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 27 | 0 | 0 | 0 | 0 | 0 | 0 | 68_ |
| Total | 1 | 77 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 58 | 0 | 0 | 0 | 0 | 0 | 0 | 136 |
| 08:00 AM | 0 | 21 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 32 |
| 08:15 AM | 0 | 22 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 0 | 0 | 39 |
| 06.13 AW | U | 22 | U | U | U | U | ' | U | U | 10 | U | U | U | U | U | U | 39 |
| Total | 0 | 43 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 27 | 0 | 0 | 0 | 0 | 0 | 0 | 71 |
| | | | | ' | | | | ' | | | | , | | | | ' | |
| 04:00 PM | 0 | 25 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 34 | 0 | 1 | 0 | 0 | 0 | 0 | 62 |
| 04:15 PM | 0 | 15 | 0 | ő | 0 | 0 | 0 | 0 | 0 | 25 | 0 | 0 | 0 | 0 | 0 | ő | 40 |
| 04:30 PM | 1 | 27 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 24 | 0 | 0 | 0 | 0 | 0 | 0 | 52 |
| 04:45 PM | 1 | 20 | Ö | ő | Ö | Ö | Ö | ő | 0 | 20 | 0 | ő | 0 | 0 | Ő | 0 | 41 |
| Total | 2 | 87 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 103 | 0 | 1 | 0 | 0 | 0 | 0 | 195 |
| 05:00 PM | 0 | 34 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 21 | 0 | 0 | 0 | 0 | 0 | 0 | 56 |
| 05:15 PM | 0 | 23 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 32 | 1 | 0 | 0 | 0 | 0 | 0 | 56 |
| 05:30 PM | 0 | 11 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 26 |
| 05:45 PM | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 29 |
| Total | 0 | 82 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 82 | 1 | 0 | 0 | 0 | 0 | 0 | 167 |
| Grand Total | 3 | 296 | 0 | 0 | 1 | 0 | 4 | 0 | 0 | 278 | 1 | 1 | 0 | 0 | 0 | 0 | 584 |
| Apprch % | 1.0 | 99.0 | 0.0 | 0.0 | 20.0 | 0.0 | 80.0 | 0.0 | 0.0 | 99.3 | 0.4 | 0.4 | 0.0 | 0.0 | 0.0 | 0.0 | |
| Total % | 0.5 | 50.7 | 0.0 | 0.0 | 0.2 | 0.0 | 0.7 | 0.0 | 0.0 | 47.6 | 0.2 | 0.2 | 0.0 | 0.0 | 0.0 | 0.0 | |
| | | | | | | | | | | | | | | | | | |

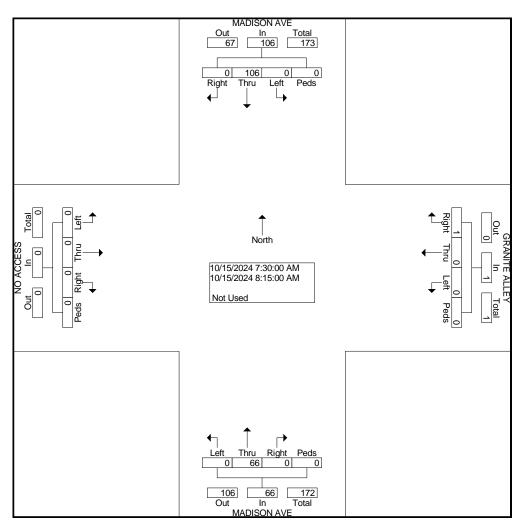
1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: GRANITE ALLEY

CITY: FRISCO COUNTY: SUMMIT File Name: MADIGRANITEALLEY

Site Code : 00000025 Start Date : 10/15/2024 Page No : 2

| | | | DISON | | | | _ | NITE A | | , | | | ISON | | | | | ACC | | | |
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| | | Sc | uthbo | und | | | W | estbou | ınd | | | No | orthbou | und | | | Ea | astbou | ınd | | |
| Start | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Int. |
| Time | Leit | u | ht | s | Total | Leit | u | ht | s | Total | Leit | u | ht | s | Total | Leit | u | ht | s | Total | Total |
| Peak Hour I | rom 0 | 7:30 A | AM to (| 08:15 | AM - Pe | eak 1 c | of 1 | | | | | | | | | | | | | | |
| Intersecti | 07:30 | ΔΜ | | | | | | | | | | | | | | | | | | | |
| on | 07.50 | / Alvi | | | | | | | | | | | | | | | | | | | |
| Volume | 0 | 106 | 0 | 0 | 106 | 0 | 0 | 1 | 0 | 1 | 0 | 66 | 0 | 0 | 66 | 0 | 0 | 0 | 0 | 0 | 173 |
| Percent | 0.0 | 100 | 0.0 | 0.0 | | 0.0 | 0.0 | 100 | 0.0 | | 0.0 | 100 | 0.0 | 0.0 | | 0.0 | 0.0 | 0.0 | 0.0 | | |
| | 0.0 | .0 | 0.0 | 0.0 | | 0.0 | 0.0 | .0 | 0.0 | | 0.0 | .0 | 0.0 | 0.0 | | 0.0 | 0.0 | 0.0 | 0.0 | | |
| 07:45 | 0 | 41 | 0 | 0 | 41 | 0 | 0 | 0 | 0 | 0 | 0 | 27 | 0 | 0 | 27 | 0 | 0 | 0 | 0 | 0 | 68 |
| Volume | Ŭ | | Ŭ | Ū | • • • | | Ŭ | Ŭ | Ū | Ū | | | Ū | Ū | | | Ŭ | · | Ū | Ū | |
| Peak | | | | | | | | | | | | | | | | | | | | | 0.636 |
| Factor | | | | | | | | | | | | | | | | | | | | | |
| High Int. | 07:45 | 5 AM | | | | 08:15 | AM | | | | 07:45 | 5 AM | | | | | | | | | |
| Volume | 0 | 41 | 0 | 0 | 41 | 0 | 0 | 1 | 0 | 1 | 0 | 27 | 0 | 0 | 27 | | | | | | |
| Peak | | | | | 0.64 | | | | | 0.25 | | | | | 0.61 | | | | | | |
| Factor | | | | | 6 | | | | | 0 | | | | | 1 | | | | | | |



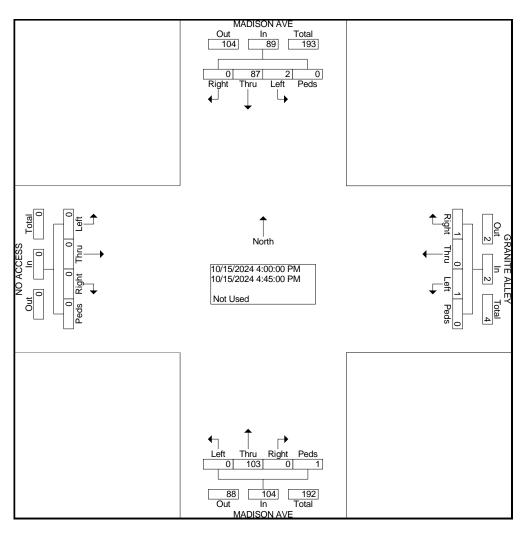
1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: GRANITE ALLEY

CITY: FRISCO COUNTY: SUMMIT File Name: MADIGRANITEALLEY

Site Code : 00000025 Start Date : 10/15/2024 Page No : 3

| | | | DISON | | | | - | | ALLEY | , | | | DISON | | | | | ACC | | | |
|------------------|-------|----------|---------|-------|---------|----------|------|----------------|-------|-------|-------|----------|--------|-----|-------|------|-----|---------------|-----|-------|-------|
| | | Sc | uthbo | und | | | W | <u>estbo</u> i | und | | | No | orthbo | | | | E | <u>astbou</u> | ınd | | |
| Start | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Int. |
| Time | | u | ht | S | Total | | u | ht | S | Total | | u | ht | S | Total | | u | ht | S | Total | Total |
| Peak Hour I | rom 0 | 4:00 F | PM to (| 04:45 | PM - Pe | eak 1 d | of 1 | | | | | | | | | | • | | | | |
| Intersecti on | 04:00 | PM | | | | | | | | | | | | | | | | | | | |
| Volume | 2 | 87 | 0 | 0 | 89 | 1 | 0 | 1 | 0 | 2 | 0 | 103 | 0 | 1 | 104 | 0 | 0 | 0 | 0 | 0 | 195 |
| Percent | 2.2 | 97. 8 | 0.0 | 0.0 | | 50. 0 | 0.0 | 50. 0 | 0.0 | | 0.0 | 99. 0 | 0.0 | 1.0 | | 0.0 | 0.0 | 0.0 | 0.0 | | |
| 04:00 Volume | 0 | 25 | 0 | 0 | 25 | 1 | 0 | 1 | 0 | 2 | 0 | 34 | 0 | 1 | 35 | 0 | 0 | 0 | 0 | 0 | 62 |
| Peak Factor | | | | | | | | | | | | | | | | | | | | | 0.786 |
| High Int. | 04:30 | PM | | | | 04:00 | PM | | | | 04:00 | PM | | | | | | | | | |
| Volume | 1 | 27 | 0 | 0 | 28 | 1 | 0 | 1 | 0 | 2 | 0 | 34 | 0 | 1 | 35 | | | | | | |
| Peak | | | | | 0.79 | | | | | 0.25 | | | | | 0.74 | | | | | | |
| Factor | | | | | 5 | | | | | 0 | | | | | 3 | | | | | | |



1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: PEAK SCHOOL ENTRANCE

CITY: FRISCO COUNTY: SUMMIT

Groups Printed- VEHICLES

File Name: MADIPEAK Site Code : 00000025 Start Date : 10/15/2024 Page No : 1

| | N | MADIS(South | ON AVE | | BU | SINESS Westk | | SS | I | MADIS(Northl | ON AVE | | F | PEAK S ENTR Eastb | _ | - | |
|------------------------------------|-----------------|---------------------|-------------------|------------|------------------|-----------------|------------------|------------------|-------------------|---------------------|-----------------|-----------------|-------------------|-------------------------|-------------------|-----------------|---------------|
| Start Time | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Int. Total |
| Factor | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | |
| 06:30 AM | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 7 |
| 06:45 AM | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 9 |
| Total | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 16 |
| 07:00 AM | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 12 | 0 | 1 | 0 | 0 | 0 | 0 | 22 |
| 07:15 AM | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 15 |
| 07:30 AM | 0 | 26 | 7 | 0 | 1 | 0 | 0 | 0 | 3 | 8 | 0 | 0 | 2 | 0 | 1 | 0 | 48 |
| 07:45 AM | 1 | 35 | 18 | 0 | 0 | 0 | 0 | 1 | 17 | 7 | 0 | 0 | 13 | 1 | 10 | 1 | 104 |
| Total | 1 | 79 | 25 | 0 | 1 | 0 | 0 | 1 | 20 | 33 | 0 | 1 | 15 | 1 | 11 | 1 | 189 |
| 08:00 AM | 0 | 17 | 10 | 0 | 1 | 0 | 0 | 0 | 3 | 4 | 1 | 0 | 8 | 0 | 5 | 3 | 52 |
| 08:15 AM | 0 | 19 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 16 | 1 | 1 | 1 | 0 | 0 | 0 | 40 |
| | | | | | | | | | | | | | | | | | |
| Total | 0 | 36 | 11 | 0 | 2 | 0 | 0 | 0 | 3 | 20 | 2 | 1 | 9 | 0 | 5 | 3 | 92 |
| | | | | | | | | | | | | | | | | | |
| 04:00 PM | 1 | 22 | 1 | 0 | 0 | 0 | 1 | 0 | 1 | 29 | 0 | 1 | 3 | 0 | 2 | 1 | 62 |
| 04:15 PM | 0 | 17 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 18 | 0 | 0 | 3 | 0 | 0 | 0 | 41 |
| 04:30 PM | 0 | 25 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 21 | 1 | 0 | 5 | 0 | 1 | 0 | 57 |
| 04:45 PM | 0 | 24 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 39 |
| Total | 1 | 88 | 8 | 0 | 0 | 0 | 1 | 0 | 1 | 83 | 1 | 1 | 11 | 0 | 3 | 1 | 199 |
| 05:00 PM | 1 | 37 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 54 |
| 05:15 PM | 0 | 23 | 1 | 0 | 0 | 0 | 1 | 1 | 1 | 27 | 0 | 0 | 0 | 0 | 0 | 0 | 54 |
| 05:30 PM | 1 | 11 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 12 | 0 | 0 | 2 | 0 | 1 | 0 | 28 |
| 05:45 PM | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 28 |
| Total | 2 | 85 | 2 | 0 | 0 | 0 | 3 | 1 | 1 | 67 | 0 | 0 | 2 | 0 | 1 | 0 | 164 |
| Grand Total Apprch % Total % | 4 1.2 0.6 | 296 85.5 44.8 | 46 13.3 7.0 | 0.0 0.0 | 3 33.3 0.5 | 0 0.0 0.0 | 4 44.4 0.6 | 2 22.2 0.3 | 25 10.3 3.8 | 211 87.2 32.0 | 3 1.2 0.5 | 3 1.2 0.5 | 37 58.7 5.6 | 1 1.6 0.2 | 20 31.7 3.0 | 5 7.9 0.8 | 660 |

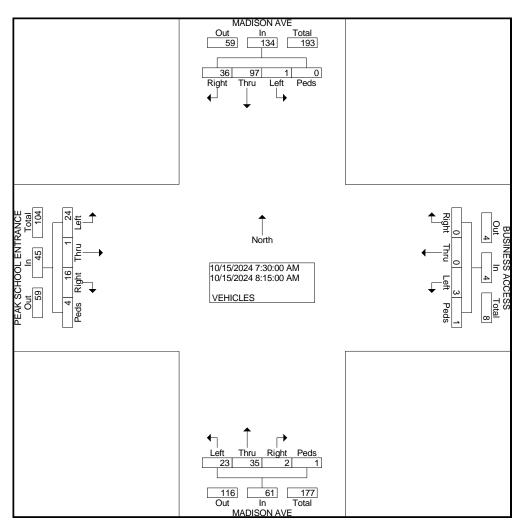
1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: PEAK SCHOOL ENTRANCE

CITY: FRISCO COUNTY: SUMMIT

File Name: MADIPEAK Site Code : 00000025 Start Date : 10/15/2024 Page No : 2

| | | | DISON | | | В | USIN | | | SS | | | ISON | | | PEA | | | | ANCE | |
|------------------|-------|---------------|----------|-------|---------|----------|------|--------|----------|-------|----------|----------|--------|-----|-------|----------|-----|----------|-----|-------|-------|
| | | So | uthbo | und | | | W | estbou | und | | | No | rthbou | und | | | Ea | astbou | ınd | | |
| Start | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Int. |
| Time | Leit | u | ht | s | Total | Leit | u | ht | S | Total | Leit | u | ht | s | Total | Leit | u | ht | s | Total | Total |
| Peak Hour I | rom 0 | 7:30 <i>F</i> | M to 0 | 08:15 | AM - Pe | eak 1 d | of 1 | | | | | | | | | | | | | | |
| Intersecti on | 07:30 | AM | | | | | | | | | | | | | | | | | | | |
| Volume | 1 | 97 | 36 | 0 | 134 | 3 | 0 | 0 | 1 | 4 | 23 | 35 | 2 | 1 | 61 | 24 | 1 | 16 | 4 | 45 | 244 |
| Percent | 0.7 | 72. 4 | 26. 9 | 0.0 | | 75. 0 | 0.0 | 0.0 | 25. 0 | | 37. 7 | 57. 4 | 3.3 | 1.6 | | 53. 3 | 2.2 | 35. 6 | 8.9 | | |
| 07:45 Volume | 1 | 35 | 18 | 0 | 54 | О | 0 | 0 | 1 | 1 | 17 | 7 | 0 | 0 | 24 | 13 | 1 | 10 | 1 | 25 | 104 |
| Peak | | | | | | | | | | | | | | | | | | | | | 0.587 |
| Factor | | | | | | | | | | | | | | | | | | | | | |
| High Int. | 07:45 | AM | | | | 07:30 | AM (| | | | 07:45 | AM | | | | 07:45 | AM. | | | | |
| Volume | 1 | 35 | 18 | 0 | 54 | 1 | 0 | 0 | 0 | 1 | 17 | 7 | 0 | 0 | 24 | 13 | 1 | 10 | 1 | 25 | |
| Peak | | | | | 0.62 | | | | | 1.00 | | | | | 0.63 | | | | | 0.45 | |
| Factor | | | | | 0 | | | | | 0 | | | | | 5 | | | | | 0 | |

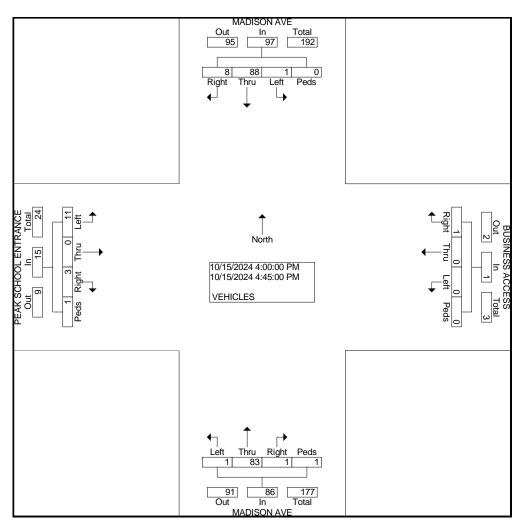


1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: PEAK SCHOOL ENTRANCE

CITY: FRISCO COUNTY: SUMMIT File Name: MADIPEAK Site Code : 00000025 Start Date : 10/15/2024 Page No : 3

| | | MAE | DISON | AVE | | В | USIN | ESS A | CCES | S | | MAD | DISON | AVE | | PEA | K SCH | OOL I | ENTR | ANCE | |
|------------------|--------|----------|---------|---------|---------|---------|------|-----------|------|-------|-------|----------|---------|-----|-------|----------|-------|----------|------|-------|-------|
| | | Sc | uthbo | und | | | W | estbou | ınd | | | No | orthbou | und | | | Ea | astbou | ınd | | |
| Start | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Int. |
| Time | Leit | u | ht | s | Total | Leit | u | ht | s | Total | Leit | u | ht | s | Total | Leit | u | ht | s | Total | Total |
| Peak Hour I | From 0 | 4:00 F | PM to (| 04:45 I | PM - P6 | eak 1 d | of 1 | | | | | | | | | | | | | | |
| Intersecti on | 04:00 | PM | | | | | | | | | | | | | | | | | | | |
| Volume | 1 | 88 | 8 | 0 | 97 | 0 | 0 | 1 | 0 | 1 | 1 | 83 | 1 | 1 | 86 | 11 | 0 | 3 | 1 | 15 | 199 |
| Percent | 1.0 | 90. 7 | 8.2 | 0.0 | | 0.0 | 0.0 | 100 .0 | 0.0 | | 1.2 | 96. 5 | 1.2 | 1.2 | | 73. 3 | 0.0 | 20. 0 | 6.7 | | |
| 04:00 Volume | 1 | 22 | 1 | 0 | 24 | 0 | 0 | 1 | 0 | 1 | 1 | 29 | 0 | 1 | 31 | 3 | 0 | 2 | 1 | 6 | 62 |
| Peak | | | | | | | | | | | | | | | | | | | | | 0.802 |
| Factor | | | | | | | | | | | | | | | | | | | | | |
| High Int. | 04:30 | PM | | | | 04:00 | PM | | | | 04:00 | PM | | | | 04:00 | PM | | | | |
| Volume | 0 | 25 | 4 | 0 | 29 | 0 | 0 | 1 | 0 | 1 | 1 | 29 | 0 | 1 | 31 | 3 | 0 | 2 | 1 | 6 | |
| Peak | | | | | 0.83 | | | | | 0.25 | | | | | 0.69 | | | | | 0.62 | |
| Factor | | | | | 6 | | | | | 0 | | | | | 4 | | | | | 5 | |



1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: W. MAIN ST

CITY: FRISCO COUNTY: SUMMIT

Groups Printed- VEHICLES
MADISON AVE

File Name: MADIMAIN Site Code : 00000008 Start Date : 10/15/2024 Page No : 1

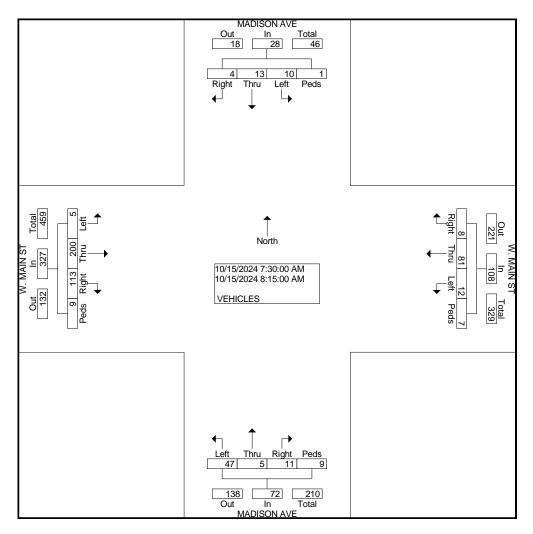
| | N | | ON AVE | | | W. MA | | | | MADIS | ON AVE | | | W. MA | IN ST | | |
|-------------|------|-------|--------|------|------|-------|-------|------|------|-------|--------|------|------|-------|-------|------|---------------|
| | | South | bound | | | West | oound | | | North | oound | | | Eastb | ound | | |
| Start Time | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Left | Thru | Right | Peds | Int. Total |
| Factor | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 | |
| 06:30 AM | 2 | 1 | 1 | 0 | 0 | 13 | 2 | 1 | 2 | 1 | 0 | 0 | 1 | 13 | 2 | 0 | 39 |
| 06:45 AM | 1_ | 0 | 0 | 0 | 1_ | 18 | 1 | 0 | 4 | 0 | 0 | 0 | 0 | 19 | 3 | 0 | 47 |
| Total | 3 | 1 | 1 | 0 | 1 | 31 | 3 | 1 | 6 | 1 | 0 | 0 | 1 | 32 | 5 | 0 | 86 |
| | | | | 1 | | | | 1 | | | | | | | | | |
| 07:00 AM | 0 | 4 | 0 | 1 | 1 | 14 | 1 | 4 | 12 | 0 | 2 | 2 | 0 | 21 | 3 | 1 | 66 |
| 07:15 AM | 0 | 3 | 2 | 0 | 1 | 11 | 4 | 1 | 2 | 1 | 1 | 0 | 0 | 40 | 7 | 0 | 73 |
| 07:30 AM | 3 | 0 | 1 | 0 | 3 | 17 | 2 | 3 | 9 | 0 | 2 | 3 | 1 | 55 | 35 | 5 | 139 |
| 07:45 AM | 1 | 3 | | 1 | 8 | 24 | 3 | 3 | 15 | 2 | 6 | 1 | 2 | 55 | 41 | 0 | 166 |
| Total | 4 | 10 | 4 | 2 | 13 | 66 | 10 | 11 | 38 | 3 | 11 | 6 | 3 | 171 | 86 | 6 | 444 |
| 08:00 AM | 3 | 8 | 1 | 0 | 0 | 24 | 2 | 0 | 9 | 2 | 2 | 4 | 2 | 46 | 18 | 4 | 125 |
| 08:15 AM | 3 | 2 | 1 | 0 | 1 | 16 | 1 | 1 | 14 | 1 | 1 | 1 | 0 | 44 | 19 | 0 | 105 |
| | - | _ | • | - 1 | - | | • | - 1 | | • | • | - 1 | - | | | - 1 | |
| Total | 6 | 10 | 2 | 0 | 1 | 40 | 3 | 1 | 23 | 3 | 3 | 5 | 2 | 90 | 37 | 4 | 230 |
| | | | | | | | | | | | | | | | | | |
| 04:00 PM | 8 | 12 | 1 | 1 | 1 | 43 | 15 | 19 | 27 | 7 | 4 | 3 | 6 | 37 | 12 | 0 | 196 |
| 04:15 PM | 10 | 6 | 3 | 4 | 3 | 41 | 8 | 4 | 23 | 4 | 0 | 0 | 4 | 37 | 10 | 2 | 159 |
| 04:30 PM | 3 | 8 | 4 | 1 | 2 | 47 | 6 | 0 | 19 | 4 | 4 | 4 | 3 | 51 | 15 | 4 | 175 |
| 04:45 PM | 18 | 4 | 1 | 3 | 1 | 34 | 8 | 9 | 15 | 3 | 0 | 0 | 0 | 45 | 21 | 0 | 162 |
| Total | 39 | 30 | 9 | 9 | 7 | 165 | 37 | 32 | 84 | 18 | 8 | 7 | 13 | 170 | 58 | 6 | 692 |
| | | | | | | | | | | | | | | | | | |
| 05:00 PM | 11 | 6 | 0 | 0 | 5 | 63 | 4 | 11 | 16 | 3 | 1 | 3 | 2 | 41 | 24 | 2 | 192 |
| 05:15 PM | 6 | 4 | 1 | 1 | 2 | 35 | 4 | 4 | 22 | 8 | 2 | 0 | 4 | 28 | 18 | 2 | 141 |
| 05:30 PM | 5 | 4 | 4 | 0 | 2 | 34 | 6 | 0 | 14 | 2 | 1 | 0 | 1 | 41 | 5 | 3 | 122 |
| 05:45 PM | 3 | 4 | 1_ | 6 | 1_ | 35 | 4 | 1 | 10 | 4 | 4 | 1 | 0 | 28 | 11 | 4 | 117 |
| Total | 25 | 18 | 6 | 7 | 10 | 167 | 18 | 16 | 62 | 17 | 8 | 4 | 7 | 138 | 58 | 11 | 572 |
| Grand Total | 77 | 69 | 22 | 18 | 32 | 469 | 71 | 61 | 213 | 42 | 30 | 22 | 26 | 601 | 244 | 27 | 2024 |
| Apprch % | 41.4 | 37.1 | 11.8 | 9.7 | 5.1 | 74.1 | 11.2 | 9.6 | 69.4 | 13.7 | 9.8 | 7.2 | 2.9 | 66.9 | 27.2 | 3.0 | 2024 |
| Total % | 3.8 | 3.4 | 1.1 | 0.9 | 1.6 | 23.2 | 3.5 | 3.0 | 10.5 | 2.1 | 1.5 | 1.1 | 1.3 | 29.7 | 12.1 | 1.3 | |
| Total 70 | 5.0 | 5.4 | 11 | 0.0 | 1.0 | 20.2 | 0.0 | 3.0 | 10.0 | 2.1 | 1.5 | | 1.5 | 20.1 | 12.1 | 1.5 | |

1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: W. MAIN ST

CITY: FRISCO COUNTY: SUMMIT File Name: MADIMAIN Site Code : 00000008 Start Date : 10/15/2024 Page No : 2

| | | | DISON | | | | | MAIN | _ | | | | ISON | | | | | MAIN | _ | | |
|------------------|----------|----------|----------|-------|---------|----------|----------|--------|-----|-------|----------|-----|----------|----------|-------|-------|----------|----------|-----|-------|-------|
| | | Sc | uthbo | und | | | W | estbou | ınd | | | No | rthbou | und | | | E | astbou | ınd | | |
| Start | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Left | Thr | Rig | Ped | App. | Int. |
| Time | Leit | u | ht | s | Total | Len | u | ht | S | Total | Len | u | ht | s | Total | Leit | u | ht | S | Total | Total |
| Peak Hour I | rom 0 | 6:30 A | AM to | 08:15 | AM - Pe | eak 1 c | of 1 | | | | | | | | | | | | | | |
| Intersecti on | 07:30 | AM (| | | | | | | | | | | | | | | | | | | |
| Volume | 10 | 13 | 4 | 1 | 28 | 12 | 81 | 8 | 7 | 108 | 47 | 5 | 11 | 9 | 72 | 5 | 200 | 113 | 9 | 327 | 535 |
| Percent | 35. 7 | 46. 4 | 14. 3 | 3.6 | | 11. 1 | 75. 0 | 7.4 | 6.5 | | 65. 3 | 6.9 | 15. 3 | 12. 5 | | 1.5 | 61. 2 | 34. 6 | 2.8 | | |
| 07:45 | 1 | 3 | 1 | 1 | 6 | 8 | 24 | 3 | 3 | 38 | 15 | 2 | 6 | 1 | 24 | 2 | 55 | 41 | 0 | 98 | 166 |
| Volume | • | Ū | | • | · | | | • | Ū | 00 | | _ | ŭ | • | | _ | • | | ŭ | | |
| Peak | | | | | | | | | | | | | | | | | | | | | 0.806 |
| Factor | | | | | | | | | | | | | | | | | | | | | |
| High Int. | 08:00 |) AM | | | | 07:45 | AM | | | | 07:45 | AM | | | | 07:45 | 5 AM | | | | |
| Volume | 3 | 8 | 1 | 0 | 12 | 8 | 24 | 3 | 3 | 38 | 15 | 2 | 6 | 1 | 24 | 2 | 55 | 41 | 0 | 98 | |
| Peak | | | | | 0.58 | | | | | 0.71 | | | | | 0.75 | | | | | 0.83 | |
| Factor | | | | | 3 | | | | | 1 | | | | | 0 | | | | | 4 | |

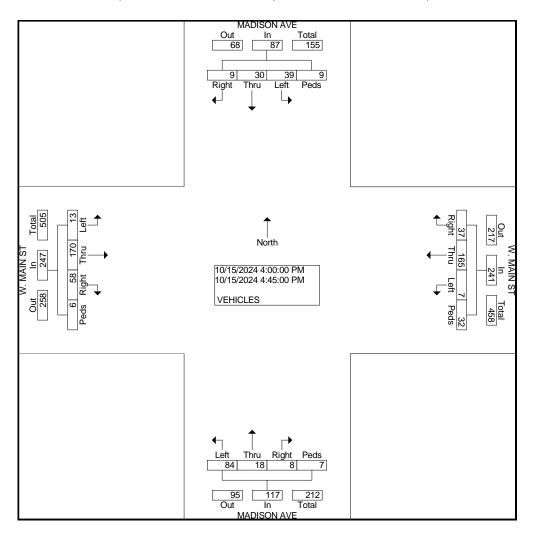


1889 YORK STREET DENVER.COLORADO 303-333-7409

N/S STREET: MADISON AVE E/W STREET: W. MAIN ST

CITY: FRISCO COUNTY: SUMMIT File Name: MADIMAIN Site Code : 00000008 Start Date : 10/15/2024 Page No : 3

| | | | DISON | | | | | MAIN | | | | | DISON | | | | | MAIN | | | |
|------------------|----------|----------|-----------|----------|---------------|---------|----------|-----------|----------|---------------|----------|----------|-----------|-----|---------------|-------|----------|----------|----------|---------------|-------|
| Start | | Thr | | | Ann | | Thr | | Ped | Ann | | Thr | | Ped | Λnn | | Thr | Rig | | Ann | Int. |
| Time | Left | u | Rig ht | reu s | App. Total | Left | u | Rig ht | reu S | App. Total | Left | u | Rig ht | s | App. Total | Left | u | ht | reu S | App. Total | Total |
| Peak Hour I | rom 0 | 4:00 F | | 05:45 | | eak 1 d | | | | | | - | | | | | | | | | |
| Intersecti on | 04:00 | PM | | | | | | | | | | | | | | | | | | | |
| Volume | 39 | 30 | 9 | 9 | 87 | 7 | 165 | 37 | 32 | 241 | 84 | 18 | 8 | 7 | 117 | 13 | 170 | 58 | 6 | 247 | 692 |
| Percent | 44. 8 | 34. 5 | 10. 3 | 10. 3 | | 2.9 | 68. 5 | 15. 4 | 13. 3 | | 71. 8 | 15. 4 | 6.8 | 6.0 | | 5.3 | 68. 8 | 23. 5 | 2.4 | | |
| 04:00 Volume | 8 | 12 | 1 | 1 | 22 | 1 | 43 | 15 | 19 | 78 | 27 | 7 | 4 | 3 | 41 | 6 | 37 | 12 | 0 | 55 | 196 |
| Peak | | | | | | | | | | | | | | | | | | | | | 0.883 |
| Factor | | | | | | | | | | | | | | | | | | | | | |
| High Int. | 04:45 | PM | | | | 04:00 | PM | | | | 04:00 | PM | | | | 04:30 | PM | | | | |
| Volume | 18 | 4 | 1 | 3 | 26 | 1 | 43 | 15 | 19 | 78 | 27 | 7 | 4 | 3 | 41 | 3 | 51 | 15 | 4 | 73 | |
| Peak | | | | | 0.83 | | | | | 0.77 | | | | | 0.71 | | | | | 0.84 | |
| Factor | | | | | 7 | | | | | 2 | | | | | 3 | | | | | 6 | |



PRIME SAUCE

Location: W. MAIN ST W-O MADISON AVE

City: FRISCO
County: SUMMIT
Direction: EAST/WEST



Site Code:2415306 2415306 Site Code:2415306

Start Date: 10152024 10/15/2024 End Date: 10162024 10/16/2024

Longitude: 0.000000 Longitude: 0.000000

| 10/15/2024 | EAST | WEST | |
|------------|-------|-------|-------|
| Time | LACT | WEOT | Total |
| 12:00 AM | * | * | 0 |
| 1:00 | * | * | 0 |
| 2:00 | * | * | 0 |
| 3:00 | * | * | 0 |
| 4:00 | * | * | 0 |
| 5:00 | 23 | 14 | 37 |
| 6:00 | 56 | 59 | 115 |
| 7:00 | 254 | 109 | 363 |
| 8:00 | 258 | 153 | 411 |
| 9:00 | 216 | 146 | 362 |
| 10:00 | 182 | 171 | 353 |
| 11:00 | 229 | 162 | 391 |
| 12:00 PM | 204 | 186 | 390 |
| 1:00 | 191 | 165 | 356 |
| 2:00 | 216 | 186 | 402 |
| 3:00 | 238 | 233 | 471 |
| 4:00 | 237 | 259 | 496 |
| 5:00 | 194 | 230 | 424 |
| 6:00 | 59 | 127 | 186 |
| 7:00 | 34 | 67 | 101 |
| 8:00 | 26 | 44 | 70 |
| 9:00 | 37 | 28 | 65 |
| 10:00 | 30 | 18 | 48 |
| 11:00 | 23 | 16 | 39 |
| Total | 2707 | 2373 | 5080 |
| Percent | 53.3% | 46.7% | |
| AM Peak | 8:00 | 10:00 | 8:00 |
| Volume | 258 | 171 | 411 |
| PM Peak | 3:00 | 4:00 | 4:00 |
| Volume | 238 | 259 | 496 |

PRIME SAUCE

Location: W. MAIN ST W-O MADISON AVE

City: FRISCO County: SUMMIT Direction: EAST/WEST



Site Code:2415306 2415306 Site Code:2415306

Start Date: 10152024 10/15/2024 End Date: 10162024 10/16/2024

Longitude: 0.000000 Longitude: 0.000000

| | | | | Longitude: 0.00000 |
|-------------|-------|------------|-------------|--------------------|
| 10/16/2024 | EAST | WEST | | |
| Time | | | | Total |
| 12:00 AM | 16 | 11 | | 27 |
| 1:00 | 11 | 8 | | 19 |
| 2:00 | 9 | 6 | | 15 |
| 3:00 | 6 | 5 | | 11 |
| 4:00 | 12 | 5 | | 17 |
| 5:00 | 19 | 16 | | 35 |
| 6:00 | 51 | 48 | | 99 |
| 7:00 | 267 | 118 | | 385 |
| 8:00 | 256 | 146 | | 402 |
| 9:00 | 205 | 151 | | 356 |
| 10:00 | 0 | 0 | | 0 |
| 11:00 | 0 | 0 | | 0 |
| 12:00 PM | 0 | 0 | | 0 |
| 1:00 | 0 | 0 | | 0 |
| 2:00 | * | * | | 0 |
| 3:00 | * | * | | 0 |
| 4:00 | * | * | | 0 |
| 5:00 | * | * | | 0 |
| 6:00 | * | * | | 0 |
| 7:00 | * | * | | 0 |
| 8:00 | * | * | | 0 |
| 9:00 | * | * | | 0 |
| 10:00 | * | * | | 0 |
| 11:00 | * | * | | 0 |
| Total | 852 | 514 | | 1366 |
| Percent | 62.4% | 37.6% | | 1000 |
| AM Peak | 7:00 | 9:00 | | 8:00 |
| Volume | 267 | 151 | | 402 |
| PM Peak | | | | 102 |
| Volume | | | | |
| Grand Total | 3559 | 2887 | | 6446 |
| Percent | 55.2% | 44.8% | | |
| ADT | | ADT: 5,164 | AADT: 5,164 | |
| , , , , , | | 5 0, . 0 . | | |

LEVEL OF SERVICE DEFINITIONS

From Highway Capacity Manual, Transportation Research Board

UNSIGNALIZED INTERSECTION LEVEL OF SERVICE (LOS)
Applicable to Two-Way Stop Control, All-Way Stop Control, and Roundabouts

| LOS | Average Vehicle Control Delay | Operational Characteristics |
|-----|-------------------------------------|--|
| A | <10 seconds | Normally, vehicles on the stop-controlled approach only have to wait up to 10 seconds before being able to clear the intersection. Left-turning vehicles on the uncontrolled street do not have to wait to make their turn. |
| В | 10 to 15 seconds | Vehicles on the stop-controlled approach will experience delays before being able to clear the intersection. The delay could be up to 15 seconds. Left-turning vehicles on the uncontrolled street may have to wait to make their turn. |
| С | 15 to 25 seconds | Vehicles on the stop-controlled approach can expect delays in the range of 15 to 25 seconds before clearing the intersection. Motorists may begin to take chances due to the long delays, thereby posing a safety risk to through traffic. Left-turning vehicles on the uncontrolled street will now be required to wait to make their turn causing a queue to be created in the turn lane. |
| D | 25 to 35 seconds | This is the point at which a traffic signal may be warranted for this intersection. The delays for the stop-controlled intersection are not considered to be excessive. The length of the queue may begin to block other public and private access points. |
| E | 35 to 50 seconds | The delays for all critical traffic movements are considered to be unacceptable. The length of the queues for the stop-controlled approaches as well as the left-turn movements are extremely long. There is a high probability that this intersection will meet traffic signal warrants. The ability to install a traffic signal is affected by the location of other existing traffic signals. Consideration may be given to restricting the accesses by eliminating the left-turn movements from and to the stop-controlled approach. |
| F | >50 seconds | The delay for the critical traffic movements are probably in excess of 100 seconds. The length of the queues are extremely long. Motorists are selecting alternative routes due to the long delays. The only remedy for these long delays is installing a traffic signal or restricting the accesses. The potential for accidents at this intersection are extremely high due to motorist taking more risky chances. If the median permits, motorists begin making two-stage left-turns. |

| Intersection | | |
|---------------------------|------|--|
| Intersection Delay, s/veh | 11.6 | |
| Intersection LOS | В | |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 7 | 270 | 151 | 16 | 110 | 11 | 65 | 7 | 15 | 13 | 17 | 5 |
| Future Vol, veh/h | 7 | 270 | 151 | 16 | 110 | 11 | 65 | 7 | 15 | 13 | 17 | 5 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 8 | 307 | 172 | 18 | 125 | 13 | 74 | 8 | 17 | 15 | 19 | 6 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 13.1 | | | 9.1 | | | 9.5 | | | 8.9 | | |
| HCM LOS | В | | | Α | | | Α | | | Α | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 75% | 2% | 12% | 37% | |
| Vol Thru, % | 8% | 63% | 80% | 49% | |
| Vol Right, % | 17% | 35% | 8% | 14% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 87 | 428 | 137 | 35 | |
| LT Vol | 65 | 7 | 16 | 13 | |
| Through Vol | 7 | 270 | 110 | 17 | |
| RT Vol | 15 | 151 | 11 | 5 | |
| Lane Flow Rate | 99 | 486 | 156 | 40 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.149 | 0.579 | 0.208 | 0.061 | |
| Departure Headway (Hd) | 5.443 | 4.285 | 4.799 | 5.491 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Cap | 653 | 841 | 744 | 646 | |
| Service Time | 3.519 | 2.324 | 2.855 | 3.575 | |
| HCM Lane V/C Ratio | 0.152 | 0.578 | 0.21 | 0.062 | |
| HCM Control Delay | 9.5 | 13.1 | 9.1 | 8.9 | |
| HCM Lane LOS | Α | В | Α | Α | |
| HCM 95th-tile Q | 0.5 | 3.8 | 8.0 | 0.2 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|--------|--------|-------|--------|------|------|--------|------|------|
| Int Delay, s/veh | 2.8 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 32 | 1 | 21 | 4 | 1 | 1 | 31 | 54 | 3 | 1 | 135 | 48 |
| Future Vol, veh/h | 32 | 1 | 21 | 4 | 1 | 1 | 31 | 54 | 3 | 1 | 135 | 48 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | e, # - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 43 | 1 | 28 | 5 | 1 | 1 | 41 | 72 | 4 | 1 | 180 | 64 |
| | | | | | | | | | | | | |
| Major/Minor I | Minor2 | | | Minor1 | | ı | Major1 | | ١ | Major2 | | |
| Conflicting Flow All | 371 | 372 | 212 | 385 | 402 | 74 | 244 | 0 | 0 | 76 | 0 | 0 |
| Stage 1 | 214 | 214 | - | 156 | 156 | - | - | - | - | - | - | - |
| Stage 2 | 157 | 158 | - | 229 | 246 | - | - | - | - | _ | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | _ | - | - | - | | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 586 | 558 | 828 | 573 | 537 | 988 | 1322 | - | - | 1523 | - | - |
| Stage 1 | 788 | 725 | - | 846 | 769 | - | - | - | - | - | - | - |
| Stage 2 | 845 | 767 | - | 774 | 703 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | - | - |
| Mov Cap-1 Maneuver | 570 | 540 | 828 | 539 | 519 | 988 | 1322 | - | - | 1523 | - | - |
| Mov Cap-2 Maneuver | 570 | 540 | - | 539 | 519 | - | - | - | - | - | - | - |
| Stage 1 | 763 | 724 | - | 819 | 744 | - | - | - | - | - | - | - |
| Stage 2 | 815 | 742 | - | 746 | 702 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 11.2 | | | 11.3 | | | 2.8 | | | 0 | | |
| HCM LOS | В | | | В | | | | | | | | |
| 20 | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NBR | EBLn1\ | WBLn1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1322 | - | - | | 579 | 1523 | - | _ | | | |
| HCM Lane V/C Ratio | | 0.031 | - | _ | 0.111 | | | _ | _ | | | |
| HCM Control Delay (s) | | 7.8 | 0 | _ | 11.2 | 11.3 | 7.4 | 0 | _ | | | |
| HCM Lane LOS | | Α | A | _ | В | В | Α | A | - | | | |
| HCM 95th %tile Q(veh) |) | 0.1 | - | - | 0.4 | 0 | 0 | - | - | | | |
| 22 / 2011 | | | | | | | - | | | | | |

| Intersection | | | | | | |
|------------------------|--------------|----------|----------|----------|----------|------|
| Int Delay, s/veh | 0.1 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | ¥ | | \$ | | | ન |
| Traffic Vol, veh/h | 1 | 2 | 86 | 1 | 1 | 159 |
| Future Vol, veh/h | 1 | 2 | 86 | 1 | 1 | 159 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | _ | | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | | - | 0 | - | _ | 0 |
| Grade, % | 0 | _ | 0 | _ | _ | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mymt Flow | 1 | 3 | 115 | 1 | 1 | 212 |
| WWW. C. LOW | | - 3 | 110 | 1 | | 212 |
| | | | | | | |
| | Minor1 | | Major1 | | Major2 | |
| Conflicting Flow All | 330 | 116 | 0 | 0 | 116 | 0 |
| Stage 1 | 116 | - | - | - | - | - |
| Stage 2 | 214 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 3.318 | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 665 | 936 | - | - | 1473 | - |
| Stage 1 | 909 | - | - | - | - | - |
| Stage 2 | 822 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 664 | 936 | - | - | 1473 | _ |
| Mov Cap-2 Maneuver | 664 | - | _ | _ | - | _ |
| Stage 1 | 909 | - | _ | _ | _ | _ |
| Stage 2 | 821 | <u>-</u> | <u>-</u> | <u>-</u> | <u>-</u> | _ |
| Olugo Z | 0 <u>Z</u> 1 | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.4 | | 0 | | 0 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | ıt | NBT | NRRV | WBLn1 | SBL | SBT |
| Capacity (veh/h) | L | INDI | - | | 1473 | - |
| HCM Lane V/C Ratio | | - | | 0.005 | | |
| HCM Control Delay (s) | | - | - | 9.4 | 7.4 | 0 |
| HCM Lane LOS | | - | - | 9.4 A | 7.4 A | A |
| THE TUST | | - | - | | | А |
| HCM 95th %tile Q(veh) | | | _ | 0 | 0 | _ |

| Intersection | |
|---------------------------|------|
| Intersection Delay, s/veh | 12.8 |
| Intersection LOS | В |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 17 | 230 | 78 | 9 | 220 | 50 | 115 | 24 | 11 | 52 | 40 | 12 |
| Future Vol, veh/h | 17 | 230 | 78 | 9 | 220 | 50 | 115 | 24 | 11 | 52 | 40 | 12 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 19 | 261 | 89 | 10 | 250 | 57 | 131 | 27 | 13 | 59 | 45 | 14 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 14 | | | 12.9 | | | 11.6 | | | 10.7 | | |
| HCM LOS | В | | | В | | | В | | | В | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 77% | 5% | 3% | 50% | |
| Vol Thru, % | 16% | 71% | 79% | 38% | |
| Vol Right, % | 7% | 24% | 18% | 12% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 150 | 325 | 279 | 104 | |
| LT Vol | 115 | 17 | 9 | 52 | |
| Through Vol | 24 | 230 | 220 | 40 | |
| RT Vol | 11 | 78 | 50 | 12 | |
| Lane Flow Rate | 170 | 369 | 317 | 118 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.287 | 0.531 | 0.465 | 0.201 | |
| Departure Headway (Hd) | 6.069 | 5.18 | 5.283 | 6.113 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Cap | 590 | 694 | 678 | 584 | |
| Service Time | 4.13 | 3.228 | 3.333 | 4.179 | |
| HCM Lane V/C Ratio | 0.288 | 0.532 | 0.468 | 0.202 | |
| HCM Control Delay | 11.6 | 14 | 12.9 | 10.7 | |
| HCM Lane LOS | В | В | В | В | |
| HCM 95th-tile Q | 1.2 | 3.2 | 2.5 | 0.7 | |

| Intersection | | | | | | | | | | | | |
|--|------------|-----------|----------|-----------|-----------|--------|--------|------|--------|--------|-----------------|-------|
| Int Delay, s/veh | 1 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| | EDL | | EDK | WDL | | WDK | INDL | | NDK | ODL | | SDK |
| Lane Configurations Traffic Vol, veh/h | 15 | ♣ | 4 | 1 | ♣ | 2 | 2 | 133 | 2 | 1 | ♣ 115 | 11 |
| Future Vol, veh/h | 15 | 1 | 4 | 1 | 1 | 2 | 2 | 133 | 2 | 1 | 115 | 11 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | Stop - | Olop - | None | Stop - | Slop - | None | - | - | None | - | - | None |
| Storage Length | <u>-</u> | | TNOTIC | _ | _ | TNOTIC | _ | _ | INOIIC | _ | _ | NONE. |
| Veh in Median Storage | | 0 | _ | _ | 0 | _ | _ | 0 | _ | _ | 0 | _ |
| Grade, % | -, 11 - | 0 | _ | _ | 0 | _ | _ | 0 | _ | _ | 0 | _ |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 20 | 1 | 5 | 1 | 1 | 3 | 3 | 177 | 3 | 1 | 153 | 15 |
| | | | | | | | | | | | | |
| Major/Minor I | Minor2 | | | Minor1 | | | Major1 | | | Major2 | | |
| Conflicting Flow All | 350 | 349 | 161 | 351 | 355 | 179 | 168 | 0 | 0 | 180 | 0 | 0 |
| Stage 1 | 163 | 163 | 101 | 185 | 185 | 1/9 | 100 | - | - | 100 | - | U |
| Stage 2 | 187 | 186 | - | 166 | 170 | - | _ | | _ | _ | | |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | _ | | 4.12 | | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | 0.22 | 6.12 | 5.52 | 0.22 | 7.12 | _ | | 7.12 | _ | _ |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | _ | 6.12 | 5.52 | | _ | _ | | _ | _ | |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | _ | _ | 2.218 | _ | _ |
| Pot Cap-1 Maneuver | 605 | 575 | 884 | 604 | 571 | 864 | 1410 | - | - | 1396 | - | - |
| Stage 1 | 839 | 763 | - | 817 | 747 | - | - | _ | _ | - | _ | _ |
| Stage 2 | 815 | 746 | - | 836 | 758 | - | - | - | _ | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | - | - |
| Mov Cap-1 Maneuver | 601 | 573 | 884 | 598 | 569 | 864 | 1410 | - | - | 1396 | - | - |
| Mov Cap-2 Maneuver | 601 | 573 | - | 598 | 569 | - | - | - | - | - | - | - |
| Stage 1 | 837 | 762 | - | 815 | 746 | - | - | - | - | - | - | - |
| Stage 2 | 809 | 745 | - | 829 | 757 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 10.9 | | | 10.2 | | | 0.1 | | | 0.1 | | |
| HCM LOS | В | | | В | | | | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NRR | EBLn1\ | WBI n1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1410 | - 1101 | - | | 696 | 1396 | | CDIC | | | |
| HCM Lane V/C Ratio | | 0.002 | <u> </u> | | | 0.008 | | _ | | | | |
| HCM Control Delay (s) | | 7.6 | 0 | | 10.9 | 10.2 | 7.6 | 0 | | | | |
| HCM Lane LOS | | Α. | A | _ | В | В | Α. | A | _ | | | |
| HCM 95th %tile Q(veh) |) | 0 | - | _ | 0.1 | 0 | 0 | - | _ | | | |
| | | | | | 0.1 | J | | | | | | |

| Intersection | | | | | | |
|------------------------|--------|-------|--------|---------------|---------------|------|
| Int Delay, s/veh | 0.2 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | ¥ | | 7> | | | ન |
| Traffic Vol, veh/h | 2 | 2 | 135 | 1 | 3 | 117 |
| Future Vol, veh/h | 2 | 2 | 135 | 1 | 3 | 117 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | _ | | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | | - | 0 | _ | _ | 0 |
| Grade, % | 0 | _ | 0 | _ | _ | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mymt Flow | 3 | 3 | 180 | 1 | 4 | 156 |
| WWW.CT IOW | | | 100 | • | • | 100 |
| | | | | | | |
| | Minor1 | | Major1 | | Major2 | |
| Conflicting Flow All | 345 | 181 | 0 | 0 | 181 | 0 |
| Stage 1 | 181 | - | - | - | - | - |
| Stage 2 | 164 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 3.318 | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 652 | 862 | - | - | 1394 | - |
| Stage 1 | 850 | - | - | - | - | - |
| Stage 2 | 865 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 650 | 862 | - | - | 1394 | - |
| Mov Cap-2 Maneuver | 650 | - | - | _ | - | - |
| Stage 1 | 850 | - | - | _ | - | - |
| Stage 2 | 862 | _ | _ | _ | _ | _ |
| Jugo 2 | 302 | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.9 | | 0 | | 0.2 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Lanc/Major Mum | + | NBT | NDDV | VBLn1 | SBL | SBT |
| Minor Lane/Major Mvm | L | INDI | | | | |
| Capacity (veh/h) | | - | - | | 1394 | - |
| | | - | - | 0.007 | | - |
| HCM Control Doloy (a) | | | | 0.0 | | |
| HCM Control Delay (s) | | - | - | 9.9 | 7.6 | 0 |
| | | - | - - | 9.9 A 0 | 7.6 A 0 | A - |

| Intersection | | | |
|---------------------------|------|--|--|
| Intersection Delay, s/veh | 11.9 | | |
| Intersection LOS | В | | |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 7 | 275 | 154 | 16 | 112 | 11 | 66 | 7 | 15 | 13 | 17 | 5 |
| Future Vol, veh/h | 7 | 275 | 154 | 16 | 112 | 11 | 66 | 7 | 15 | 13 | 17 | 5 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 8 | 313 | 175 | 18 | 127 | 13 | 75 | 8 | 17 | 15 | 19 | 6 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 13.4 | | | 9.2 | | | 9.5 | | | 9 | | |
| HCM LOS | В | | | Α | | | Α | | | Α | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 75% | 2% | 12% | 37% | |
| Vol Thru, % | 8% | 63% | 81% | 49% | |
| Vol Right, % | 17% | 35% | 8% | 14% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 88 | 436 | 139 | 35 | |
| LT Vol | 66 | 7 | 16 | 13 | |
| Through Vol | 7 | 275 | 112 | 17 | |
| RT Vol | 15 | 154 | 11 | 5 | |
| Lane Flow Rate | 100 | 495 | 158 | 40 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.152 | 0.591 | 0.211 | 0.061 | |
| Departure Headway (Hd) | 5.472 | 4.293 | 4.816 | 5.52 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Cap | 650 | 837 | 741 | 643 | |
| Service Time | 3.548 | 2.332 | 2.873 | 3.607 | |
| HCM Lane V/C Ratio | 0.154 | 0.591 | 0.213 | 0.062 | |
| HCM Control Delay | 9.5 | 13.4 | 9.2 | 9 | |
| HCM Lane LOS | А | В | Α | Α | |
| HCM 95th-tile Q | 0.5 | 4 | 0.8 | 0.2 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|--------|--------|-------|--------|------|------|--------|------|------|
| Int Delay, s/veh | 2.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 55 | 0 | 0 | 139 | 48 |
| Future Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 55 | 0 | 0 | 139 | 48 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | ,# - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 44 | 0 | 28 | 0 | 0 | 0 | 43 | 73 | 0 | 0 | 185 | 64 |
| | | | | | | | | | | | | |
| Major/Minor N | Minor2 | | | Minor1 | | | Major1 | | | Major2 | | |
| Conflicting Flow All | 376 | 376 | 217 | 390 | 408 | 73 | 249 | 0 | 0 | 73 | 0 | 0 |
| Stage 1 | 217 | 217 | - | 159 | 159 | - | - | - | - | - | - | - |
| Stage 2 | 159 | 159 | - | 231 | 249 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 581 | 555 | 823 | 569 | 533 | 989 | 1317 | - | - | 1527 | - | - |
| Stage 1 | 785 | 723 | - | 843 | 766 | - | - | - | - | - | - | - |
| Stage 2 | 843 | 766 | - | 772 | 701 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | - | - |
| Mov Cap-1 Maneuver | 566 | 536 | 823 | 535 | 515 | 989 | 1317 | - | - | 1527 | - | - |
| Mov Cap-2 Maneuver | 566 | 536 | - | 535 | 515 | - | - | - | - | - | - | - |
| Stage 1 | 758 | 723 | - | 814 | 740 | - | - | - | - | - | - | - |
| Stage 2 | 814 | 740 | - | 746 | 701 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 11.3 | | | 0 | | | 2.9 | | | 0 | | |
| HCM LOS | В | | | Α | | | | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NBR | EBLn1V | VBLn1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1317 | - | - | | - | 1527 | - | - | | | |
| HCM Lane V/C Ratio | | 0.032 | - | - | 0.112 | - | - | - | _ | | | |
| HCM Control Delay (s) | | 7.8 | 0 | - | | 0 | 0 | - | - | | | |
| HCM Lane LOS | | Α | A | - | В | A | A | - | - | | | |
| HCM 95th %tile Q(veh) | | 0.1 | - | - | 0.4 | - | 0 | - | - | | | |
| | | | | | | | | | | | | |

| Intersection | | | | | | |
|------------------------|--------|-------|--------|-------|--------|----------------|
| Int Delay, s/veh | 0.1 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | ¥* | וטיי | 1\D1 | TIDIT | ODL | - 6 |
| Traffic Vol, veh/h | 1 | 2 | 85 | 1 | 1 | 159 |
| Future Vol, veh/h | 1 | 2 | 85 | 1 | 1 | 159 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | | - | None |
| Storage Length | 0 | - | _ | - | _ | - |
| Veh in Median Storage | | _ | 0 | _ | _ | 0 |
| Grade, % | 0 | _ | 0 | _ | _ | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mymt Flow | 1 | 3 | 113 | 1 | 1 | 212 |
| IVIVIIIL I IOW | Į. | J | 110 | ļ | Į. | 212 |
| | | | | | | |
| Major/Minor | Minor1 | N | Major1 | ا | Major2 | |
| Conflicting Flow All | 328 | 114 | 0 | 0 | 114 | 0 |
| Stage 1 | 114 | - | - | - | - | - |
| Stage 2 | 214 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | _ | _ | - |
| Follow-up Hdwy | 3.518 | 3.318 | - | _ | 2.218 | - |
| Pot Cap-1 Maneuver | 666 | 939 | - | - | 1475 | - |
| Stage 1 | 911 | - | - | - | _ | - |
| Stage 2 | 822 | - | - | _ | _ | - |
| Platoon blocked, % | | | - | _ | | _ |
| Mov Cap-1 Maneuver | 665 | 939 | _ | _ | 1475 | _ |
| Mov Cap-2 Maneuver | 665 | - | _ | _ | - | _ |
| Stage 1 | 911 | _ | _ | _ | _ | _ |
| Stage 2 | 821 | _ | _ | _ | _ | |
| Olaye Z | 021 | _ | _ | _ | _ | <u>-</u> |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.4 | | 0 | | 0 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minar Lana/Maiar Muse | -4 | NDT | NDDV | MDI 1 | CDI | CDT |
| Minor Lane/Major Mvm | IL | NBT | | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - | - | 826 | 1475 | - |
| HCM Lane V/C Ratio | | - | | 0.005 | | - |
| HCM Control Delay (s) | | - | - | 9.4 | 7.4 | 0 |
| HCM Lane LOS | | - | - | A | A | Α |
| HCM 95th %tile Q(veh) |) | - | - | 0 | 0 | - |

| Intersection | | |
|---------------------------|------|--|
| Intersection Delay, s/veh | 13.1 | |
| Intersection LOS | В | |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 17 | 235 | 80 | 9 | 225 | 51 | 117 | 24 | 11 | 53 | 41 | 12 |
| Future Vol, veh/h | 17 | 235 | 80 | 9 | 225 | 51 | 117 | 24 | 11 | 53 | 41 | 12 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 19 | 267 | 91 | 10 | 256 | 58 | 133 | 27 | 13 | 60 | 47 | 14 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 14.4 | | | 13.2 | | | 11.8 | | | 10.9 | | |
| HCM LOS | В | | | В | | | В | | | В | | |

| Vol Left, % 77% 5% 3% 50% Vol Thru, % 16% 71% 79% 39% Vol Right, % 7% 24% 18% 11% Sign Control Stop Stop Stop Traffic Vol by Lane 152 332 285 106 LT Vol 117 17 9 53 Through Vol 24 235 225 41 RT Vol 11 80 51 12 Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.2 | Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|--|------------------------|-------|-------|-------|-------|--|
| Vol Right, % 7% 24% 18% 11% Sign Control Stop Stop Stop Traffic Vol by Lane 152 332 285 106 LT Vol 117 17 9 53 Through Vol 24 235 225 41 RT Vol 11 80 51 12 Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS | Vol Left, % | 77% | 5% | 3% | 50% | |
| Sign Control Stop Stop Stop Stop Traffic Vol by Lane 152 332 285 106 LT Vol 117 17 9 53 Through Vol 24 235 225 41 RT Vol 11 80 51 12 Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Vol Thru, % | 16% | 71% | 79% | 39% | |
| Traffic Vol by Lane 152 332 285 106 LT Vol 117 17 9 53 Through Vol 24 235 225 41 RT Vol 11 80 51 12 Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Vol Right, % | 7% | 24% | 18% | 11% | |
| LT Vol 117 17 9 53 Through Vol 24 235 225 41 RT Vol 11 80 51 12 Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Sign Control | Stop | Stop | Stop | Stop | |
| Through Vol 24 235 225 41 RT Vol 11 80 51 12 Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Traffic Vol by Lane | 152 | 332 | 285 | 106 | |
| RT Vol 11 80 51 12 Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | LT Vol | 117 | 17 | 9 | 53 | |
| Lane Flow Rate 173 377 324 120 Geometry Grp 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Through Vol | 24 | 235 | 225 | 41 | |
| Geometry Grp 1 1 1 1 1 Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | RT Vol | 11 | 80 | 51 | 12 | |
| Degree of Util (X) 0.294 0.547 0.479 0.207 Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Lane Flow Rate | 173 | 377 | 324 | 120 | |
| Departure Headway (Hd) 6.127 5.215 5.322 6.174 Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Geometry Grp | 1 | 1 | 1 | 1 | |
| Convergence, Y/N Yes Yes Yes Yes Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Degree of Util (X) | 0.294 | 0.547 | 0.479 | 0.207 | |
| Cap 584 690 674 578 Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Departure Headway (Hd) | 6.127 | 5.215 | 5.322 | 6.174 | |
| Service Time 4.195 3.27 3.379 4.248 HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Convergence, Y/N | Yes | Yes | Yes | Yes | |
| HCM Lane V/C Ratio 0.296 0.546 0.481 0.208 HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B B | Сар | 584 | 690 | 674 | 578 | |
| HCM Control Delay 11.8 14.4 13.2 10.9 HCM Lane LOS B B B | Service Time | 4.195 | 3.27 | 3.379 | 4.248 | |
| HCM Lane LOS B B B | HCM Lane V/C Ratio | 0.296 | 0.546 | 0.481 | 0.208 | |
| | | 11.8 | 14.4 | 13.2 | 10.9 | |
| HCM 95th-tile Q 1.2 3.3 2.6 0.8 | HCM Lane LOS | В | В | В | В | |
| | HCM 95th-tile Q | 1.2 | 3.3 | 2.6 | 8.0 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|--------|--------|-------|--------|------|------|--------|------|------|
| Int Delay, s/veh | 0.9 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 16 | 0 | 4 | 0 | 0 | 0 | 3 | 136 | 0 | 0 | 119 | 11 |
| Future Vol, veh/h | 16 | 0 | 4 | 0 | 0 | 0 | 3 | 136 | 0 | 0 | 119 | 11 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | , # - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 21 | 0 | 5 | 0 | 0 | 0 | 4 | 181 | 0 | 0 | 159 | 15 |
| | | | | | | | | | | | | |
| Major/Minor I | Minor2 | | | Minor1 | | | Major1 | | I | Major2 | | |
| Conflicting Flow All | 356 | 356 | 167 | 358 | 363 | 181 | 174 | 0 | 0 | 181 | 0 | 0 |
| Stage 1 | 167 | 167 | - | 189 | 189 | - | - | - | - | - | - | - |
| Stage 2 | 189 | 189 | - | 169 | 174 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | _ | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 599 | 570 | 877 | 597 | 565 | 862 | 1403 | - | - | 1394 | - | - |
| Stage 1 | 835 | 760 | - | 813 | 744 | - | - | - | - | - | - | - |
| Stage 2 | 813 | 744 | - | 833 | 755 | - | _ | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | - | - |
| Mov Cap-1 Maneuver | 598 | 568 | 877 | 592 | 563 | 862 | 1403 | - | - | 1394 | - | - |
| Mov Cap-2 Maneuver | 598 | 568 | - | 592 | 563 | - | - | - | - | - | - | - |
| Stage 1 | 832 | 760 | - | 811 | 742 | - | - | - | - | - | - | - |
| Stage 2 | 811 | 742 | - | 828 | 755 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 10.9 | | | 0 | | | 0.2 | | | 0 | | |
| HCM LOS | В | | | Α | | | | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NBR | EBLn1V | VBLn1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1403 | - | - | 639 | - | 1394 | - | _ | | | |
| HCM Lane V/C Ratio | | 0.003 | - | - | 0.042 | - | - | _ | - | | | |
| HCM Control Delay (s) | | 7.6 | 0 | - | 10.9 | 0 | 0 | - | - | | | |
| HCM Lane LOS | | Α | A | - | В | A | A | - | - | | | |
| HCM 95th %tile Q(veh) | | 0 | - | - | 0.1 | - | 0 | - | - | | | |
| | | | | | | | | | | | | |

| Intersection | | | | | | |
|------------------------|--------|-------|--------|---------------|---------------|-------------|
| Int Delay, s/veh | 0.2 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | 7 | וטוי | 1> | HOIL | ODL | ्री |
| Traffic Vol, veh/h | 2 | 2 | 137 | 1 | 3 | 120 |
| Future Vol, veh/h | 2 | 2 | 137 | 1 | 3 | 120 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | | _ | 0 | _ | _ | 0 |
| Grade, % | 0 | _ | 0 | _ | _ | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mymt Flow | 3 | 3 | 183 | 1 | 4 | 160 |
| WWITETIOW | U | U | 100 | ļ. | 7 | 100 |
| | | | | | | |
| | Minor1 | | Major1 | | Major2 | |
| Conflicting Flow All | 352 | 184 | 0 | 0 | 184 | 0 |
| Stage 1 | 184 | - | - | - | - | - |
| Stage 2 | 168 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 3.318 | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 646 | 858 | - | - | 1391 | - |
| Stage 1 | 848 | - | - | - | - | - |
| Stage 2 | 862 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 644 | 858 | _ | - | 1391 | - |
| Mov Cap-2 Maneuver | 644 | - | _ | _ | - | _ |
| Stage 1 | 848 | _ | _ | _ | _ | _ |
| Stage 2 | 859 | _ | _ | _ | _ | _ |
| Olago Z | 000 | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.9 | | 0 | | 0.2 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | nt | NBT | NRRV | VBLn1 | SBL | SBT |
| | ıı | INDI | - | 736 | 1391 | |
| Capacity (veh/h) | | - | | 0.007 | | - |
| HCM Lane V//C Datio | | - | | | | |
| HCM Control Delay (s) | | | | 0.0 | 16 | (1 |
| HCM Control Delay (s) | | - | - | 9.9 | 7.6 | 0 |
| | | - | - - | 9.9 A 0 | 7.6 A 0 | 0 A - |

| Intersection | | | |
|---------------------------|------|--|--|
| Intersection Delay, s/veh | 12.2 | | |
| Intersection LOS | В | | |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 7 | 275 | 159 | 27 | 112 | 11 | 71 | 8 | 25 | 13 | 18 | 5 |
| Future Vol, veh/h | 7 | 275 | 159 | 27 | 112 | 11 | 71 | 8 | 25 | 13 | 18 | 5 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 8 | 313 | 181 | 31 | 127 | 13 | 81 | 9 | 28 | 15 | 20 | 6 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 14 | | | 9.5 | | | 9.8 | | | 9.1 | | |
| HCM LOS | В | | | Α | | | Α | | | Α | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 68% | 2% | 18% | 36% | |
| Vol Thru, % | 8% | 62% | 75% | 50% | |
| Vol Right, % | 24% | 36% | 7% | 14% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 104 | 441 | 150 | 36 | |
| LT Vol | 71 | 7 | 27 | 13 | |
| Through Vol | 8 | 275 | 112 | 18 | |
| RT Vol | 25 | 159 | 11 | 5 | |
| Lane Flow Rate | 118 | 501 | 170 | 41 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.18 | 0.607 | 0.232 | 0.065 | |
| Departure Headway (Hd) | 5.471 | 4.361 | 4.903 | 5.711 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Cap | 649 | 822 | 726 | 631 | |
| Service Time | 3.561 | 2.412 | 2.975 | 3.711 | |
| HCM Lane V/C Ratio | 0.182 | 0.609 | 0.234 | 0.065 | |
| HCM Control Delay | 9.8 | 14 | 9.5 | 9.1 | |
| HCM Lane LOS | Α | В | Α | Α | |
| HCM 95th-tile Q | 0.7 | 4.2 | 0.9 | 0.2 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|--------|--------|-------|--------|------|-------|--------|------|------|
| Int Delay, s/veh | 2.4 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | LDIN | 1100 | 4 | 7,51 | ,,,,,, | 4 | 11511 | UDL | 4 | ODIT |
| Traffic Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 71 | 0 | 0 | 156 | 48 |
| Future Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 71 | 0 | 0 | 156 | 48 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | _ | _ | None | - | - | None | - | _ | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | e,# - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 44 | 0 | 28 | 0 | 0 | 0 | 43 | 95 | 0 | 0 | 208 | 64 |
| | | | | | | | | | | | | |
| Major/Minor I | Minor2 | | | Minor1 | | | Major1 | | ı | Major2 | | |
| Conflicting Flow All | 421 | 421 | 240 | 435 | 453 | 95 | 272 | 0 | 0 | 95 | 0 | 0 |
| Stage 1 | 240 | 240 | - | 181 | 181 | - | - | - | - | - | - | - |
| Stage 2 | 181 | 181 | - | 254 | 272 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 543 | 524 | 799 | 531 | 503 | 962 | 1291 | - | - | 1499 | - | - |
| Stage 1 | 763 | 707 | - | 821 | 750 | - | - | - | - | - | - | - |
| Stage 2 | 821 | 750 | - | 750 | 685 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | - | - |
| Mov Cap-1 Maneuver | 528 | 506 | 799 | 499 | 485 | 962 | 1291 | - | - | 1499 | - | - |
| Mov Cap-2 Maneuver | 528 | 506 | - | 499 | 485 | - | - | - | - | - | - | - |
| Stage 1 | 736 | 707 | - | 792 | 724 | - | - | - | - | - | - | - |
| Stage 2 | 792 | 724 | - | 724 | 685 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 11.7 | | | 0 | | | 2.4 | | | 0 | | |
| HCM LOS | В | | | Α | | | | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NBR | EBLn1V | WBLn1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1291 | - | - | 608 | - | 1499 | - | | | | |
| HCM Lane V/C Ratio | | 0.033 | - | _ | 0.118 | - | - | - | - | | | |
| HCM Control Delay (s) | | 7.9 | 0 | - | | 0 | 0 | - | - | | | |
| HCM Lane LOS | | Α | Α | - | В | Α | Α | - | - | | | |
| HCM 95th %tile Q(veh) |) | 0.1 | - | - | 0.4 | - | 0 | - | - | | | |
| - | | | | | | | | | | | | |

| Intersection | | | | | | |
|--|---------|------|---------------|---------|--------|--------------|
| Int Delay, s/veh | 1 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| | WDL | אטוז | | NON | ODL | <u>अज्ञा</u> |
| Lane Configurations Traffic Vol, veh/h | Ϋ́ 1 | 16 | 1 → 87 | 1 | 17 | 160 |
| | 1 | 16 | 87 | 1 | | 160 |
| Future Vol, veh/h | 0 | 0 | 0 | 0 | 17 | 0 |
| Conflicting Peds, #/hr | | | | | | |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | | - | 0 | - | - | 0 |
| Grade, % | 0 | - | 0 | - | - | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 1 | 21 | 116 | 1 | 23 | 213 |
| | | | | | | |
| Major/Minor | Minor1 | N | Major1 | ı | Major2 | |
| | 376 | 117 | 0 | 0 | 117 | 0 |
| Conflicting Flow All Stage 1 | 117 | | | U | 117 | |
| | | - | - | - | | - |
| Stage 2 | 259 | - | - | - | - 4.40 | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 625 | 935 | - | - | 1471 | - |
| Stage 1 | 908 | - | - | - | - | - |
| Stage 2 | 784 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 614 | 935 | - | - | 1471 | - |
| Mov Cap-2 Maneuver | 614 | - | - | - | - | - |
| Stage 1 | 908 | - | - | - | _ | - |
| Stage 2 | 770 | - | _ | _ | _ | _ |
| | | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.1 | | 0 | | 0.7 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Lanc/Major Muss | nt | NBT | NDDV | VBLn1 | SBL | SBT |
| Minor Lane/Major Mvn | IL | | | | | |
| Capacity (veh/h) | | - | - | • • • • | 1471 | - |
| HCM Lane V/C Ratio | | - | | 0.025 | | - |
| HCM Control Delay (s) | | - | - | ~ | 7.5 | 0 |
| HCM Lane LOS | | - | - | A | A | Α |
| HCM 95th %tile Q(veh |) | - | - | 0.1 | 0 | - |
| | | | | | | |

| Intersection | | | | | | |
|------------------------|--------|----------|--------|----------|--------|-------------|
| Int Delay, s/veh | 0.1 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | WDL. | וטייי | 1\D1 | NOI | ODL | ्र <u>ी</u> |
| | | 2 | | 1 | 1 | |
| Traffic Vol, veh/h | 1 | 2 | 86 | 1 | 1 | 160 |
| Future Vol, veh/h | 1 | 2 | 86 | 1 | 1 | 160 |
| Conflicting Peds, #/hr | 0 | 0 | _ 0 | _ 0 | _ 0 | _ 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | e, # 0 | - | 0 | - | - | 0 |
| Grade, % | 0 | - | 0 | - | - | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mymt Flow | 1 | 3 | 115 | 1 | 1 | 213 |
| IVIVIII I IOW | | 0 | 110 | | | 210 |
| | | | | | | |
| Major/Minor | Minor1 | <u> </u> | Major1 | <u> </u> | Major2 | |
| Conflicting Flow All | 331 | 116 | 0 | 0 | 116 | 0 |
| Stage 1 | 116 | - | - | - | _ | - |
| Stage 2 | 215 | _ | _ | _ | _ | _ |
| Critical Hdwy | 6.42 | 6.22 | _ | _ | 4.12 | _ |
| Critical Hdwy Stg 1 | 5.42 | - | _ | <u>_</u> | 1.12 | _ |
| Critical Hdwy Stg 2 | 5.42 | _ | | _ | _ | |
| Follow-up Hdwy | 3.518 | 3.318 | _ | _ | 2.218 | _ |
| | | | | | | |
| Pot Cap-1 Maneuver | 664 | 936 | - | - | 1473 | - |
| Stage 1 | 909 | - | - | - | - | - |
| Stage 2 | 821 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 663 | 936 | - | - | 1473 | - |
| Mov Cap-2 Maneuver | 663 | - | - | - | - | - |
| Stage 1 | 909 | _ | - | - | - | _ |
| Stage 2 | 820 | - | - | - | - | - |
| J | | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.4 | | 0 | | 0 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Long (Maior M | _1 | NDT | NDDV | VDL 4 | ODI | CDT |
| Minor Lane/Major Mvn | π | NBT | | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - | - | | 1473 | - |
| HCM Lane V/C Ratio | | - | - | 0.005 | | - |
| HCM Control Delay (s) | | - | - | 9.4 | 7.4 | 0 |
| HCM Lane LOS | | - | - | Α | Α | Α |
| HCM 95th %tile Q(veh |) | - | - | 0 | 0 | - |
| | | | | | | |

| Intersection | | | |
|---------------------------|------|--|--|
| Intersection Delay, s/veh | 13.8 | | |
| Intersection LOS | В | | |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 17 | 235 | 86 | 22 | 225 | 51 | 121 | 25 | 19 | 53 | 42 | 12 |
| Future Vol, veh/h | 17 | 235 | 86 | 22 | 225 | 51 | 121 | 25 | 19 | 53 | 42 | 12 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 19 | 267 | 98 | 25 | 256 | 58 | 138 | 28 | 22 | 60 | 48 | 14 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 15.1 | | | 14.1 | | | 12.2 | | | 11.1 | | |
| HCM LOS | С | | | В | | | В | | | В | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 73% | 5% | 7% | 50% | |
| Vol Thru, % | 15% | 70% | 76% | 39% | |
| Vol Right, % | 12% | 25% | 17% | 11% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 165 | 338 | 298 | 107 | |
| LT Vol | 121 | 17 | 22 | 53 | |
| Through Vol | 25 | 235 | 225 | 42 | |
| RT Vol | 19 | 86 | 51 | 12 | |
| Lane Flow Rate | 188 | 384 | 339 | 122 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.322 | 0.566 | 0.51 | 0.213 | |
| Departure Headway (Hd) | 6.183 | 5.305 | 5.422 | 6.3 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Сар | 578 | 676 | 660 | 565 | |
| Service Time | 4.26 | 3.368 | 3.487 | 4.385 | |
| HCM Lane V/C Ratio | 0.325 | 0.568 | 0.514 | 0.216 | |
| HCM Control Delay | 12.2 | 15.1 | 14.1 | 11.1 | |
| HCM Lane LOS | В | С | В | В | |
| HCM 95th-tile Q | 1.4 | 3.6 | 2.9 | 0.8 | |

| Note Note |
|---|
| Traffic Vol, veh/h |
| Traffic Vol, veh/h 16 0 4 0 0 0 3 149 0 0 139 11 Future Vol, veh/h 16 0 4 0 |
| Traffic Vol, veh/h 16 0 4 0 0 0 3 149 0 0 139 11 Future Vol, veh/h 16 0 4 0 0 0 3 149 0 0 139 11 Conflicting Peds, #/hr 0 |
| Conflicting Peds, #/hr |
| Sign Control Stop Stop Stop Stop Stop Stop Free |
| RT Channelized - None -< |
| Storage Length - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - - 0 - |
| Veh in Median Storage, # 0 - 0 0 - 0 0 - 0 0 - 0 0 - - 0 0 - - 0 0 - - 0 0 - - 0 0 - - 7 2 2 2 2 2 |
| Grade, % - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 - - 0 0 - - 0 0 - - 0 0 - - 0 0 - - 0 0 - - 0 0 - - 0 1 - 0 1 - 0 1 0 1 0 1 1 0 0 0 1 1 0 0 0 1 1 0 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0< |
| Peak Hour Factor 75 |
| Heavy Vehicles, % 2 2 2 2 2 2 2 2 2 |
| Mount Flow 21 0 5 0 0 0 4 199 0 0 185 15 Major/Minor Minor2 Minor1 Major1 Major2 Conflicting Flow All 400 400 193 402 407 199 200 0 0 199 0 0 Stage 1 193 193 - 207 207 - |
| Major/Minor Minor2 Minor1 Major1 Major2 Conflicting Flow All 400 400 193 402 407 199 200 0 0 199 0 0 Stage 1 193 193 - 207 207 - |
| Conflicting Flow All 400 400 193 402 407 199 200 0 199 0 0 Stage 1 193 193 - 207 207 - <t< td=""></t<> |
| Conflicting Flow All 400 400 193 402 407 199 200 0 199 0 0 Stage 1 193 193 - 207 207 - <t< td=""></t<> |
| Stage 1 193 193 - 207 207 - |
| Stage 2 207 207 - 195 200 - |
| Critical Hdwy 7.12 6.52 6.22 7.12 6.52 6.22 4.12 - - 4.12 - |
| Critical Hdwy Stg 1 6.12 5.52 - 6.12 5.52 - |
| Critical Hdwy Stg 2 6.12 5.52 - 6.12 5.52 - |
| Follow-up Hdwy 3.518 4.018 3.318 3.518 4.018 3.318 2.218 2.218 Pot Cap-1 Maneuver 560 538 849 559 533 842 1372 - 1373 Stage 1 809 741 - 795 731 |
| Pot Cap-1 Maneuver 560 538 849 559 533 842 1372 - - 1373 - - Stage 1 809 741 - 795 731 - |
| Stage 1 809 741 - 795 731 - |
| Stage 2 795 731 - 807 736 |
| Platoon blocked, % |
| · |
| Mov Cap-1 Maneuver 559 536 849 554 531 842 1372 1373 |
| · |
| Mov Cap-2 Maneuver 559 536 - 554 531 |
| Stage 1 807 741 - 793 729 |
| Stage 2 793 729 - 802 736 |
| |
| Approach EB WB NB SB |
| HCM Control Delay, s 11.3 0 0.2 0 |
| HCM LOS B A |
| |
| Minor Lane/Major Mvmt NBL NBT NBR EBLn1WBLn1 SBL SBT SBR |
| Capacity (veh/h) 1372 600 - 1373 |
| HCM Lane V/C Ratio 0.003 0.044 |
| HCM Control Delay (s) 7.6 0 - 11.3 0 0 |
| HCM Lane LOS A A - B A A |
| HCM 95th %tile Q(veh) 0 0.1 - 0 |

| Intersection | | | | | | |
|------------------------|--------|----------|--------|----------|----------|------|
| Int Delay, s/veh | 1 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | ¥ | | 1 | | | सी |
| Traffic Vol, veh/h | 1 | 13 | 139 | 1 | 20 | 123 |
| Future Vol. veh/h | 1 | 13 | 139 | 1 | 20 | 123 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | | - | None |
| Storage Length | 0 | - | _ | - | _ | - |
| Veh in Median Storage | | _ | 0 | _ | _ | 0 |
| Grade, % | 0, # 0 | <u>-</u> | 0 | <u>-</u> | _ | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| | 2 | 2 | 2 | 2 | 2 | 2 |
| Heavy Vehicles, % | | | | | | |
| Mvmt Flow | 1 | 17 | 185 | 1 | 27 | 164 |
| | | | | | | |
| Major/Minor | Minor1 | N | Major1 | ľ | Major2 | |
| Conflicting Flow All | 404 | 186 | 0 | 0 | 186 | 0 |
| Stage 1 | 186 | - | _ | _ | - | _ |
| Stage 2 | 218 | _ | _ | _ | _ | _ |
| Critical Hdwy | 6.42 | 6.22 | _ | _ | 4.12 | _ |
| Critical Hdwy Stg 1 | 5.42 | - | _ | _ | 7.12 | _ |
| Critical Hdwy Stg 2 | 5.42 | _ | | _ | _ | _ |
| | 3.518 | | _ | | 2.218 | - |
| Follow-up Hdwy | | | _ | | 1388 | |
| Pot Cap-1 Maneuver | 603 | 856 | - | - | 1300 | - |
| Stage 1 | 846 | - | - | - | | - |
| Stage 2 | 818 | - | - | - | - | - |
| Platoon blocked, % | | 0-0 | - | - | 1000 | - |
| Mov Cap-1 Maneuver | | 856 | - | - | 1388 | - |
| Mov Cap-2 Maneuver | | - | - | - | - | - |
| Stage 1 | 846 | - | - | - | - | - |
| Stage 2 | 801 | - | - | - | - | - |
| | | | | | | |
| Annroach | WB | | NB | | SB | |
| Approach | | | | | | |
| HCM Control Delay, s | | | 0 | | 1.1 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Lane/Major Mvr | mt | NBT | NBRV | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - | - | 829 | 1388 | |
| HCM Lane V/C Ratio | | _ | _ | 0.023 | | - |
| HCM Control Delay (s | .) | _ | | 9.4 | 7.6 | 0 |
| HCM Lane LOS | 1 | _ | - | 9.4 A | 7.0 A | A |
| HCM 95th %tile Q(veh | 2) | | | 0.1 | 0.1 | |
| | 1) | - | _ | 0.1 | 0.1 | - |

| Intersection | | | | | | |
|-------------------------------------|--------|-------|---------|--------|----------|--------|
| Int Delay, s/veh | 0.2 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | Y | | ₽ | | | ્ન |
| Traffic Vol, veh/h | 2 | 2 | 138 | 1 | 3 | 121 |
| Future Vol, veh/h | 2 | 2 | 138 | 1 | 3 | 121 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | _ | - | _ | - |
| Veh in Median Storage | | _ | 0 | _ | _ | 0 |
| Grade, % | 0 | _ | 0 | _ | _ | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mymt Flow | 3 | 3 | 184 | 1 | 4 | 161 |
| IVIVIIIL I IOW | 3 | J | 104 | ļ | 7 | 101 |
| | | | | | | |
| Major/Minor | Minor1 | N | //ajor1 | ا | Major2 | |
| Conflicting Flow All | 354 | 185 | 0 | 0 | 185 | 0 |
| Stage 1 | 185 | - | - | - | - | - |
| Stage 2 | 169 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | _ |
| Follow-up Hdwy | 3.518 | 3.318 | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 644 | 857 | - | - | 1390 | - |
| Stage 1 | 847 | - | - | - | - | - |
| Stage 2 | 861 | - | - | _ | - | - |
| Platoon blocked, % | | | _ | _ | | - |
| Mov Cap-1 Maneuver | 642 | 857 | - | - | 1390 | - |
| Mov Cap-2 Maneuver | 642 | - | _ | _ | - | _ |
| Stage 1 | 847 | _ | _ | _ | _ | _ |
| Stage 2 | 858 | _ | _ | _ | _ | _ |
| Clago 2 | 000 | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.9 | | 0 | | 0.2 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | nt | NBT | NRRV | VBLn1 | SBL | SBT |
| | ıı | וטוו | INDIX | 734 | 1390 | ODT |
| Capacity (veh/h) HCM Lane V/C Ratio | | - | - | 0.007 | | - |
| HCM Control Delay (s) | | - | - | 9.9 | 7.6 | 0 |
| HCM Lane LOS | | _ | - | | 7.0 A | A |
| HCM 95th %tile Q(veh) | ١ | - | - | A 0 | 0 | - - |
| HOW SOUT WITH Q(VEI) | | - | - | U | U | - |

| Intersection | | |
|---------------------------|------|--|
| Intersection Delay, s/veh | 15.8 | |
| Intersection LOS | С | |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 9 | 330 | 186 | 20 | 135 | 14 | 80 | 9 | 18 | 16 | 21 | 6 |
| Future Vol, veh/h | 9 | 330 | 186 | 20 | 135 | 14 | 80 | 9 | 18 | 16 | 21 | 6 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 10 | 375 | 211 | 23 | 153 | 16 | 91 | 10 | 20 | 18 | 24 | 7 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 19.2 | | | 10.1 | | | 10.4 | | | 9.6 | | |
| HCM LOS | С | | | В | | | В | | | Α | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 75% | 2% | 12% | 37% | |
| Vol Thru, % | 8% | 63% | 80% | 49% | |
| Vol Right, % | 17% | 35% | 8% | 14% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 107 | 525 | 169 | 43 | |
| LT Vol | 80 | 9 | 20 | 16 | |
| Through Vol | 9 | 330 | 135 | 21 | |
| RT Vol | 18 | 186 | 14 | 6 | |
| Lane Flow Rate | 122 | 597 | 192 | 49 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.201 | 0.738 | 0.276 | 0.082 | |
| Departure Headway (Hd) | 5.939 | 4.451 | 5.169 | 6.05 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Сар | 607 | 805 | 699 | 594 | |
| Service Time | 3.952 | 2.535 | 3.169 | 4.068 | |
| HCM Lane V/C Ratio | 0.201 | 0.742 | 0.275 | 0.082 | |
| HCM Control Delay | 10.4 | 19.2 | 10.1 | 9.6 | |
| HCM Lane LOS | В | С | В | Α | |
| HCM 95th-tile Q | 0.7 | 6.7 | 1.1 | 0.3 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|--------|--------|-------|--------|------|------|--------|------|------|
| Int Delay, s/veh | 2.3 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 74 | 0 | 0 | 179 | 48 |
| Future Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 74 | 0 | 0 | 179 | 48 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | e,# - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 44 | 0 | 28 | 0 | 0 | 0 | 43 | 99 | 0 | 0 | 239 | 64 |
| | | | | | | | | | | | | |
| Major/Minor I | Minor2 | | | Minor1 | | | Major1 | | | Major2 | | |
| Conflicting Flow All | 456 | 456 | 271 | 470 | 488 | 99 | 303 | 0 | 0 | 99 | 0 | 0 |
| Stage 1 | 271 | 271 | - | 185 | 185 | - | - | - | - | - | - | - |
| Stage 2 | 185 | 185 | - | 285 | 303 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 515 | 501 | 768 | 504 | 480 | 957 | 1258 | - | - | 1494 | - | - |
| Stage 1 | 735 | 685 | - | 817 | 747 | - | - | - | - | - | - | - |
| Stage 2 | 817 | 747 | - | 722 | 664 | - | - | - | - | - | - | - |
| Platoon blocked, % | | , | | , | , | | 10=5 | - | - | | - | - |
| Mov Cap-1 Maneuver | 501 | 483 | 768 | 472 | 463 | 957 | 1258 | - | - | 1494 | - | - |
| Mov Cap-2 Maneuver | 501 | 483 | - | 472 | 463 | - | - | - | - | - | - | - |
| Stage 1 | 709 | 685 | - | 788 | 720 | - | - | - | - | - | - | - |
| Stage 2 | 788 | 720 | - | 696 | 664 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 12.1 | | | 0 | | | 2.4 | | | 0 | | |
| HCM LOS | В | | | Α | | | | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NBR | EBLn1V | WBLn1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1258 | - | - | 579 | - | 1494 | - | | | | |
| HCM Lane V/C Ratio | | 0.034 | - | - | 0.124 | - | - | - | - | | | |
| HCM Control Delay (s) | | 8 | 0 | - | | 0 | 0 | - | - | | | |
| HCM Lane LOS | | Α | Α | - | В | Α | Α | - | - | | | |
| HCM 95th %tile Q(veh) |) | 0.1 | - | - | 0.4 | - | 0 | - | - | | | |
| | | | | | | | | | | | | |

| Intersection | | | | | | |
|------------------------|--------|--------|--------|----------|----------|------|
| Int Delay, s/veh | 0.3 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | ¥ | | ₽ | | | ન |
| Traffic Vol, veh/h | 3 | 5 | 101 | 3 | 3 | 197 |
| Future Vol, veh/h | 3 | 5 | 101 | 3 | 3 | 197 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | _ | - | _ | - |
| Veh in Median Storage | | _ | 0 | _ | - | 0 |
| Grade, % | 0 | _ | 0 | _ | _ | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mymt Flow | 4 | 7 | 135 | 4 | 4 | 263 |
| IVIVIIIL I IOW | 7 | - 1 | 100 | 7 | 7 | 200 |
| | | | | | | |
| Major/Minor I | Minor1 | | Major1 | | Major2 | |
| Conflicting Flow All | 408 | 137 | 0 | 0 | 139 | 0 |
| Stage 1 | 137 | - | - | - | - | - |
| Stage 2 | 271 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 3.318 | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 599 | 911 | - | - | 1445 | - |
| Stage 1 | 890 | - | - | - | - | - |
| Stage 2 | 775 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 597 | 911 | - | - | 1445 | - |
| Mov Cap-2 Maneuver | 597 | - | - | - | - | - |
| Stage 1 | 890 | - | - | - | - | - |
| Stage 2 | 773 | - | - | _ | _ | _ |
| 5 III G = | | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.8 | | 0 | | 0.1 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | nt | NBT | NRRV | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - 1101 | - | 761 | 1445 | - |
| HCM Lane V/C Ratio | | _ | _ | 0.014 | | _ |
| HCM Control Delay (s) | | | _ | 9.8 | 7.5 | 0 |
| HCM Lane LOS | | _ | _ | 9.0 A | 7.5 A | A |
| HCM 95th %tile Q(veh) | \ | | _ | 0 | 0 | - |
| HOW JOHN JOHN GUVEN | | | | U | U | |

| Intersection | | |
|---------------------------|------|--|
| Intersection Delay, s/veh | 19.4 | |
| Intersection LOS | С | |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 21 | 285 | 96 | 11 | 270 | 62 | 142 | 30 | 14 | 64 | 49 | 15 |
| Future Vol, veh/h | 21 | 285 | 96 | 11 | 270 | 62 | 142 | 30 | 14 | 64 | 49 | 15 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 24 | 324 | 109 | 13 | 307 | 70 | 161 | 34 | 16 | 73 | 56 | 17 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 23.7 | | | 19.4 | | | 14.7 | | | 12.9 | | |
| HCM LOS | С | | | С | | | В | | | В | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 76% | 5% | 3% | 50% | |
| Vol Thru, % | 16% | 71% | 79% | 38% | |
| Vol Right, % | 8% | 24% | 18% | 12% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 186 | 402 | 343 | 128 | |
| LT Vol | 142 | 21 | 11 | 64 | |
| Through Vol | 30 | 285 | 270 | 49 | |
| RT Vol | 14 | 96 | 62 | 15 | |
| Lane Flow Rate | 211 | 457 | 390 | 145 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.406 | 0.739 | 0.646 | 0.285 | |
| Departure Headway (Hd) | 6.907 | 5.827 | 5.97 | 7.049 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Cap | 520 | 621 | 604 | 508 | |
| Service Time | 4.97 | 3.878 | 4.025 | 5.12 | |
| HCM Lane V/C Ratio | 0.406 | 0.736 | 0.646 | 0.285 | |
| HCM Control Delay | 14.7 | 23.7 | 19.4 | 12.9 | |
| HCM Lane LOS | В | С | С | В | |
| HCM 95th-tile Q | 2 | 6.4 | 4.7 | 1.2 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|--------|--------|-------|--------|------|------|--------|------|------|
| Int Delay, s/veh | 0.7 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 16 | 0 | 4 | 0 | 0 | 0 | 3 | 170 | 0 | 0 | 145 | 11 |
| Future Vol, veh/h | 16 | 0 | 4 | 0 | 0 | 0 | 3 | 170 | 0 | 0 | 145 | 11 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | ,# - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 21 | 0 | 5 | 0 | 0 | 0 | 4 | 227 | 0 | 0 | 193 | 15 |
| | | | | | | | | | | | | |
| Major/Minor N | Minor2 | | | Minor1 | | | Major1 | | | Major2 | | |
| Conflicting Flow All | 436 | 436 | 201 | 438 | 443 | 227 | 208 | 0 | 0 | 227 | 0 | 0 |
| Stage 1 | 201 | 201 | - | 235 | 235 | - | - | - | - | - | - | - |
| Stage 2 | 235 | 235 | - | 203 | 208 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 531 | 514 | 840 | 529 | 509 | 812 | 1363 | - | - | 1341 | - | - |
| Stage 1 | 801 | 735 | - | 768 | 710 | - | - | - | - | - | - | - |
| Stage 2 | 768 | 710 | - | 799 | 730 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | - | - |
| Mov Cap-1 Maneuver | 530 | 512 | 840 | 524 | 507 | 812 | 1363 | - | - | 1341 | - | - |
| Mov Cap-2 Maneuver | 530 | 512 | - | 524 | 507 | - | - | - | - | - | - | - |
| Stage 1 | 799 | 735 | - | 766 | 708 | - | - | - | - | - | - | - |
| Stage 2 | 766 | 708 | - | 794 | 730 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 11.6 | | | 0 | | | 0.1 | | | 0 | | |
| HCM LOS | В | | | Α | | | | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NBR | EBLn1V | VBLn1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1363 | - | - | | - | 1341 | - | | | | |
| HCM Lane V/C Ratio | | 0.003 | - | _ | 0.047 | _ | - | _ | _ | | | |
| HCM Control Delay (s) | | 7.6 | 0 | _ | 11.6 | 0 | 0 | _ | _ | | | |
| HCM Lane LOS | | A | A | - | В | A | A | - | - | | | |
| HCM 95th %tile Q(veh) | | 0 | - | - | 0.1 | - | 0 | - | - | | | |
| | | | | | | | | | | | | |

| Intersection | | | | | | |
|------------------------|--------|-------|--------|-------|--------|------|
| Int Delay, s/veh | 0.5 | | | | | |
| | | 14/55 | NET | NE | 00' | 00= |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | Y | | 1 | | | र्भ |
| Traffic Vol, veh/h | 5 | 5 | 168 | 3 | 5 | 144 |
| Future Vol, veh/h | 5 | 5 | 168 | 3 | 5 | 144 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | e, # 0 | - | 0 | - | - | 0 |
| Grade, % | 0 | - | 0 | - | - | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 7 | 7 | 224 | 4 | 7 | 192 |
| | | | | ſ | • | .02 |
| | | | | | | |
| | Minor1 | | Major1 | | Major2 | |
| Conflicting Flow All | 432 | 226 | 0 | 0 | 228 | 0 |
| Stage 1 | 226 | - | - | - | - | - |
| Stage 2 | 206 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | _ | _ | _ | _ |
| Critical Hdwy Stg 2 | 5.42 | - | _ | _ | _ | _ |
| Follow-up Hdwy | 3.518 | 3.318 | _ | - | 2.218 | - |
| Pot Cap-1 Maneuver | 581 | 813 | _ | _ | 1340 | _ |
| Stage 1 | 812 | - | _ | _ | - | _ |
| Stage 2 | 829 | _ | _ | _ | _ | _ |
| Platoon blocked, % | 023 | | | | | |
| Mov Cap-1 Maneuver | 578 | 813 | _ | _ | 1340 | |
| | 578 | 013 | - | - | 1340 | - |
| Mov Cap-2 Maneuver | | | - | - | - | - |
| Stage 1 | 812 | - | - | - | - | - |
| Stage 2 | 824 | - | - | - | - | - |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 10.4 | | 0 | | 0.3 | |
| HCM LOS | В | | U | | 0.5 | |
| I IOIVI LOO | ט | | | | | |
| | | | | | | |
| Minor Lane/Major Mvn | nt | NBT | NBRV | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - | - | 676 | 1340 | - |
| HCM Lane V/C Ratio | | - | - | | 0.005 | - |
| HCM Control Delay (s) | | - | - | 10.4 | 7.7 | 0 |
| HCM Lane LOS | | - | - | В | Α | A |
| HCM 95th %tile Q(veh |) | _ | _ | 0.1 | 0 | - |
| | , | | | 3.1 | | |

| Intersection | |
|---------------------------|------|
| Intersection Delay, s/veh | 17.2 |
| ntersection Delay, s/ven | 17.2 |
| Intersection LOS | C |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 9 | 330 | 191 | 31 | 135 | 14 | 85 | 10 | 28 | 16 | 22 | 6 |
| Future Vol, veh/h | 9 | 330 | 191 | 31 | 135 | 14 | 85 | 10 | 28 | 16 | 22 | 6 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 10 | 375 | 217 | 35 | 153 | 16 | 97 | 11 | 32 | 18 | 25 | 7 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 21.6 | | | 10.5 | | | 10.8 | | | 9.8 | | |
| HCM LOS | С | | | В | | | В | | | Α | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 69% | 2% | 17% | 36% | |
| Vol Thru, % | 8% | 62% | 75% | 50% | |
| Vol Right, % | 23% | 36% | 8% | 14% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 123 | 530 | 180 | 44 | |
| LT Vol | 85 | 9 | 31 | 16 | |
| Through Vol | 10 | 330 | 135 | 22 | |
| RT Vol | 28 | 191 | 14 | 6 | |
| Lane Flow Rate | 140 | 602 | 205 | 50 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.232 | 0.774 | 0.299 | 0.086 | |
| Departure Headway (Hd) | 5.964 | 4.625 | 5.261 | 6.168 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Сар | 601 | 789 | 683 | 580 | |
| Service Time | 4.011 | 2.625 | 3.298 | 4.222 | |
| HCM Lane V/C Ratio | 0.233 | 0.763 | 0.3 | 0.086 | |
| HCM Control Delay | 10.8 | 21.6 | 10.5 | 9.8 | |
| HCM Lane LOS | В | С | В | Α | |
| HCM 95th-tile Q | 0.9 | 7.6 | 1.3 | 0.3 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|--------|-----------|--------|--------|------|------|--------|------|------|
| Int Delay, s/veh | 2.2 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 90 | 0 | 0 | 196 | 48 |
| Future Vol, veh/h | 33 | 0 | 21 | 0 | 0 | 0 | 32 | 90 | 0 | 0 | 196 | 48 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | e,# - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 44 | 0 | 28 | 0 | 0 | 0 | 43 | 120 | 0 | 0 | 261 | 64 |
| | | | | | | | | | | | | |
| Major/Minor I | Minor2 | | | Minor1 | | | Major1 | | 1 | Major2 | | |
| Conflicting Flow All | 499 | 499 | 293 | 513 | 531 | 120 | 325 | 0 | 0 | 120 | 0 | 0 |
| Stage 1 | 293 | 293 | | 206 | 206 | - | - | _ | _ | - | _ | - |
| Stage 2 | 206 | 206 | - | 307 | 325 | _ | _ | _ | _ | _ | _ | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | _ | 6.12 | 5.52 | _ | _ | - | _ | _ | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | _ | _ | _ | _ | _ | - | - |
| Follow-up Hdwy | 3.518 | | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | _ | 2.218 | - | - |
| Pot Cap-1 Maneuver | 482 | 473 | 746 | 472 | 454 | 931 | 1235 | _ | _ | 1468 | - | - |
| Stage 1 | 715 | 670 | - | 796 | 731 | - | _ | - | _ | _ | - | - |
| Stage 2 | 796 | 731 | - | 703 | 649 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | _ | - |
| Mov Cap-1 Maneuver | 469 | 455 | 746 | 441 | 437 | 931 | 1235 | - | - | 1468 | - | - |
| Mov Cap-2 Maneuver | 469 | 455 | - | 441 | 437 | _ | - | - | - | - | - | - |
| Stage 1 | 689 | 670 | - | 767 | 704 | - | - | - | - | - | - | - |
| Stage 2 | 767 | 704 | - | 677 | 649 | - | - | - | - | - | - | - |
| Ŭ. | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 12.6 | | | 0 | | | 2.1 | | | 0 | | |
| HCM LOS | В | | | A | | | =:: | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvm | nt | NBL | NBT | NBR | EBLn1V | WBI n1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1235 | - | - | 548 | - | 1468 | - | - | | | |
| HCM Lane V/C Ratio | | 0.035 | _ | | 0.131 | _ | - | _ | _ | | | |
| HCM Control Delay (s) | | 8 | 0 | _ | 12.6 | 0 | 0 | _ | _ | | | |
| HCM Lane LOS | | A | A | _ | 12.0 B | A | A | _ | | | | |
| HCM 95th %tile Q(veh) |) | 0.1 | - | _ | 0.5 | - | 0 | _ | _ | | | |
| TION JOHN JUNE Q(VEII) | J | 0.1 | | | 0.0 | | | | | | | |

| Intersection | | | | | | |
|---|--------|-------|--------|--------------|--------------|--------------|
| Int Delay, s/veh | 0.8 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| | | וטייי | | וטוו | ODL | |
| Lane Configurations | Y | 40 | 100 | 4 | 47 | વ |
| Traffic Vol, veh/h | 1 | 16 | 106 | 1 | 17 | 200 |
| Future Vol, veh/h | 1 | 16 | 106 | 1 | 17 | 200 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | e, # 0 | - | 0 | - | - | 0 |
| Grade, % | 0 | - | 0 | - | - | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 1 | 21 | 141 | 1 | 23 | 267 |
| IVIVIIIL I IOW | | 21 | 141 | | 23 | 201 |
| | | | | | | |
| Major/Minor I | Minor1 | N | Major1 | N | Major2 | |
| Conflicting Flow All | 455 | 142 | 0 | 0 | 142 | 0 |
| Stage 1 | 142 | _ | _ | _ | _ | _ |
| Stage 2 | 313 | _ | _ | _ | _ | _ |
| Critical Hdwy | 6.42 | 6.22 | _ | _ | 4.12 | _ |
| Critical Hdwy Stg 1 | 5.42 | - | _ | _ | - | |
| | 5.42 | | - | - | _ | - |
| Critical Hdwy Stg 2 | | - | - | - | - 0.40 | - |
| Follow-up Hdwy | 3.518 | | - | | 2.218 | - |
| Pot Cap-1 Maneuver | 563 | 906 | - | - | 1441 | - |
| Stage 1 | 885 | - | - | - | - | - |
| Stage 2 | 741 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 552 | 906 | - | - | 1441 | - |
| Mov Cap-2 Maneuver | 552 | _ | - | - | _ | _ |
| Stage 1 | 885 | _ | _ | _ | _ | _ |
| Stage 2 | 727 | _ | _ | _ | _ | _ |
| Glage 2 | 121 | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.2 | | 0 | | 0.6 | |
| HCM LOS | Α | | | | | |
| 110111 200 | , , | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | nt | NBT | NBRV | VBLn1 | SBL | SBT |
| IVIIIIOI Lane/Iviajoi Ivivii | 11 | | | 072 | 1441 | - |
| | ıı | - | - | 873 | | |
| Capacity (veh/h) | | - | | | | - |
| Capacity (veh/h) HCM Lane V/C Ratio | | | | 0.026 | 0.016 | |
| Capacity (veh/h) HCM Lane V/C Ratio HCM Control Delay (s) | | - | - | 0.026 9.2 | 0.016 7.5 | 0 |
| Capacity (veh/h) HCM Lane V/C Ratio |) | - | - | 0.026 | 0.016 | |

| Intersection | | | | | | |
|------------------------|--------|----------|--------|-------|--------|--------------|
| Int Delay, s/veh | 0.3 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | WDL | אטוז | | NON | ODL | <u>अज्ञा</u> |
| Traffic Vol, veh/h | 3 | 5 | 102 | 3 | 3 | 198 |
| Future Vol, veh/h | 3 | 5 | 102 | 3 | 3 | 198 |
| - | 0 | 0 | 102 | 0 | 0 | |
| Conflicting Peds, #/hr | | | | | | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | | - | 0 | - | - | 0 |
| Grade, % | 0 | - | 0 | - | - | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 4 | 7 | 136 | 4 | 4 | 264 |
| | | | | | | |
| Majay/Minay | N 4: 1 | | 10:01 | | Maia#0 | |
| | Minor1 | | Major1 | | Major2 | |
| Conflicting Flow All | 410 | 138 | 0 | 0 | 140 | 0 |
| Stage 1 | 138 | - | - | - | - | - |
| Stage 2 | 272 | - | - | - | - | - |
| Critical Hdwy | 6.42 | 6.22 | - | - | 4.12 | - |
| Critical Hdwy Stg 1 | 5.42 | - | - | - | - | - |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 3.318 | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 598 | 910 | - | - | 1443 | - |
| Stage 1 | 889 | - | - | - | - | - |
| Stage 2 | 774 | _ | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 596 | 910 | _ | _ | 1443 | _ |
| Mov Cap-2 Maneuver | 596 | - | _ | _ | - | _ |
| Stage 1 | 889 | _ | _ | _ | _ | _ |
| Stage 2 | 772 | <u>-</u> | | | _ | |
| Stage 2 | 112 | - | - | - | - | _ |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.8 | | 0 | | 0.1 | |
| HCM LOS | A | | | | | |
| | , , | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | nt | NBT | NBRV | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - | - | 760 | 1443 | - |
| HCM Lane V/C Ratio | | - | - | 0.014 | 0.003 | - |
| HCM Control Delay (s) | | - | - | 9.8 | 7.5 | 0 |
| HCM Lane LOS | | - | - | Α | Α | Α |
| HCM 95th %tile Q(veh) |) | - | - | 0 | 0 | - |
| | | | | | | |

| Intersection | |
|---------------------------|------|
| Intersection Delay, s/veh | 21.2 |
| Intersection LOS | С |

| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
|----------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 21 | 285 | 102 | 24 | 270 | 62 | 146 | 31 | 22 | 64 | 50 | 15 |
| Future Vol, veh/h | 21 | 285 | 102 | 24 | 270 | 62 | 146 | 31 | 22 | 64 | 50 | 15 |
| Peak Hour Factor | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 24 | 324 | 116 | 27 | 307 | 70 | 166 | 35 | 25 | 73 | 57 | 17 |
| Number of Lanes | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 1 | 0 |
| Approach | EB | | | WB | | | NB | | | SB | | |
| Opposing Approach | WB | | | EB | | | SB | | | NB | | |
| Opposing Lanes | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Left | SB | | | NB | | | EB | | | WB | | |
| Conflicting Lanes Left | 1 | | | 1 | | | 1 | | | 1 | | |
| Conflicting Approach Right | NB | | | SB | | | WB | | | EB | | |
| Conflicting Lanes Right | 1 | | | 1 | | | 1 | | | 1 | | |
| HCM Control Delay | 26.1 | | | 21.6 | | | 15.5 | | | 13.3 | | |
| HCM LOS | D | | | С | | | С | | | В | | |

| Lane | NBLn1 | EBLn1 | WBLn1 | SBLn1 | |
|------------------------|-------|-------|-------|-------|--|
| Vol Left, % | 73% | 5% | 7% | 50% | |
| Vol Thru, % | 16% | 70% | 76% | 39% | |
| Vol Right, % | 11% | 25% | 17% | 12% | |
| Sign Control | Stop | Stop | Stop | Stop | |
| Traffic Vol by Lane | 199 | 408 | 356 | 129 | |
| LT Vol | 146 | 21 | 24 | 64 | |
| Through Vol | 31 | 285 | 270 | 50 | |
| RT Vol | 22 | 102 | 62 | 15 | |
| Lane Flow Rate | 226 | 464 | 405 | 147 | |
| Geometry Grp | 1 | 1 | 1 | 1 | |
| Degree of Util (X) | 0.44 | 0.767 | 0.686 | 0.294 | |
| Departure Headway (Hd) | 7.007 | 5.959 | 6.109 | 7.231 | |
| Convergence, Y/N | Yes | Yes | Yes | Yes | |
| Сар | 511 | 607 | 591 | 494 | |
| Service Time | 5.082 | 4.017 | 4.171 | 5.317 | |
| HCM Lane V/C Ratio | 0.442 | 0.764 | 0.685 | 0.298 | |
| HCM Control Delay | 15.5 | 26.1 | 21.6 | 13.3 | |
| HCM Lane LOS | С | D | С | В | |
| HCM 95th-tile Q | 2.2 | 7 | 5.3 | 1.2 | |

| Intersection | | | | | | | | | | | | |
|------------------------|--------|-------|-------|----------|-----------|--------|--------|------|------|--------|------|------|
| Int Delay, s/veh | 0.7 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | 4 | | | 4 | | | 4 | | | 4 | |
| Traffic Vol, veh/h | 16 | 0 | 4 | 0 | 0 | 0 | 3 | 183 | 0 | 0 | 165 | 11 |
| Future Vol, veh/h | 16 | 0 | 4 | 0 | 0 | 0 | 3 | 183 | 0 | 0 | 165 | 11 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage | e,# - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 21 | 0 | 5 | 0 | 0 | 0 | 4 | 244 | 0 | 0 | 220 | 15 |
| | | | | | | | | | | | | |
| Major/Minor | Minor2 | | | Minor1 | | | Major1 | | | Major2 | | |
| Conflicting Flow All | 480 | 480 | 228 | 482 | 487 | 244 | 235 | 0 | 0 | 244 | 0 | 0 |
| Stage 1 | 228 | 228 | | 252 | 252 | | - | - | _ | - | - | - |
| Stage 2 | 252 | 252 | - | 230 | 235 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 496 | 485 | 811 | 495 | 481 | 795 | 1332 | - | - | 1322 | - | - |
| Stage 1 | 775 | 715 | - | 752 | 698 | - | - | - | - | - | - | - |
| Stage 2 | 752 | 698 | - | 773 | 710 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | | - | - |
| Mov Cap-1 Maneuver | 495 | 484 | 811 | 491 | 480 | 795 | 1332 | - | - | 1322 | - | - |
| Mov Cap-2 Maneuver | 495 | 484 | - | 491 | 480 | - | - | - | - | - | - | - |
| Stage 1 | 773 | 715 | - | 750 | 696 | - | - | - | - | - | - | - |
| Stage 2 | 750 | 696 | - | 768 | 710 | - | - | - | - | - | - | - |
| | | | | | | | | | | | | |
| Approach | EB | | | WB | | | NB | | | SB | | |
| HCM Control Delay, s | 12.1 | | | 0 | | | 0.1 | | | 0 | | |
| HCM LOS | В | | | A | | | | | | | | |
| | | | | | | | | | | | | |
| Minor Lane/Major Mvn | nt | NBL | NBT | NRR | EBLn1\ | VBI n1 | SBL | SBT | SBR | | | |
| Capacity (veh/h) | | 1332 | - | - | 537 | - | 1322 | - | ODIT | | | |
| HCM Lane V/C Ratio | | 0.003 | _ | <u> </u> | 0.05 | _ | 1022 | _ | | | | |
| HCM Control Delay (s) | | 7.7 | 0 | | 12.1 | 0 | 0 | _ | | | | |
| HCM Lane LOS | | Α. | A | <u>-</u> | 12.1 B | A | A | _ | _ | | | |
| HCM 95th %tile Q(veh |) | 0 | - | _ | 0.2 | - | 0 | _ | _ | | | |
| | 1 | - | | | V.L | | • | | | | | |

| Intersection | | | | | | |
|------------------------|--------|------|------------|-------|--------|-------------|
| Int Delay, s/veh | 0.8 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| | | אטוו | | אטוז | JDL | |
| Lane Configurations | Y | 40 | 170 | 4 | 00 | ્ર ન |
| Traffic Vol, veh/h | 1 | 13 | 173 | 1 | 20 | 149 |
| Future Vol, veh/h | 1 | 13 | 173 | 1 | 20 | 149 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Free | Free | Free | Free |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | 0 | - | - | - | - | - |
| Veh in Median Storage | , # 0 | - | 0 | - | - | 0 |
| Grade, % | 0 | - | 0 | - | - | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 1 | 17 | 231 | 1 | 27 | 199 |
| IVIVIIIL FIOW | 1 | 17 | 231 | ı | 21 | 199 |
| | | | | | | |
| Major/Minor N | Minor1 | N | Major1 | N | Major2 | |
| Conflicting Flow All | 485 | 232 | 0 | 0 | 232 | 0 |
| Stage 1 | 232 | | _ | _ | | _ |
| Stage 2 | 253 | _ | _ | _ | _ | _ |
| Critical Hdwy | 6.42 | 6.22 | _ | _ | 4.12 | _ |
| • | 5.42 | 0.22 | _ | _ | 4.12 | |
| Critical Hdwy Stg 1 | | | _ | - | _ | _ |
| Critical Hdwy Stg 2 | 5.42 | - | - | - | - | - |
| Follow-up Hdwy | | | - | - | 2.218 | - |
| Pot Cap-1 Maneuver | 541 | 807 | - | - | 1336 | - |
| Stage 1 | 807 | - | - | - | - | - |
| Stage 2 | 789 | - | - | - | - | - |
| Platoon blocked, % | | | - | - | | - |
| Mov Cap-1 Maneuver | 529 | 807 | - | - | 1336 | - |
| Mov Cap-2 Maneuver | 529 | - | - | _ | - | - |
| Stage 1 | 807 | _ | - | _ | _ | _ |
| Stage 2 | 771 | _ | _ | _ | _ | _ |
| Stage 2 | 771 | | | | | |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 9.7 | | 0 | | 0.9 | |
| HCM LOS | Α | | | | | |
| | | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | nt | NBT | NBRV | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - | - | 110 | 1336 | - |
| HCM Lane V/C Ratio | | - | - | 0.024 | 0.02 | - |
| HCM Control Delay (s) | | - | - | 9.7 | 7.7 | 0 |
| HCM Lane LOS | | - | - | Α | Α | A |
| HCM 95th %tile Q(veh) | | - | _ | 0.1 | 0.1 | - |
| 2111 701110 2(1011) | | | | | | |

| Intersection | | | | | | |
|-----------------------------|------------|--------------|-----------|-------|-----------|------------|
| Int Delay, s/veh | 0.4 | | | | | |
| Movement | WBL | WBR | NBT | NBR | SBL | SBT |
| Lane Configurations | ₩. | אטא | 1 T | TIDIX | ODL | <u>उठा</u> |
| Traffic Vol, veh/h | 'T' | 5 | 169 | 3 | 5 | 145 |
| Future Vol, veh/h | 5 | 5 | 169 | 3 | 5 | 145 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| | | | Free | Free | Free | Free |
| Sign Control RT Channelized | Stop - | Stop None | Free - | None | Free - | None |
| | | None - | | | | None |
| Storage Length | 0 | | - | - | - | - |
| Veh in Median Storage | | - | 0 | - | - | 0 |
| Grade, % | 0 | - | 0 | - | - | 0 |
| Peak Hour Factor | 75 | 75 | 75 | 75 | 75 | 75 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 7 | 7 | 225 | 4 | 7 | 193 |
| | | | | | | |
| Major/Minor I | Minor1 | N | Major1 | ı | Major2 | |
| Conflicting Flow All | 434 | 227 | 0 | 0 | 229 | 0 |
| Stage 1 | 227 | - | _ | _ | - | - |
| Stage 2 | 207 | <u>-</u> | _ | _ | _ | _ |
| Critical Hdwy | 6.42 | 6.22 | _ | _ | 4.12 | _ |
| Critical Hdwy Stg 1 | 5.42 | - 0.22 | _ | _ | 7.12 | _ |
| Critical Hdwy Stg 2 | 5.42 | | - | | - | _ |
| Follow-up Hdwy | 3.518 | | - | _ | 2.218 | - |
| Pot Cap-1 Maneuver | 579 | 812 | _ | - | 1339 | - |
| • | 811 | 012 | - | - | 1339 | - |
| Stage 1 | | - | - | - | - | |
| Stage 2 | 828 | - | - | - | - | - |
| Platoon blocked, % | F70 | 040 | - | - | 4220 | - |
| Mov Cap-1 Maneuver | 576 | 812 | - | - | 1339 | - |
| Mov Cap-2 Maneuver | 576 | - | - | - | - | - |
| Stage 1 | 811 | - | - | - | - | - |
| Stage 2 | 823 | - | - | - | - | - |
| | | | | | | |
| Approach | WB | | NB | | SB | |
| HCM Control Delay, s | 10.4 | | 0 | | 0.3 | |
| HCM LOS | В | | U | | 0.5 | |
| HOW LOS | D | | | | | |
| | | | | | | |
| Minor Lane/Major Mvm | nt | NBT | NBRV | VBLn1 | SBL | SBT |
| Capacity (veh/h) | | - | - | 674 | 1339 | - |
| HCM Lane V/C Ratio | | - | - | | 0.005 | - |
| HCM Control Delay (s) | | - | - | | 7.7 | 0 |
| HCM Lane LOS | | - | - | В | Α | A |
| HCM 95th %tile Q(veh) | | - | - | 0.1 | 0 | - |
| | | | | | | |